



# CITY OF CHANDLER, ARIZONA CHANDLER MUNICIPAL AIRPORT TAXIWAY 'B' IMPROVEMENTS PHASE 1: TAXIWAY 'L' TO TAXIWAY 'N'

**MAYOR**  
KEVIN HARTKE

**VICE MAYOR**  
OD HARRIS

**COUNCIL**  
CHRISTINE ELLIS  
ANGEL ENCINAS  
MATT ORLANDO  
JANE POSTON  
MARK STEWART

**CITY PROJECT NO. AI2302.401  
ADOT GRANT NO. E3S4C  
DATE: SEPTEMBER 27, 2024**

**APPROVED:**

	DATE
<small>PUBLIC WORKS &amp; UTILITIES DIRECTOR 10/25/2024, 2:30:47 PM CIV24-0054 Daniel Haskins</small>	<small>DATE</small>
	DATE
<small>CITY ENGINEER FOR CIP 10/25/2024, 2:31:01 PM CIV24-0054 Daniel Haskins</small>	<small>DATE</small>
	DATE
<small>CIVIL PLAN REVIEW MANAGER 10/25/2024, 12:32:48 PM CIV24-0054 Jason Richardson</small>	<small>DATE</small>

**AS-BUILT CERTIFICATION:**

I HEREBY CERTIFY THAT THE "AS-BUILT" INFORMATION AS SHOWN HEREON WAS MADE UNDER MY SUPERVISION, OR AS NOTED, AND IS CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.

REGISTERED LAND SURVEYOR SIGNATURE	PRINTED NAME	DATE
FIRM NAME:		

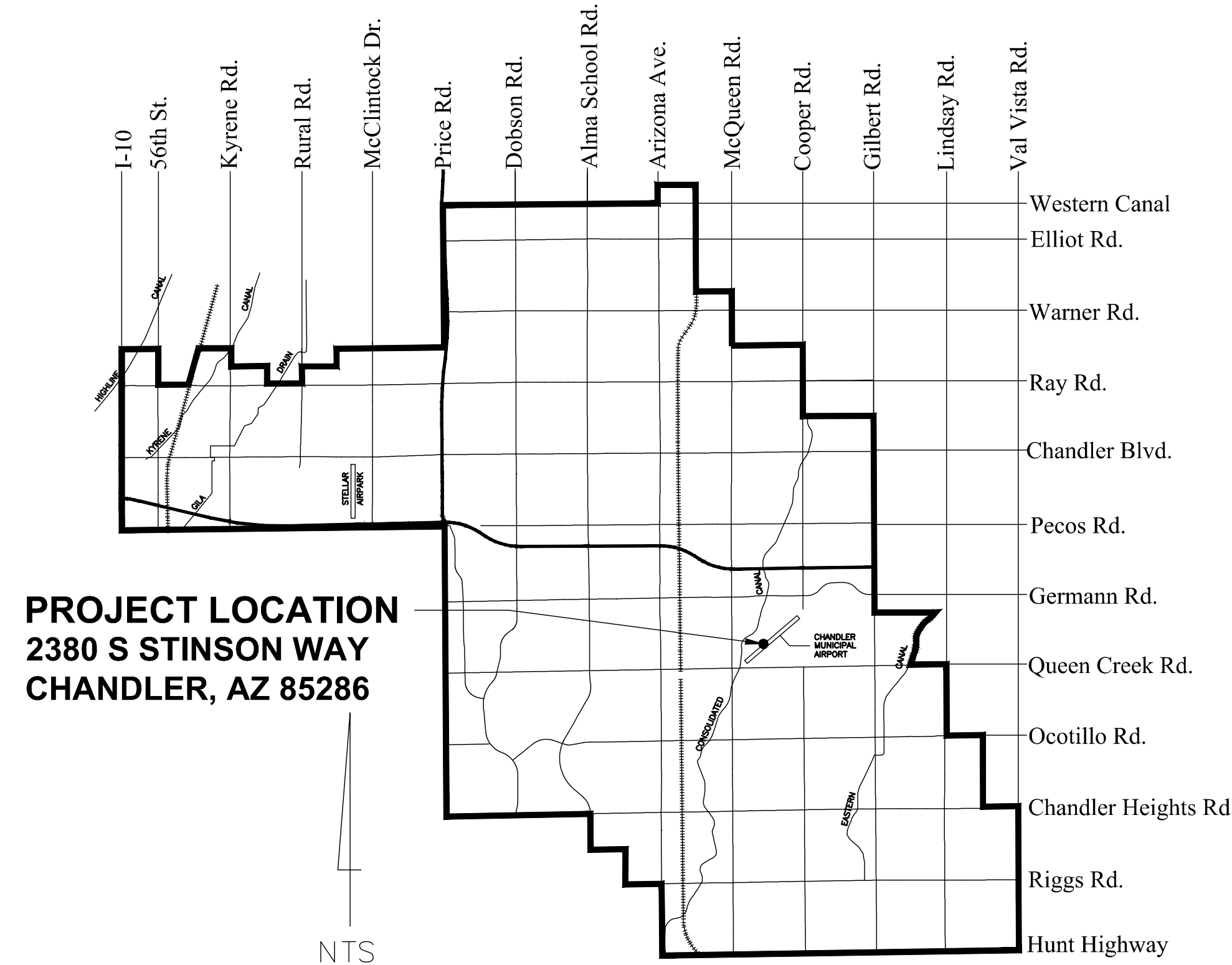
REGISTRATION NO.	EXPIRATION DATE
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I HEREBY CERTIFY THAT THE "AS-BUILT" INFORMATION SHOWN HEREON SATISFIES THE INTENT OF THE DESIGN.

REGISTERED CIVIL ENGINEER SIGNATURE	PRINTED NAME	DATE
FIRM NAME:		

REGISTRATION NO.	EXPIRATION DATE
------------------	-----------------

COORDINATION LIST		
DESIGN		
ORGANIZATION	CONTACT	PHONE
ENGINEER (CIVIL)	DUANE DANA	480.757.7876
UTILITIES		
CENTURY LINK (COMM)	JEANNET DEBOARD	480.768.4541
COX COMMUNICATIONS (COMM)	JENNIFER HERNANDEZ	623.328.3514
SALT RIVER PROJECT (IRRIGATION)	SUSANNA ORTEGA	602.236.5799
SALT RIVER PROJECT (ELECTRIC)	CHRISTY BALTRUS	602.236.3112
SOUTHWEST GAS (GAS)	GENE FLOREZ	480.730.3841



**PROJECT LOCATION**  
2380 S STINSON WAY  
CHANDLER, AZ 85286

**VICINITY MAP**

**CITY OF CHANDLER  
CHANDLER MUNICIPAL AIRPORT**  
2380 S STINSON WAY  
CHANDLER, AZ 85286  
PHONE NO. (480) 782 - 3545

**DIBBLE**  
DUANE DANA, PE  
1640 S. STAPLEY DRIVE, SUITE 120  
MESA, AZ 85204  
PHONE NO. (480) 757 - 7876  
DUANE.DANA@DIBBLECORP.COM



SHEET NO.  
**G1.0**  
1 OF 39

**AIRPORT (PROJECT) BENCHMARK:**

FOUND NGS PUBLISHED BENCHMARK CHD A DESCRIBED AS:  
BRASS CAP FLUSH, LOCATED SOUTH OF THE NORTHEAST END OF RUNWAY 22L  
N 826889.61  
E 735238.56  
PUBLISHED ELEVATION = 1239.90  
OBSERVED ELEVATION = 1239.94

**FEMA FLOOD ZONE INFORMATION:**

THIS PROJECT AREA IS ENTIRELY WITHIN THE LIMITS OF THE CHANDLER MUNICIPAL AIRPORT WITHIN FLOOD HAZARD AREA ZONE X, FIRM PANELS 04013C2743M.

STATUS: 100%

REV. NO.	DATE	DRWN	CHKD	REMARKS

J:\2024\1021015.11\_CHD\_TW\_B\_IMPROVEMENTS\_PHASE\_1\CAO\21015\_11-G10.DWG Sep. 27, 2024 11:14 AM DARIN OAKELEY

TAXIWAY 'B' IMPROVEMENT PHASE 1: TAXIWAY 'L' TO TAXIWAY 'N' PROJECT NO. AI2302.401 100% SUBMITTAL SEPTEMBER 27, 2024

**CHANDLER GENERAL NOTES**

1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE MOST CURRENT ADOPTED MAG SPECIFICATIONS AND STANDARD DETAILS AS SUPPLEMENTED BY THE CITY OF CHANDLER.
2. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO CONSTRUCTION.
3. THE CITY ENGINEER'S OFFICE SHALL BE NOTIFIED 24 HOURS PRIOR TO STARTING EACH PHASE OF CONSTRUCTION (480-782-3320).
4. PROJECT ELEVATIONS ARE BASED UPON NAVD 88 DATUM. THE FOLLOWING BENCHMARK WAS UTILIZED:

BENCHMARK NUMBER	NGVD 29 ELEVATION	DESCRIPTION	EQUATION	NAVD 88 ELEVATION
POINT 21	1241.595	NGS PUBLISHED BENCHMARK CHD A DESCRIBED AS: BRASS CAP FLUSH, LOCATED SOUTH OF THE NORTHEAST END OF RUNWAY 22L OBSERVED NORTHING = 826889.61 OBSERVED EASTING = 735238.56	NAVD'88 -1.70' = NGVD'29	PUBLISHED 1239.90 OBSERVED 1239.94

5. PRIOR TO ANY CONSTRUCTION IN THE PUBLIC RIGHT-OF-WAY, THE CONTRACTOR/DEVELOPER SHALL NOTIFY THE LANDSCAPE COMPLIANCE COORDINATOR AT 480-782-3428. ANY CONSTRUCTION WITHIN THE PUBLIC RIGHT-OF-WAY SHALL BE RESTORED TO ORIGINAL CONDITIONS USING THE FOLLOWING REQUIREMENTS:
  - A. ALL TREES IMPACTED BY CONSTRUCTION SHALL BE REPLACED WITH THE SAME SIZE AND TYPE OF TREE AT A LOCATION DETERMINED BY THE CITY;
  - B. ALL IRRIGATION SYSTEMS SHALL BE RESTORED TO FULLY FUNCTIONING STATUS. ANY IRRIGATION LOCATED BENEATH ASPHALT OR CONCRETE SHALL BE SLEEVED WITH SCHEDULE 40 PVC TWO NOMINAL SIZES LARGER;
  - C. GRANITE OF A SIZE AND COLOR TO MATCH EXISTING SHALL BE SPREAD A MINIMUM OF TWO INCHES THICK;
  - D. THE STREET DIVISION SHALL BE CONTACTED TO INSPECT ALL WORK BEFORE A CONDITIONAL ACCEPTANCE IS ISSUED;
  - E. THE CONTRACTOR/DEVELOPER SHALL MAINTAIN THE AREA FOR NINETY DAYS AFTER CONDITIONAL ACCEPTANCE. AFTER 90 DAYS THE CITY SHALL BE CONTACTED FOR FINAL ACCEPTANCE AND ASSUMPTION OF MAINTENANCE.
6. WORK PERFORMED BY THE CONTRACTOR:
  - A. ANY WORK PERFORMED WITHOUT THE APPROVAL OF THE CITY ENGINEER AND/OR ALL WORK AND MATERIAL NOT IN CONFORMANCE WITH THE SPECIFICATIONS IS SUBJECT TO REMOVAL AND REPLACEMENT AT THE CONTRACTOR'S EXPENSE.
  - B. WHENEVER THE INSPECTOR FINDS ANY WORK BEING PERFORMED IN A DANGEROUS OR UNSAFE MANNER, OR CONTRARY TO THE PLANS OR SPECIFICATIONS, OR NOT MEETING THE INTENT OF THE PLANS OR SPECIFICATIONS, THE INSPECTOR WILL ISSUE A STOP WORK ORDER. UPON ISSUANCE OF A STOP WORK ORDER, THE CITED WORK SHALL IMMEDIATELY CEASE. THE STOP WORK ORDER SHALL STATE THE REASON FOR THE ORDER, AND THE CONDITIONS WHICH CITED WORK IS AUTHORIZED TO RESUME UPON WRITTEN ORDER TO RESUME WORK. WHERE AN EMERGENCY EXISTS, THE INSPECTOR SHALL NOT BE REQUIRED TO GIVE A WRITTEN NOTICE PRIOR TO STOPPING THE WORK. THE CONTRACTOR IS RESPONSIBLE FOR ALL COSTS AND DELAYS FOR THE WORK RELATED TO THE STOP WORK ORDER.
7. THE CONTRACTOR SHALL UNCOVER ALL EXISTING CITY UTILITY LINES BEING TIED INTO TO VERIFY THEIR TYPE, CONDITION, LOCATION, INVERT, SLOPE, AND ANY OTHER INFORMATION NEEDED TO DETERMINE THAT THE UTILITY CONNECTION WILL FUNCTION AS DESIGNED. THE CONTRACTOR IS RESPONSIBLE FOR MAKING ANY REPAIRS NECESSARY TO THE LATERAL OR MAIN LINES OF THE CITY WATER, RECLAIMED WATER, SANITARY SEWER, AND/OR STORM DRAIN SYSTEM NECESSARY FOR THE CONNECTION TO FUNCTION AS DESIGNED. THE CONTRACTOR SHALL LOCATE OR HAVE LOCATED ALL EXISTING UNDERGROUND PRIVATE UTILITIES (ELECTRIC, TELEPHONE, PIPELINES, ETC.) AND STRUCTURES IN ADVANCE OF CONSTRUCTION AND SHALL ELIMINATE ALL CONFLICTS PRIOR TO START OF CONSTRUCTION. BLUE STAKE TELEPHONE (602) 263-1100.
8. THE CITY OF CHANDLER IS NOT RESPONSIBLE FOR LIABILITY INCURRED DUE TO DELAYS AND/OR DAMAGES TO UTILITIES IN CONJUNCTION WITH THIS CONSTRUCTION. THE CITY WILL NOT PARTICIPATE IN THE COST OF CONSTRUCTION OR UTILITY RELOCATION.
9. NO FINAL ACCEPTANCE SHALL BE ISSUED UNTIL 4 MIL PHOTO MYLAR REPRODUCIBLE "AS-BUILT" PLANS CERTIFIED AND SEALED BY A REGISTERED CIVIL ENGINEER, HAVE BEEN SUBMITTED AND ACCEPTED BY THE CITY ENGINEER.
10. APPLICATIONS FOR STREET CUT PERMITS MUST BE APPROVED BY THE CITY ENGINEER PRIOR TO APPROVAL OF IMPROVEMENT PLANS. ALL PAVEMENT REPLACEMENT SHALL BE IN ACCORDANCE WITH SECTION 336 AND AS MODIFIED BY CITY SUPPLEMENTS.

**CHANDLER GENERAL NOTES (CONT'D)**

11. BACKFILLING SHALL NOT BE STARTED UNTIL LINES ARE APPROVED BY THE CITY ENGINEER. ALL BACKFILL SHALL BE HALF-SACK CLSM UNLESS OTHERWISE APPROVED ON THE PLANS OR SPECIFICATIONS. ABC OR NATIVE MATERIAL SHALL BE INSTALLED IN ACCORDANCE WITH MAG SECTION 601, TYPE I. BACKFILL PLACEMENT AND SURFACE REPLACEMENT SHALL BE IN ACCORDANCE WITH MAG STD DTL 200 T-TOP WITH 16" MIN DEPTH ABC SHELF, UNLESS NOTED OTHERWISE ON THE PLANS OR SPECIFICATIONS.
12. CONSTRUCTION OF CONCRETE OR PAVING ITEMS SHALL NOT COMMENCE UNTIL ALL UNDERGROUND WORK HAS BEEN INSPECTED AND TESTED.
13. DISPOSAL OF AND STOCKPILING OF EXCESS MATERIAL WITHIN THE CHANDLER CITY LIMITS OR PLANNING AREA SHALL BE DONE IN SUCH A WAY THAT WILL NOT CREATE A NUISANCE. THE PLACING OF MATERIAL ON PRIVATE PROPERTY OF ANOTHER REQUIRES WRITTEN AUTHORIZATION. EARTHWORK STOCKPILES ARE NOT TO EXCEED 6 FEET IN HEIGHT. SLOPES ON ALL SIDES OF THE STOCKPILE SHALL NOT EXCEED A 1 TO 2 RATIO OF HEIGHT TO LENGTH. ANY EARTHWORK STOCKPILE, EVEN LESS THAN 6 FEET, MUST BE REMOVED WITHIN 7 DAYS OF CITY NOTIFICATION IF DUST SUPPRESSION EFFORTS FAIL TO MAINTAIN SATISFACTORY AIRBORNE CONTAMINANT CONTROL.
14. TRAFFIC CONTROL SHALL BE MAINTAINED IN ACCORDANCE WITH THE CHANDLER TRAFFIC BARRICADE MANUAL.
15. THE CONTRACTOR SHALL PROVIDE ADEQUATE MEANS FOR CLEANING TRUCKS AND/OR OTHER EQUIPMENT OF MUD PRIOR TO ENTERING PUBLIC STREETS, AND IT IS THE CONTRACTOR'S RESPONSIBILITY TO CLEAN STREETS, ALLAY DUST, AND TAKE WHATEVER MEASURES ARE NECESSARY TO INSURE THAT ALL ROADS ARE MAINTAINED IN A CLEAN, MUD AND DUST-FREE CONDITION AT ALL TIMES THE CONTRACTOR SHALL UTILIZE A PM10-CERTIFIED STREET SWEEPER FOR STREET CLEANING.
16. AN APPROVED SET OF PLANS SHALL BE MAINTAINED ON THE JOB SITE AT ALL TIMES THAT WORK IS IN PROGRESS. DEVIATION FROM THE PLANS SHALL NOT BE ALLOWED WITHOUT AN APPROVED PLAN REVISION.
17. A MINIMUM HORIZONTAL SEPARATION OF SIX (6) FEET IS REQUIRED BETWEEN SEWER SERVICES AND WATER OR FIRELINE SERVICES. A MINIMUM HORIZONTAL SEPARATION OF SIX (6) FEET IS REQUIRED BETWEEN RECLAIMED WATER SERVICES AND SEWER, WATER, OR FIRELINE SERVICES.
18. SETBACKS FOR UTILITY POLES, STRUCTURES, AND OTHER SIMILAR FACILITIES (NOT INCLUDING LANDSCAPING) GREATER THAN 18 INCHES IN HEIGHT SHALL BE 5.5 FEET FROM THE BACK OF CURB UNLESS APPROVED IN WRITING BY THE CITY ENGINEER. IN CASES WHERE THE FACILITIES ARE ADJACENT TO A DECELERATION LANE, BUS BAY, OR MEDIAN CURB, THE SETBACK CAN BE REDUCED TO 2.5 FEET FROM THE BACK OF CURB.
19. SCREEN WALLS OVER 7', RETAINING WALLS OVER 4' MUST HAVE A BUILDING PERMIT AND BE IN CONFORMANCE WITH CHANDLER BUILDING CODE.
20. ALL UNDERGROUND FACILITIES INSTALLED IN ANY REAL PROPERTY OUTSIDE THE CITY'S RIGHT-OF-WAY SHALL COMPLY WITH THE FOLLOWING MARKING STANDARDS IN ACCORDANCE WITH HOUSE BILL 2256. A YELLOW INSULATED COPPER WIRE OR OTHER APPROVED CONDUCTOR SHALL BE INSTALLED ADJACENT TO UNDERGROUND UTILITIES FOR FIRE LINE PIPING, POTABLE WATER DISTRIBUTION PIPING, SANITARY SEWER LINES, STORMWATER PIPING, RECLAIMED WATER PIPING, GRAVITY FLOW IRRIGATION PIPING AND PRESSURIZED IRRIGATION PIPING LARGER THAN TWO (2) INCHES IN DIAMETER UNLESS THESE FACILITIES CAN BE DETECTED FROM ABOVE GROUND WITH AN ELECTRONIC LOCATING DEVICE. ACCESS SHALL BE PROVIDED TO THE TRACER WIRE OR THE TRACER WIRE SHALL TERMINATE ABOVE GROUND AT EACH END OF THE PIPING. THE TRACER WIRE SHALL NOT BE LESS THAN 18 AWG AND THE INSULATION TYPE SHALL BE SUITABLE FOR DIRECT BURIAL.
21. ANY CONSTRUCTION DETOURS WILL REQUIRE AN ALL-WEATHER SURFACE PER MARICOPA COUNTY AIR QUALITY REQUIREMENTS.
22. THE CONTRACTOR SHALL SUPPLY PUBLIC WORKS INSPECTOR CONSTRUCTION MATERIAL TESTING DOCUMENTATION THROUGHOUT THE PROJECT.
23. ALL SITE IMPROVEMENTS, INCLUDING LANDSCAPE AND SITE CLEANUP, MUST BE COMPLETE PRIOR TO CERTIFICATE OF OCCUPANCY FOR ANY BUILDING WITHIN A PHASE.
24. THE FOLLOWING UTILITY COMPANIES HAVE BEEN IDENTIFIED BY BLUE STAKE COORDINATION AND PROJECT PLANS HAVE BEEN SUBMITTED MOST RECENTLY AS NOTED:

UTILITIES WITHIN AREA AFFECTED BY THE PROJECT		
UTILITY CONTACT	DATE SUBMITTED	DATE OF RESPONSE
CENTURY LINK	06/20/2024	NO RESPONSE
COX COMMUNICATIONS	06/20/2024	07/01/2024
SALT RIVER PROJECT (IRRIGATION)	06/20/2024	07/26/2024
SALT RIVER PROJECT (ELECTRIC)	06/20/2024	07/26/2024
SOUTHWEST GAS	06/20/2024	06/27/2024

25. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL CONSTRUCTION STAKING AND SURVEY.

**CHANDLER PAVING NOTES**

1. THE LOCATION OF ALL VALVES MUST BE REFERENCED AT ALL TIMES BY THE CONTRACTOR DURING CONSTRUCTION.
2. NO PAVING CONSTRUCTION SHALL BE PERFORMED UNTIL ALL UNDERGROUND UTILITIES WITHIN THE RIGHT-OF-WAY HAVE BEEN COMPLETED.
3. THE BASE COURSE WILL NOT BE PLACED ON SUBGRADE UNTIL BASE REQUIREMENTS HAVE BEEN COMPLETED AND ACCEPTED BY THE CITY ENGINEER.
4. GUTTERS WILL BE WATER TESTED IN THE PRESENCE OF THE CITY ENGINEER TO INSURE PROPER DRAINAGE PRIOR TO FINAL APPROVAL.
5. THE EXACT POINT OF PAVEMENT MATCHING FOR TERMINATION AND OVERLAY MAY BE DETERMINED IN THE FIELD BY THE CITY ENGINEER.
6. NO JOB WILL BE CONSIDERED COMPLETE UNTIL ALL CURBS, PAVEMENT AND SIDEWALKS HAVE BEEN SWEEPED CLEAN OF ALL DIRT AND DEBRIS.
7. STREET NAME SIGNS WILL BE INSTALLED BY THE CITY ON DEVELOPER INSTALLED POLES AT THE DEVELOPER'S EXPENSE. POLES SHALL BE PER C-613. PAYMENT WILL BE COLLECTED AT THE TIME A PAVING PERMIT IS ISSUED.
8. ALL TRAFFIC CONTROL SIGNS SHALL BE CONSTRUCTED OF HIGH INTENSITY GRADE SHEETING, UNLESS OTHERWISE NOTED.
9. ALL STREET SECTIONS EXCEPT ARTERIALS SHALL HAVE INSTALLED A PRESERVATIVE SEAL COAT PER MAG SECTION 334 AND SHALL BE TIRE RUBBER MODIFIED SURFACE SEAL PER MAG SECTION 718. THE SEAL SHALL BE INSTALLED AT THE END OF THE WARRANTY PERIOD, OR AT THE CITY'S OPTION, A FEE MAY BE COLLECTED IN LIEU OF THE APPLICATION.
10. ALL CURB-OPENING CATCH BASIN ACCESS COVERS SHALL BE PER MAG STD DTL 536, ALTERNATE COVER. A STORM DRAIN INLET MARKER IN ACCORDANCE WITH C-508 SHALL BE INSTALLED ON THE TOP OF THE CURB AND ALIGNED WITH THE CENTER OF THE INLET.
11. PAVEMENT MATCHING AND SURFACING REPLACEMENT SHALL BE IN ACCORDANCE WITH MAG STD DTL 200, MAG SECTION 336, AND SECTION 337.

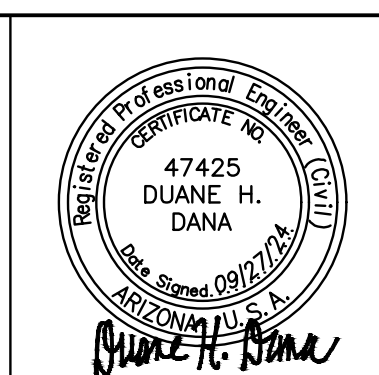
**CHANDLER GRADING & DRAINAGE NOTES**

1. A GRADING PERMIT IS REQUIRED.
2. NO MINIMUM FINISH FLOOR ELEVATION SHALL BE ALTERED.
3. STAKING FINISH FLOOR ELEVATION IS THE RESPONSIBILITY OF THE DEVELOPER AND HIS ENGINEER.
4. CONTRACTOR SHALL PROVIDE GRADING FOR POSITIVE DRAINAGE IN ALL RETENTION BASINS AT ELEVATIONS AS SHOWN ON THE PLANS. BOTTOM OF BASIN SHALL BE GRADED TO DRAIN TOWARD DRYWELLS (WHEN USED). MAXIMUM SIDESLOPES SHALL BE 4:1.
5. DRYWELL INLET GRATE SHALL BE FLUSH WITH ROAD SURFACE OR TURF, OR 1-1/2" ABOVE THE FINISHED GRADE OF DECOMPOSED GRANITE LANDSCAPED AREAS.
6. DRILLING LOGS FOR DRYWELLS WILL BE FURNISHED TO THE CITY INSPECTOR PRIOR TO FINAL ACCEPTANCE.
7. A PERCOLATION TEST SHALL BE REQUIRED OF COMPLETED DRYWELLS PRIOR TO ACCEPTANCE. SHOULD EXISTING SOIL CONDITIONS BE ENCOUNTERED WHICH LACK SUFFICIENT PERCOLATION RATES, ADDITIONAL DRYWELLS OR AN ALTERNATE METHOD OF STORM WATER RUN-OFF DISPOSAL WILL BE REQUIRED.
8. DRYWELL CONSTRUCTION SHALL BE DONE ONLY BY CONTRACTORS LICENSED BY THE ARIZONA DEPARTMENT OF ENVIRONMENTAL QUALITY. APPLICATION FOR DRYWELL REGISTRATION WAS SUBMITTED TO ARIZONA DEPARTMENT OF ENVIRONMENTAL QUALITY ON NOVEMBER 15, 2018.
9. THE APPROVED DRYWELL REGISTRATION SHALL BE SUBMITTED TO THE CITY BY THE DEVELOPER OR HIS ENGINEER AT THE TIME AS-BUILTS ARE SUBMITTED.
10. ALL WEEP HOLES IN WALLS SHALL BE PROVIDED WITH EROSION PROTECTION 12" THICK WITH D50 = 4" RIPRAP, 24" IN WIDTH, EXTENDED TO THE BACK OF SIDEWALK OR TO THE BOTTOM OF RETENTION BASIN, WHICHEVER APPLIES.
11. A RETAINING WALL WILL BE REQUIRED IF AT THE COMPLETION OF GRADING THERE EXISTS MORE THAN ONE FOOT OF DIFFERENCE IN ELEVATION BETWEEN THIS SITE AND ADJACENT PROPERTIES.
12. SCREEN WALLS OVER 7', RETAINING WALLS OVER 4' MUST HAVE A BUILDING PERMIT AND BE IN CONFORMANCE WITH CHANDLER BUILDING CODE.
13. THE EXISTING RETENTION AND DRAINAGE FACILITIES ON THIS SITE WILL NOT BE REMOVED FROM SERVICE UNTIL THE PERMANENT RETENTION AND DRAINAGE FACILITIES ARE FUNCTIONAL.

DRYWELL KEY MAP #	ADEQ REG. #	FIELD PERC RATE (CFS)

REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24



CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
**TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'**

**GENERAL NOTES**

PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-G1X.DWG

DWG NO.  
**G1.1**

SHEET 2 OF 39

**ABBREVIATIONS**

AB	AGGREGATE BASE	LT	LEFT
ABND	ABANDONED		
AC	ASPHALTIC CONCRETE	MAG	MARICOPA ASSOCIATION OF GOVERNMENTS/MAG NAIL
ADOT	ARIZONA DEPARTMENT OF TRANSPORTATION	MAX	MAXIMUM
ASTM	AMERICAN SOCIETY FOR TESTING MATERIALS	MIN	MINIMUM
AWG	AMERICAN WIRE GAUGE	ME	MATCH EXISTING
BCF	BRASS CAP FLUSH		
BCHH	BRASS CAP IN HANDHOLE	NPI	NON-PAY ITEM
BM	BENCHMARK	NTS	NOT TO SCALE
C	FINISH CONCRETE ELEVATION	OFZ	OBSTACLE FREE ZONE
COC	CITY OF CHANDLER		
CHD	CHANDLER MUNICIPAL AIRPORT	PSI	POUNDS PER SQUARE INCH
CL	CENTERLINE	PVC	POINT OF VERTICAL CURVATURE
CLSM	CONTROLLED LOW STRENGTH MATERIAL	PVI	POINT OF VERTICAL INTERSECTION
CONC	CONCRETE	PVMT	PAVEMENT
CONST	CONSTRUCTION		
CTR	CENTER	ROFA	RUNWAY OBJECT FREE AREA
DET	DETAIL	RGRCP	RUBBER GASKET REINFORCED CONCRETE PIPE
DIA	DIAMETER	RT	RIGHT
DWG	DRAWING	RSA	RUNWAY SAFETY AREA
		RW	RUNWAY
ELEC	ELECTRICAL	SECT	SECTION
EL	ELEVATION	SHT	SHEET
EXST	EXIST(ING)	SPEC	SPECIFICATIONS
		STA	STATION
FAA	FEDERAL AVIATION ADMINISTRATION	TL	TAXILANE
FND	FOUND	TW	TAXIWAY
GB	GRADE BREAK	TSA	TAXIWAY SAFETY AREA
GND	GROUND	TOFA	TAXIWAY OBJECT FREE AREA
GTP	GENERAL TECHNICAL PROVISION	TYP	TYPICAL
		UNK	UNKNOWN

**LEGEND**

	SURVEY MONUMENT AS NOTED		TAXIWAY SAFETY AREA
	PROJECT BENCHMARK		TAXIWAY OBJECT FREE AREA
	EXISTING AIRFIELD LIGHT		RUNWAY OBSTACLE FREE ZONE
	EXISTING EDGE OF PAVEMENT		RUNWAY SAFETY AREA
	EXISTING MAJOR CONTOURS		RUNWAY OBJECT FREE AREA
	EXISTING MINOR CONTOURS		GRADE BREAK
	PROPOSED MAJOR CONTOURS		FLOW LINE
	PROPOSED MINOR CONTOURS		STORM DRAIN W/ HEADWALL
	CHAIN-LINK FENCE		EXISTING AC PAVEMENT
	LOW PROFILE LIGHTED BARRICADE		AC PAVEMENT REMOVAL
	TYPE II BARRICADE		AC TAXIWAY PAVEMENT SECT A, DWG G2.1
	AIRCRAFT DETOUR		AC TAXIWAY SHOULDER PAVEMENT SECT B, DWG G2.1
	CONTRACTOR SITE ACCESS & HAUL ROUTE		EXISTING AC PAVEMENT
	SAWCUT		SEEDING
	PAVEMENT ELEV		
	EXISTING GRADE		
	WIND CONE		
	PHASE LINE		

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**EARTHWORK QUANTITIES**

NOTE: THE FOLLOWING RAW, UNADJUSTED, IN-PLACE EARTHWORK QUANTITIES ARE PROVIDED FOR BIDDING PURPOSES. EXCESS EXCAVATION (IF NEEDED) IS STOCKPILED ON SITE AS SHOWN ON SHT G1.4.

1,024 CY CUT  
1,301 CY FILL

**CHANDLER MUNICIPAL AIRPORT  
TAXIWAY 'B' IMPROVEMENTS PHASE 1  
100% ESTIMATED QUANTITIES**

LINE No.	ITEM No.	DESCRIPTION	APPROX. QTY.	UNIT	AS-BUILT QTY.
<b>CIVIL</b>					
1	C-100.14.1	Contractor Quality Control Program (CQCP)	1	LS	
2	C-102-6.1	Storm Water Pollution Prevention Plan (SWPPP)	1	LS	
3	C-105-6.1	Mobilization	1	LS	
4	SP-50.01.1	Location of Underground Utilities	1	LS	
5	SP-60.05.1	Airfield Safety and Security	1	LS	
6	SP-80.01.1	Riprap (D50=6-Inch, T = 12-Inch)	31	SY	
7	P-101-5.1	Sawcut AC Pavement (Full Depth ± 4-Inch)	421	LF	
8	P-101-5.2	Remove AC Pavement (Full Depth ± 4-Inch)	583	SY	
9	P-152-4.1	Unclassified Excavation	1,100	CY	
10	P-152-4.2	Unsuitable Excavation and Replacement, Backfill and Compaction	220	CY	
11	P-152-4.3	Compacted Subgrade (12-Inch Depth)	9,251	SY	
12	P-208-5.1	Taxiway Aggregate Base Course (9-Inch Depth)	6,318	SY	
13	P-208-5.2	Shoulder Aggregate Base Course (6-Inch Depth)	2,933	SY	
14	P-401-8.1	Taxiway Asphalt Surface Course (4-Inch Depth, PG 70-22)	6,318	SY	
15	P-403-8.1	Shoulder Asphalt Surface Course (3-Inch Depth, PG 70-22)	2,933	SY	
16	P-620-5.1	Obliterate & Seal Pavement Markings	1,583	SF	
17	P-620-5.2	Temporary Pavement Markings	3,975	SF	
18	P-620-5.3	Permanent Pavement Markings	3,975	SF	
19	T-901-5.1	Seeding	1.9	AC	
<b>ELECTRICAL</b>					
20	L-100-5.1	Remove and Salvage Existing Taxiway Edge Light and Isolation Transformer Demolish Base Can	6	EA	
21	L-100-5.2	Remove and Salvage Existing Taxiway Edge Light and Isolation Transformer and Return to Owner - Base Can to Remain	3	EA	
22	L-100-5.3	Remove Existing Conductors Back to Next Adjacent Light fixture or Hand Hole, Conduit to Remain	726	LF	
23	L-100-5.4	Excavate and Remove Existing Conduit and Conductors	418	LF	
24	L-100-5.5	Remove and Salvage Existing Airfield Guidance Sign and Return to Owner - Remove Concrete Sign Base	2	EA	
25	L-100-5.6	Remove and Salvage Existing Airfield Guidance Sign and Isolation Transformer and Return to Owner, Sign Base to Remain	1	EA	
26	L-108-5.1	L-824, Type C, 1/C #8 AWG, 5kV Cable	4,525	LF	
27	L-108-5.2	L-824, Type C, 2/C #8 AWG, 5kV Cable	941	LF	
28	L-110-5.1	Single-Way 1-2" Conduit Direct Buried	3,424	LF	
29	L-110-5.2	Multiple-Way 2-2" Conduit Concrete Encased	180	LF	
30	L-110-5.3	Multiple-Way 4-2" Conduit Concrete Encased	95	LF	
31	L-115-5.1	New 2'x3'x3' Hand Hole with Aircraft-Rated Lid with Spring Assisted Opening	2	EA	
32	L-115-5.2	Install New Steel Blank Cover on Existing L-867 Base Can with New Bolts	3	EA	
33	L-125-5.1	New L-861T Quartz Taxiway Edge Light and Isolation Transformer on New L-867 Base Can	37	EA	
34	L-125-5.2	New L-858 (L) LED Size 2, Style 3, Class 2, 2-Module Airfield Guidance Sign on New Concrete Base	2	EA	
35	L-125-5.3	New L-858 (L) LED Size 2, Style 3, Class 2, 3-Module Airfield Guidance Sign on New Concrete Base	3	EA	
36	L-125-5.4	New L-858(L) LED Size 2, Style 3, Class 2, 3-Module Airfield Guidance Sign on Existing Concrete Sign Base	1	EA	
37	L-125-5.5	New L-858 Size 2, Unlighted Taxiway Ending Marker Sign on New Concrete Sign Base	1	EA	



CITY OF CHANDLER, ARIZONA  
CHANDLER MUNICIPAL AIRPORT  
TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
TAXIWAY 'L' TO TAXIWAY 'N'

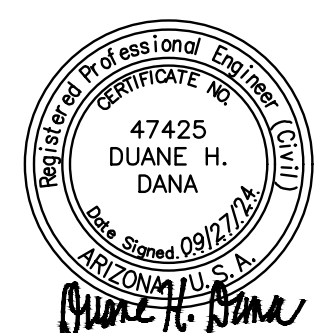
ABBREVIATIONS, LEGEND,  
SHEET INDEX & QUANTITIES

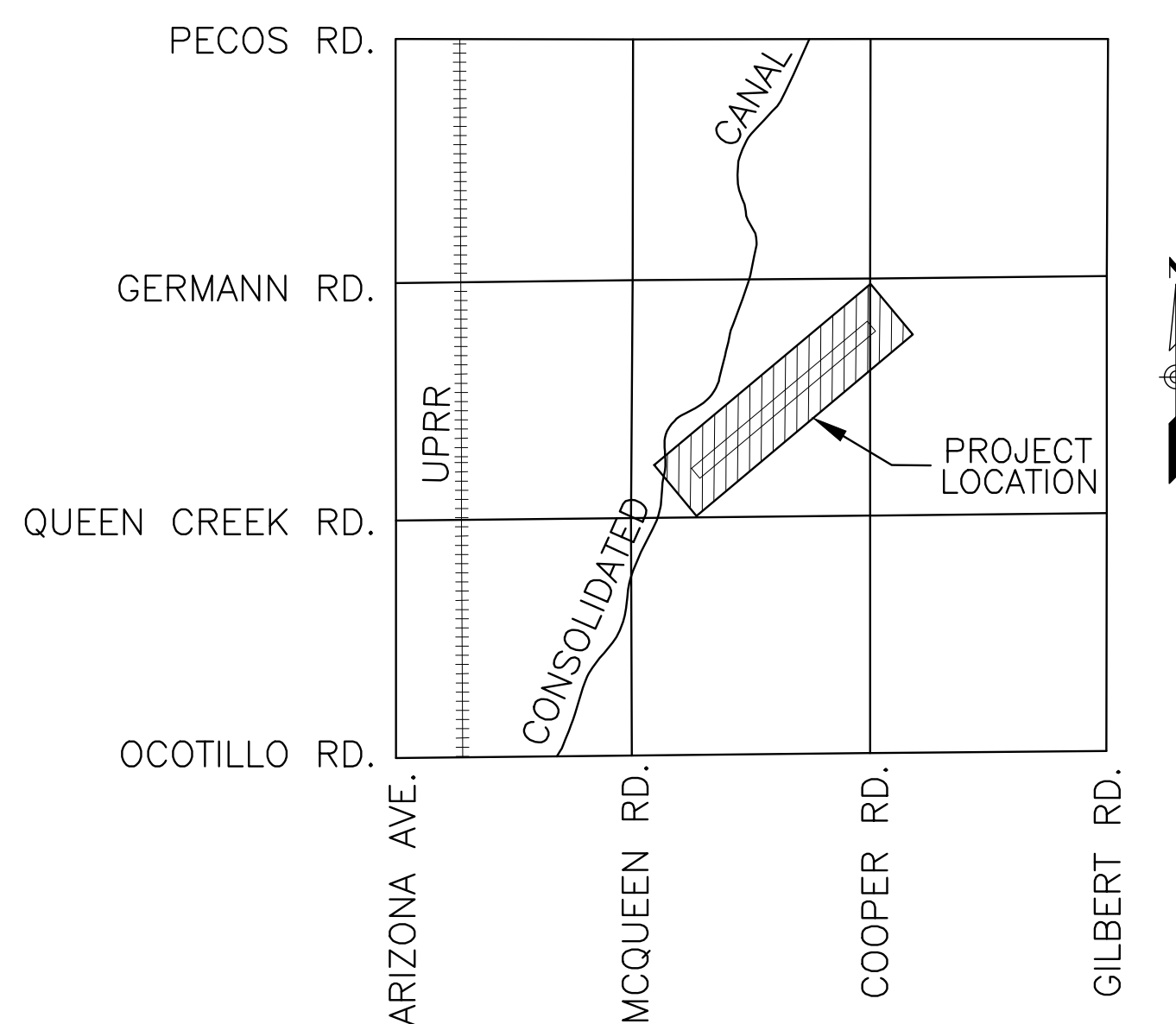
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FILE NAME: 21015\_11-G1X.DWG  
DWG NO.  
**G1.2**  
SHEET 3 OF 39

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REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
DRAWN BY: ERG  
SHEET CHK'D BY: MJH  
CROSS CHK'D BY: DHD  
APPROVED BY: DHD  
DATE: 09/27/24





POINT DATA TABLE				
POINT NO	GRID NORTHING	GRID EASTING	ELEVATION	DESCRIPTION
13	823837.273	729584.224	1229.02	FND-BCF CHD C
15	824442.072	731044.209	1232.46	FNC-BCF P10 C
16	823286.172	733613.815	1239.78	FND-BCF C.M.C.N. BM 42
20	826354.009	730961.371	1230.29	FND-IBHH N522
21	826889.614	735238.557	1239.94	BM FND-BCF CHD A
22	826027.751	732154.536	1232.94	FND-BCF CHD B
36	824225.649	731330.476	1235.91	FND-MAG
37	827383.125	735036.607	1243.29	FND-MAG

**LEGEND**

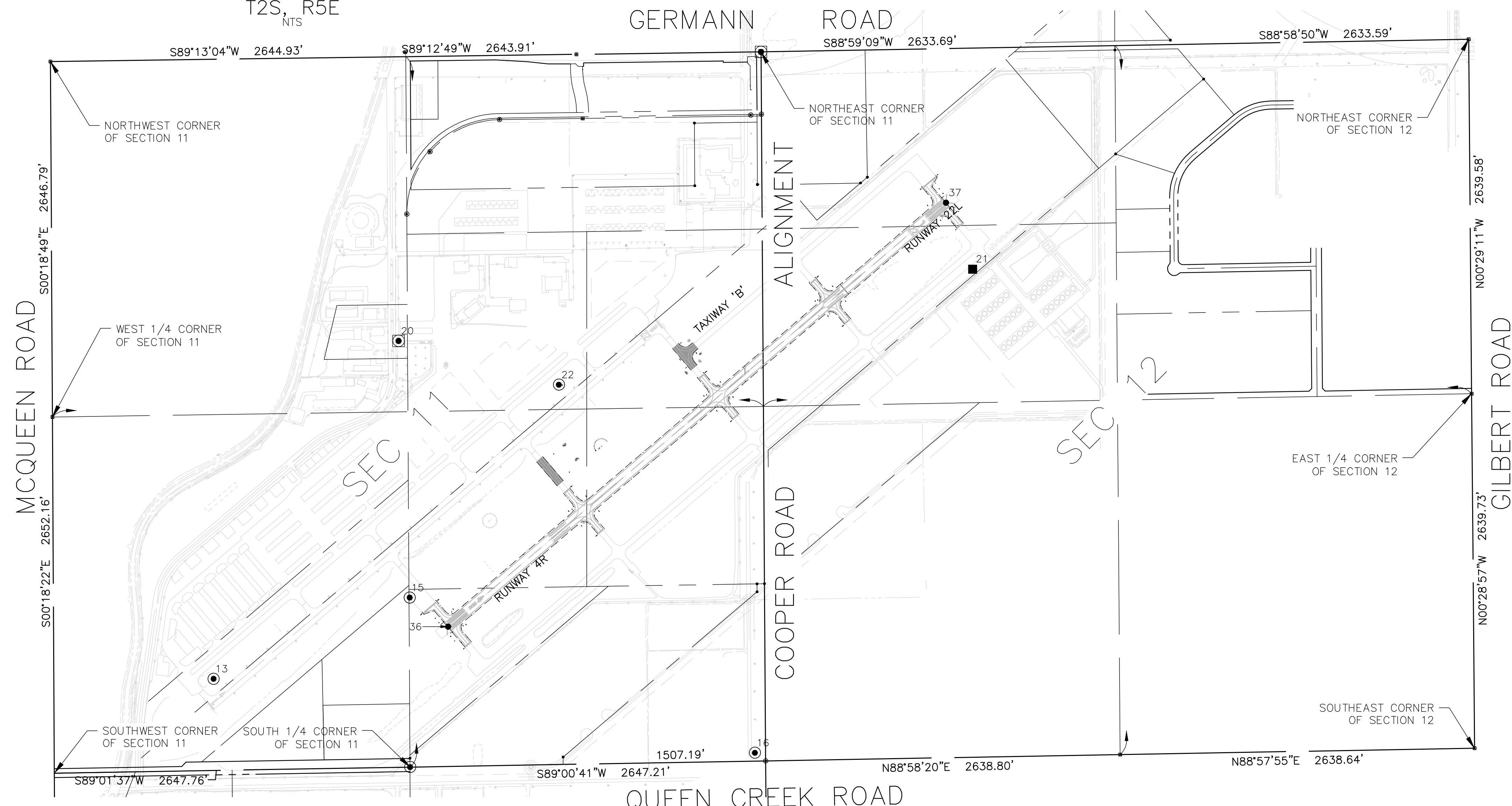
- ◻ FOUND MONUMENT AS NOTED
- PROJECT BENCHMARK
- SECTION LINE
- - - MID-SECTION LINE
- · - · - CENTERLINE
- · - · - PROPERTY LINE
- FND-BCHH FOUND BRASS CAP IN HANDHOLE
- FND-BCF FOUND BRASS CAP FLUSH
- FND-MAG FOUND MAG NAIL
- COC CITY OF CHANDLER

**BASIS OF BEARING**

AN ASSUMED BEARING ALONG THE CENTERLINE OF RUNWAY 4 RIGHT BEING N49°34'17"E MEASURED BETWEEN TWO MAG NAIL IN ASPHALT.

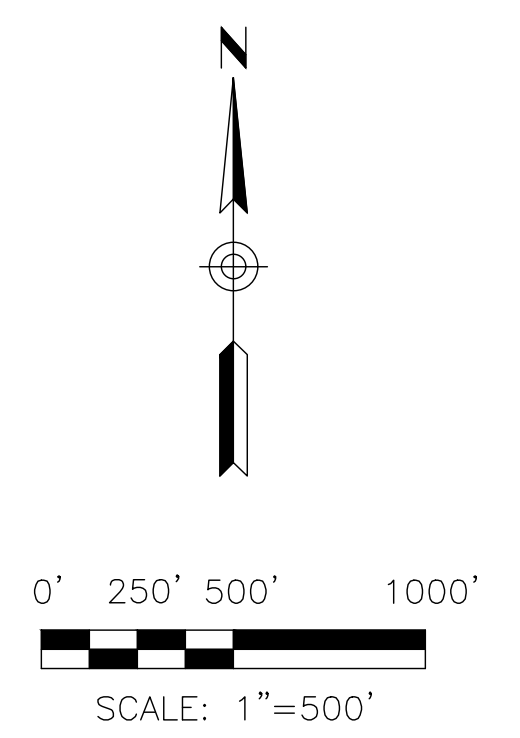
**SURVEYOR'S NOTES**

- COORDINATES WERE VERIFIED IN THE FIELD USING REAL TIME KINEMATIC GPS OBSERVATIONS RELATIVE TO PUBLISHED CONTROL POINTS.
- SURVEYED DURING THE MONTH OF MAY 2023.
- CONTRACTOR SHALL VERIFY HORIZONTAL AND VERTICAL CONTROLS IN THE FIELD PRIOR TO CONSTRUCTION.
- PROJECT METADATA:  
UNITS:  
COORDINATES, DISTANCES AND ELEVATIONS ARE SHOWN IN US SURVEY FEET.  
HORIZONTAL DATUM (BASIS OF BEARINGS):  
NAD83 (2011 EPOCH) ARIZONA CENTRAL ZONE, GRID  
VERTICAL DATUM:  
NAVD 88  
PROJECT BENCHMARK: POINT# 21  
FOUND NGS PUBLISHED BENCHMARK CHD A DESCRIBED AS:  
BRASS CAP FLUSH, LOCATED SOUTH OF THE NORTHEAST END OF RUNWAY 22L  
OBSERVED NORTHING = 826889.614  
OBSERVED EASTING = 735238.557  
PUBLISHED ELEVATION = 1239.895  
OBSERVED ELEVATION = 1239.943
- THE COORDINATES PRESENTED ARE SHOWN TO THREE DECIMAL PLACES FOR CALCULATION PURPOSES AND ARE NOT REPRESENTATIVE OF THE PRECISION OF THE SURVEY MEASUREMENTS
- THIS IS NOT A PROPERTY BOUNDARY SURVEY.



THE SURVEY DATA USED FOR DESIGN MEETS (OR EXCEEDS) THE CRITERIA OF A LEVEL 1A SURVEY AS FOLLOWS

HORIZONTAL		VERTICAL	
CODE	TOLERANCE	CODE	TOLERANCE
1	+20FT (6 M)	A	+3FT (1 M)



REV. NO.	DATE	DRWN	CHKD	REMARKS

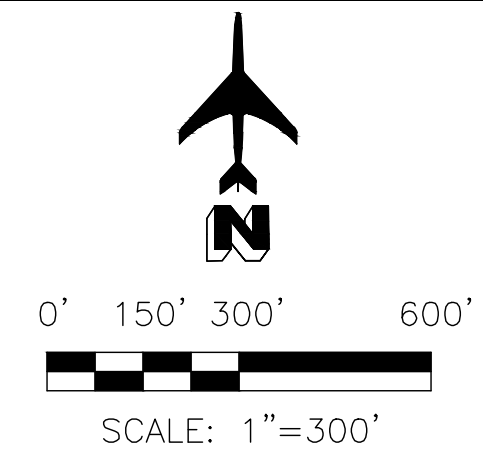
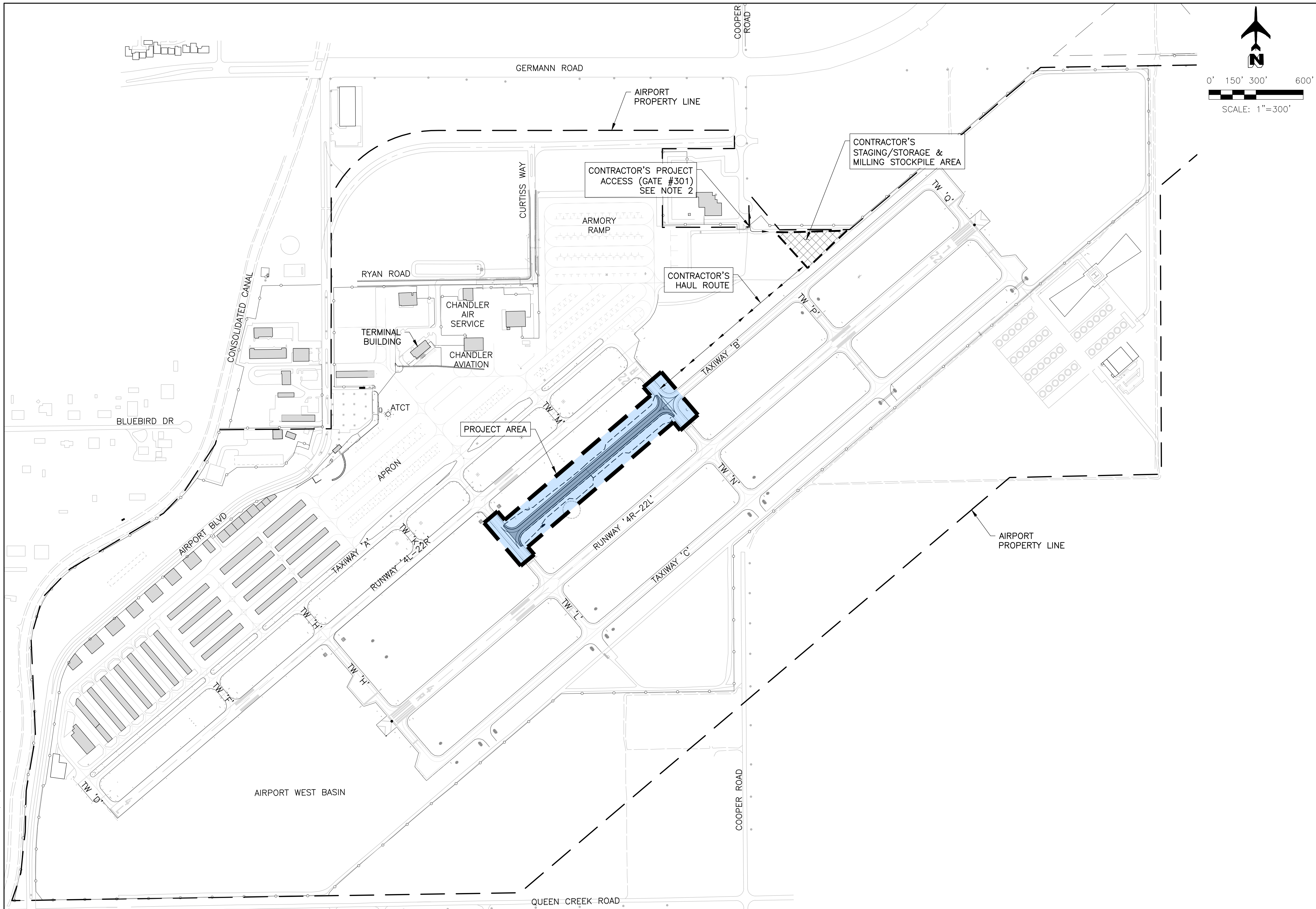
DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/06/24



CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

**SURVEY CONTROL PLAN**



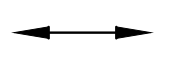

PROJECT NO. 1021015.11  
 FILE NAME: 21015.11-CTRL.DWG  
 DWG NO.  
**G1.3**  
 SHEET 4 OF 39



**CONTRACTOR ACCESS & STAGING NOTES**

1. CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF THEIR OWN EQUIPMENT. CONTRACTOR MAY INSTALL TEMPORARY FENCING AROUND THEIR STAGING AREA AT THEIR OWN EXPENSE, (NPI).
2. CONTRACTOR SHALL PROTECT ALL HAUL ROAD ACCESS POINTS TO THE AIRFIELD FROM UNAUTHORIZED ENTRY. CONTRACTOR SHALL KEEP ACCESS GATES CLOSED AND LOCKED OR POST GATE GUARD(S) AT GATES DURING CONTRACTOR'S WORKING HOURS, (NPI).
3. CONTRACTOR SHALL REPAIR ANY HAUL ROADS USED AND ANY OTHER AREAS DISTURBED BY THE CONTRACTOR THAT ARE OUTSIDE OF THE PROJECT LIMITS TO PRE-CONTRACT CONDITIONS. REPAIRS SHALL BE MADE TO THE SATISFACTION OF THE OWNER/AIRPORT AND/OR ENGINEER, AND SHALL BE AT THE CONTRACTOR'S SOLE EXPENSE, (NPI). THE CONTRACTOR SHALL PERFORM A PRE-CONSTRUCTION VISUAL SURVEY OF THE HAUL ROUTE IN CONCERT WITH THE ENGINEER, AIRPORT STAFF AND CITY STAFF.
4. CONTRACTOR WILL BE RESPONSIBLE FOR OBTAINING AND HAULING WATER TO THE WORK AREA, (NPI).
5. CONTRACTOR SHALL NOT STRAY FROM HAUL ROUTE ON EXISTING MILLINGS ROAD IN THE VICINITY OF SECURE GATE AND THE CONTRACTOR MILLING STOCKPILE AREA.

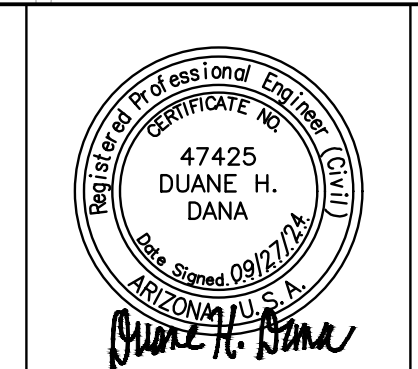
**LEGEND**

-  CONTRACTOR'S STAGING/STORAGE & MILLINGS STOCKPILE AREA
-  APPROXIMATE WORK LIMITS
-  CONTRACTOR HAUL ROUTE
-  FOUND MONUMENT AS NOTED ON SURVEY CONTROL PLAN - G1.3



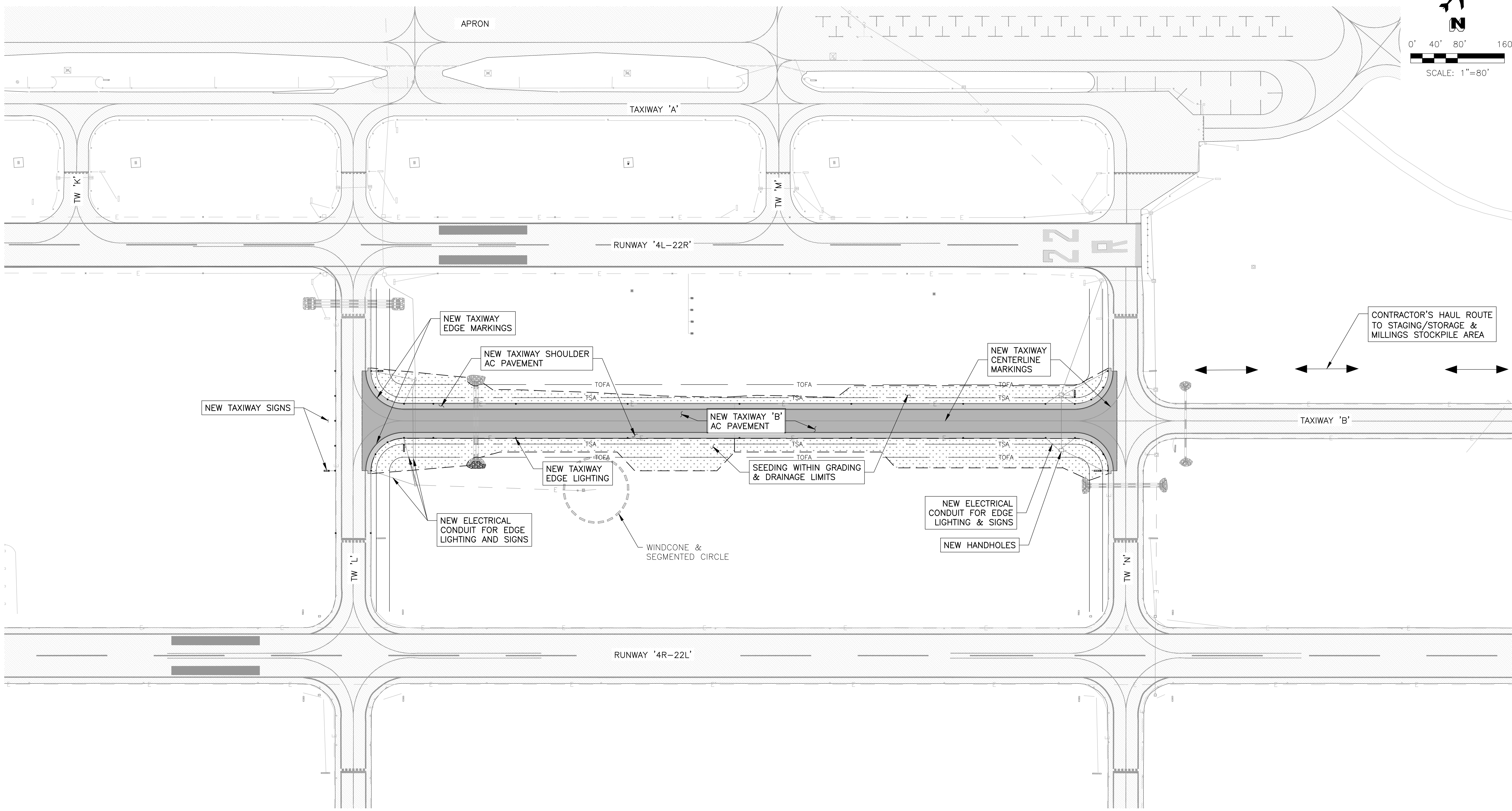
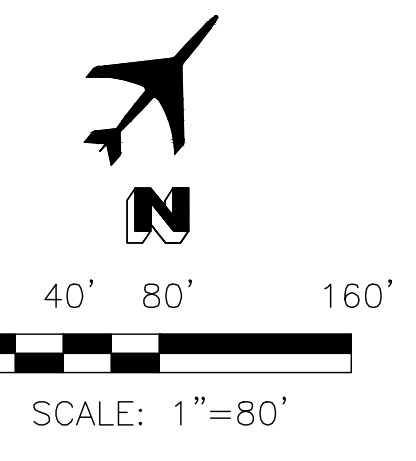
REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24



CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
**TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'**

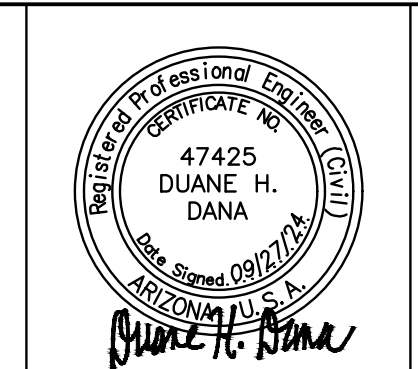
**AIRPORT SITE PLAN**  
 PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-G14.DWG  
 DWG NO.  
**G1.4**  
 SHEET 5 OF 39



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REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24



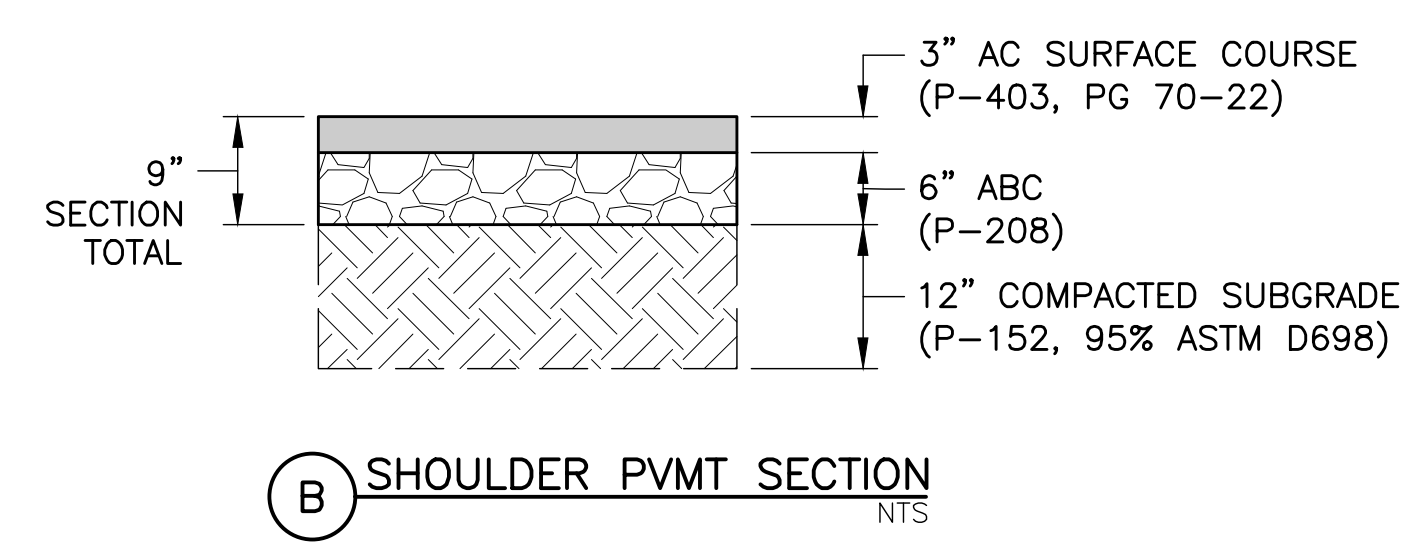
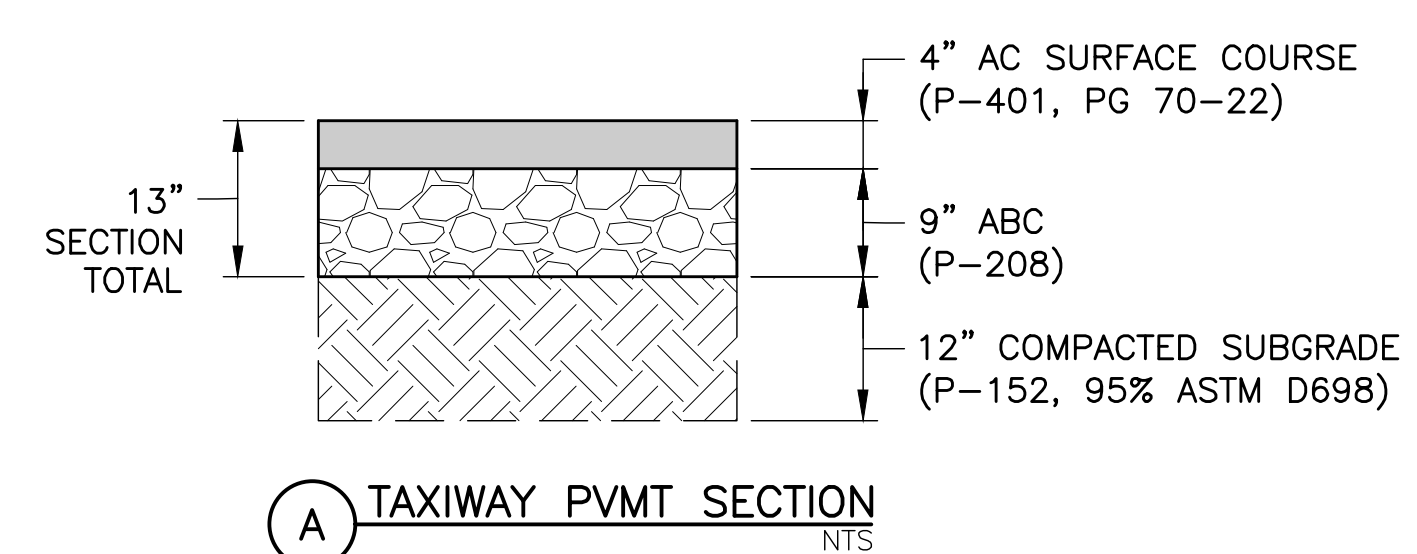
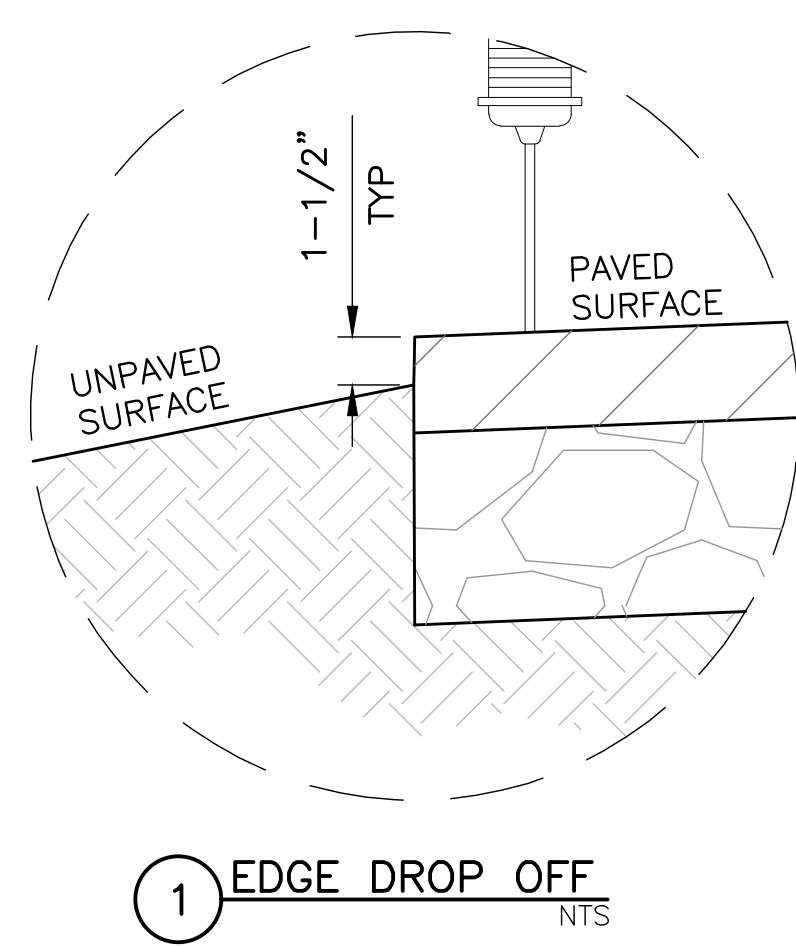
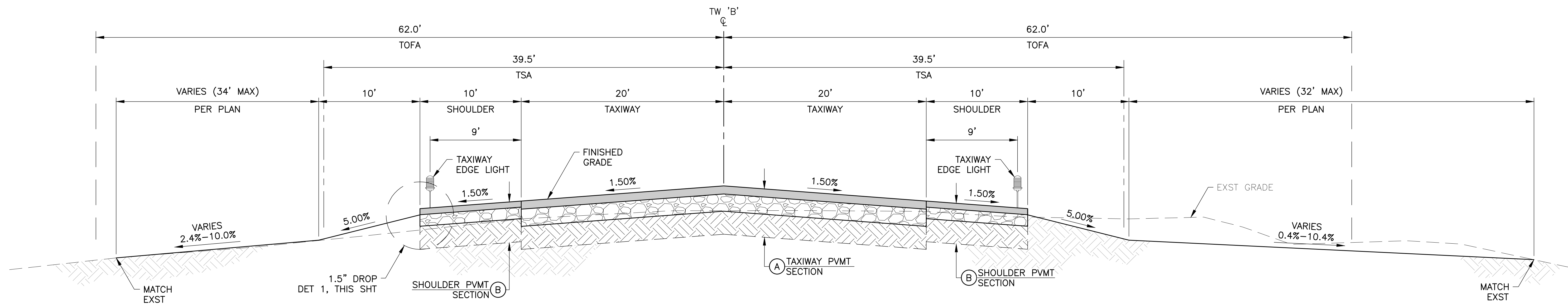
CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

PROJECT SITE PLAN

PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-G15.DWG

DWG NO.  
**G1.5**  
 SHEET 6 OF 39

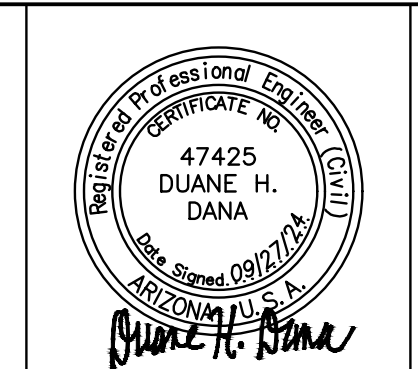




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REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24



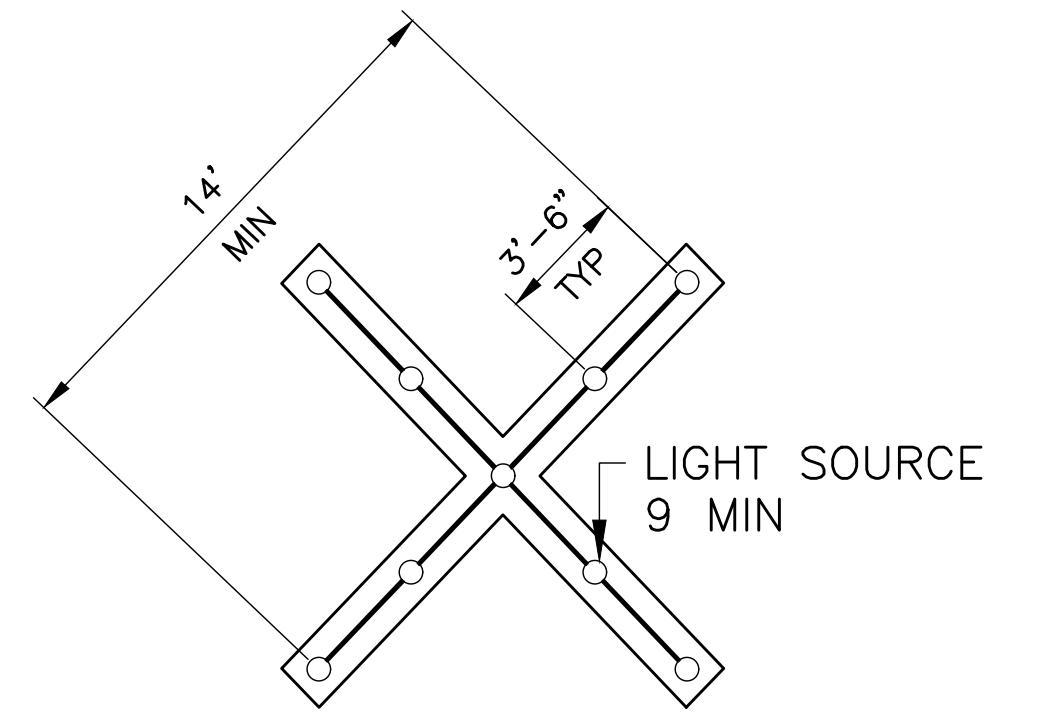
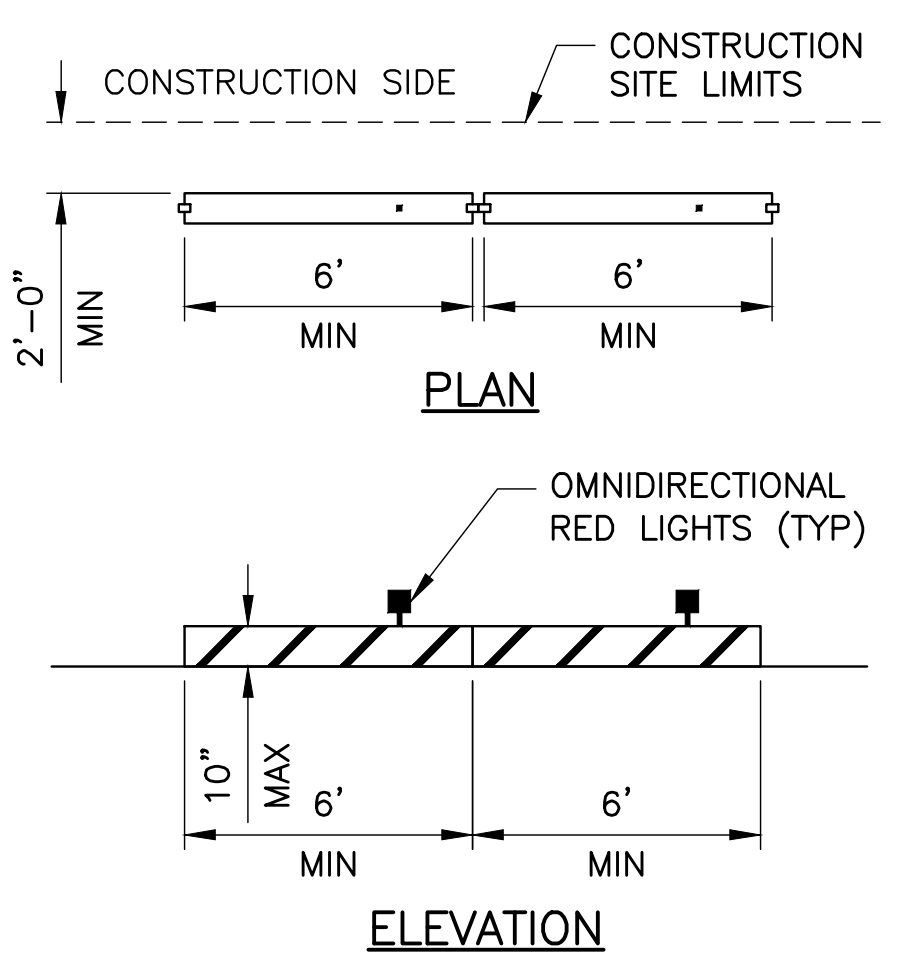
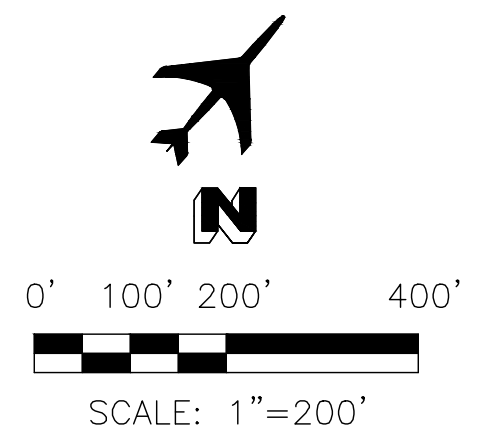
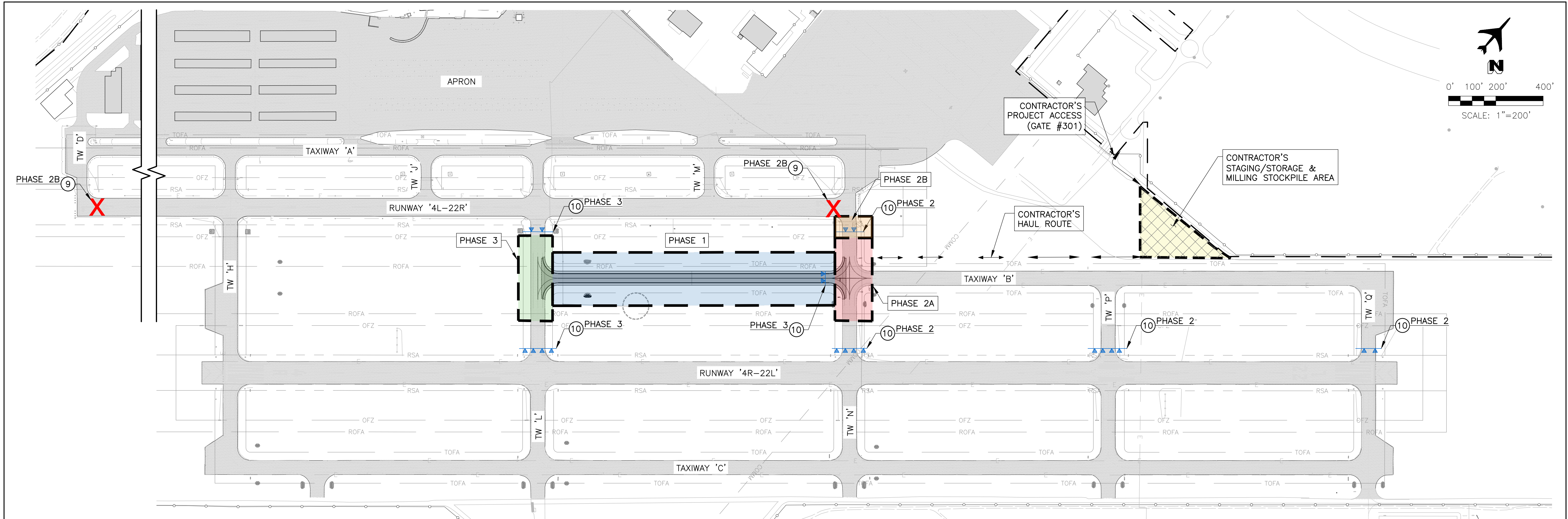
CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

TYPICAL SECTIONS

PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-G21.DWG

DWG NO.  
**G2.1**  
 SHEET 7 OF 39





- TEMPORARY RUNWAY CLOSURE LIGHTED 'X' NOTES:**
- 'X'S SHALL BE LIGHTED PER FAA AC 150/5345-55A, NPI.
  - THE USE AND APPLICATION OF A LIGHTED VISUAL AID FOR RUNWAY CLOSURE IS THE SUBJECT OF AC 150/5340-1, STANDARDS FOR AIRPORT MARKINGS, AND AC 150/5370-2, OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.
  - CONTRACTOR SHALL PROVIDE THE NECESSARY MEANS TO SECURE THE 'X'S IN A MANNER THAT ENSURES AIRPORT SAFETY, (ANCHORING, SAND BAGS, ETC.), NPI.

PROJECT ELEMENTS	DURATION
PHASE 1 CONSTRUCTION OUTSIDE OF TAXIWAY OBJECT FREE AREA. GRADING, PAVING & ELECTRICAL.	42 CALENDAR DAYS
PHASE 2A CONSTRUCTION IMPACTING TAXIWAY 'N' & TAXIWAY 'B' REQUIRING THEIR CLOSURE. GRADING, PAVING & ELECTRICAL.	21 CALENDAR DAYS CONCURRENT W/PHASE 1
PHASE 2B CONSTRUCTION WITHIN THE RW '4L-22R' OFZ REQUIRING NIGHT WORK REQUIRING A RUNWAY CLOSURE. ELECTRICAL.	5 NIGHTS CONCURRENT W/PHASE 2A
PHASE 3 CONSTRUCTION IMPACTING TAXIWAY 'L' REQUIRING ITS CLOSURE. GRADING, PAVING & ELECTRICAL.	21 CALENDAR DAYS CONCURRENT W/PHASE 1 SEQUENTIAL W/PHASE 2
PHASE 4 TEMPORARY MARKINGS	1 CALENDAR DAY SEQUENTIAL W/PHASE 1
PHASE 5 PERMANENT PAVEMENT MARKINGS	1 CALENDAR DAY
STOP TIME (PAVEMENT CURE PERIOD)	30 CALENDAR DAYS
FINAL COMPLETION	5 CALENDAR DAYS
TOTAL DURATION	49 CALENDAR DAYS

LEGEND	CONSTRUCTION NOTES
[Blue dashed box]	PHASE 1
[Red dashed box]	PHASE 2A
[Orange dashed box]	PHASE 2B
[Green dashed box]	PHASE 3
[Yellow hatched box]	CONTRACTOR'S STAGING/STORAGE & MILLINGS STOCKPILE AREA
[Blue triangles]	LOW-PROFILE BARRICADES
[Double arrow]	CONTRACTOR'S HAUL ROUTE
[Red X]	LIGHTED 'X'
[Circle with 9]	LIGHTED X FURNISHED BY AIRPORT MAINTAINED & OPERATED BY CONTRACTOR DET 2, DWG G3.0
[Circle with 10]	LOW PROFILE BARRICADES W/ OMNI-DIRECTIONAL LIGHTS DET 1, DWG G3.0

- PHASING NOTES**
- ALL DURATIONS ARE CALENDAR DAYS.
  - BARRICADE PLACEMENT MAY BE ADJUSTED AT THE DISCRETION OF THE CITY AND/OR AIRPORT STAFF TO ACCOMMODATE SPECIFIC AIRCRAFT MOVEMENT NEEDS.
  - CONTRACTOR SHALL HOLD SAFETY BRIEFINGS WITH AIRPORT STAFF PRIOR TO THE START OF PHASE 1.
  - CONTRACTOR SHALL COORDINATE WITH AIRPORT FOR TAXIWAY LIGHTING & SIGNAGE CIRCUIT LOCKOUT/TAGOUT PROCEDURE.
  - TAXIWAY 'L' & TAXIWAY 'N' MAY NOT BE CLOSED AT THE SAME TIME.
  - TEMPORARY MARKINGS WILL BE PLACED AT 50% RATE W/FINAL PAVEMENT MARKINGS BEING PLACED AT A 100% RATE AND AT LEAST 30 DAYS FOLLOWING PAVING OPERATIONS.
  - NIGHTWORK SHALL OCCUR BETWEEN 9:00PM-6:00AM & SHALL BE COORDINATED W/AIRPORT OPERATIONS.
  - RUNWAY CLOSED DURING NIGHT OPERATIONS WHILE WORK IS TAKING PLACE IN THE ROFZ.

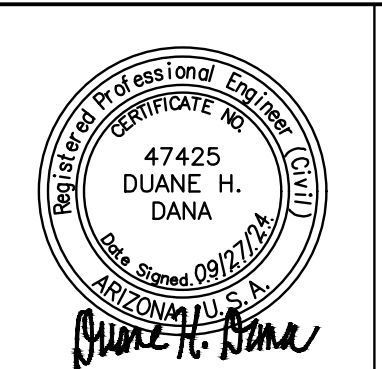
- NOTES:**
- LOW PROFILE BARRICADES AND LIGHTS SHALL BE PROVIDED AND MAINTAINED (DAY AND NIGHT) BY THE CONTRACTOR (NPI).
  - THE OWNER MAY DIRECT THE CONTRACTOR TO RELOCATE ANY BARRICADES AT THEIR DISCRETION.

1 LOW PROFILE BARRICADES  
NO SCALE

2 TEMPORARY RUNWAY CLOSURE LIGHTED 'X' DETAIL  
NPS

REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24

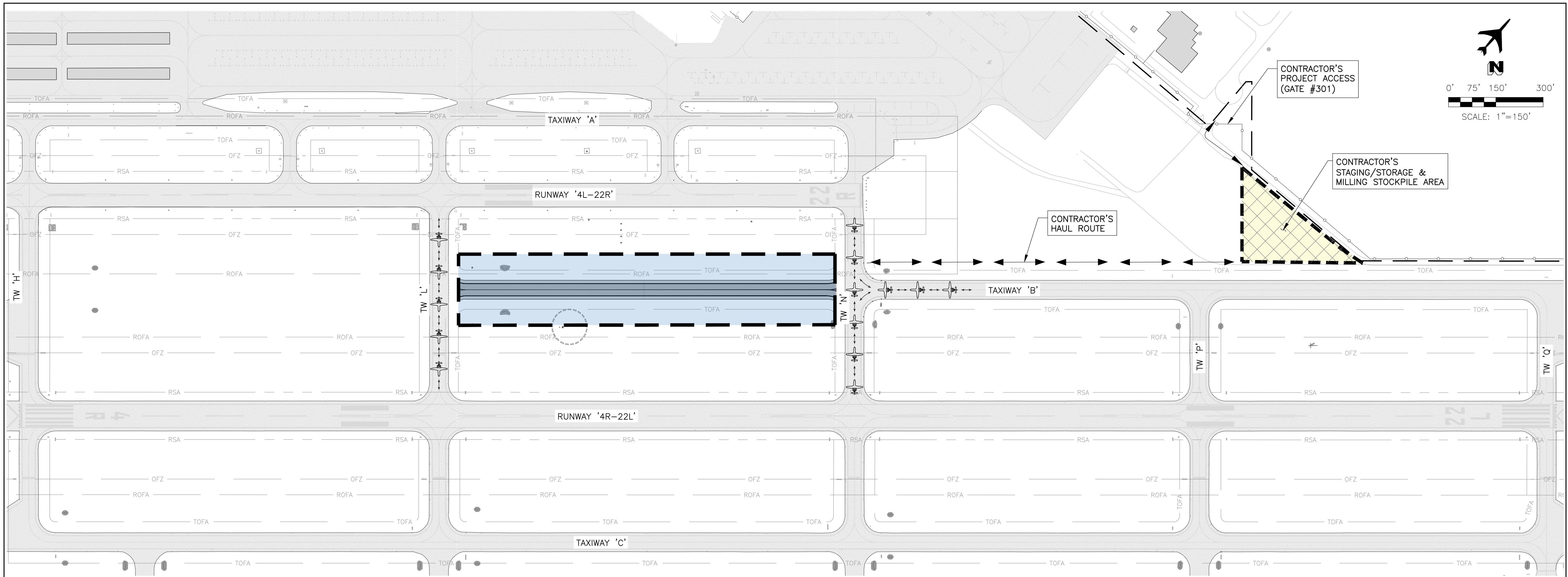


CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

CONSTRUCTION PHASING &  
 BARRICADE PLAN  
 OVERALL

PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-G30.DWG  
 DWG NO.  
**G3.0**  
 SHEET OF 39






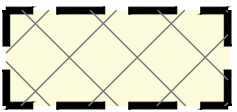
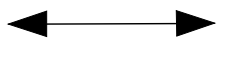

**PROJECT ELEMENTS**

PHASE 1  
CONSTRUCTION OUTSIDE OF TAXIWAY  
OBJECT FREE AREA. GRADING, PAVING  
& ELECTRICAL

**DURATION**

42 CALENDAR DAYS

**LEGEND**

-  PHASE 1
-  CONTRACTOR'S STAGING/STORAGE & MILLINGS STOCKPILE AREA
-  CONTRACTOR'S HAUL ROUTE
-  AIRCRAFT ROUTE DURING CONSTRUCTION PHASE

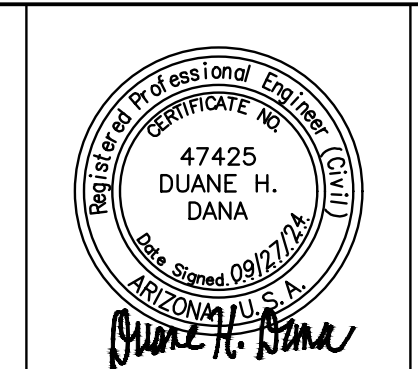
**PHASING NOTES**

1. ALL DURATIONS ARE CALENDAR DAYS.
2. BARRICADE PLACEMENT MAY BE ADJUSTED AT THE DISCRETION OF THE CITY AND/OR AIRPORT STAFF TO ACCOMMODATE SPECIFIC AIRCRAFT MOVEMENT NEEDS.
3. CONTRACTOR SHALL HOLD SAFETY BRIEFINGS WITH AIRPORT STAFF PRIOR TO THE START OF PHASE 1.
4. CONTRACTOR SHALL COORDINATE WITH AIRPORT FOR TAXIWAY LIGHTING & SIGNAGE CIRCUIT LOCKOUT/TAGOUT PROCEDURE.
5. TAXIWAY 'L' & TAXIWAY 'N' MAY NOT BE CLOSED AT THE SAME TIME.

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REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24

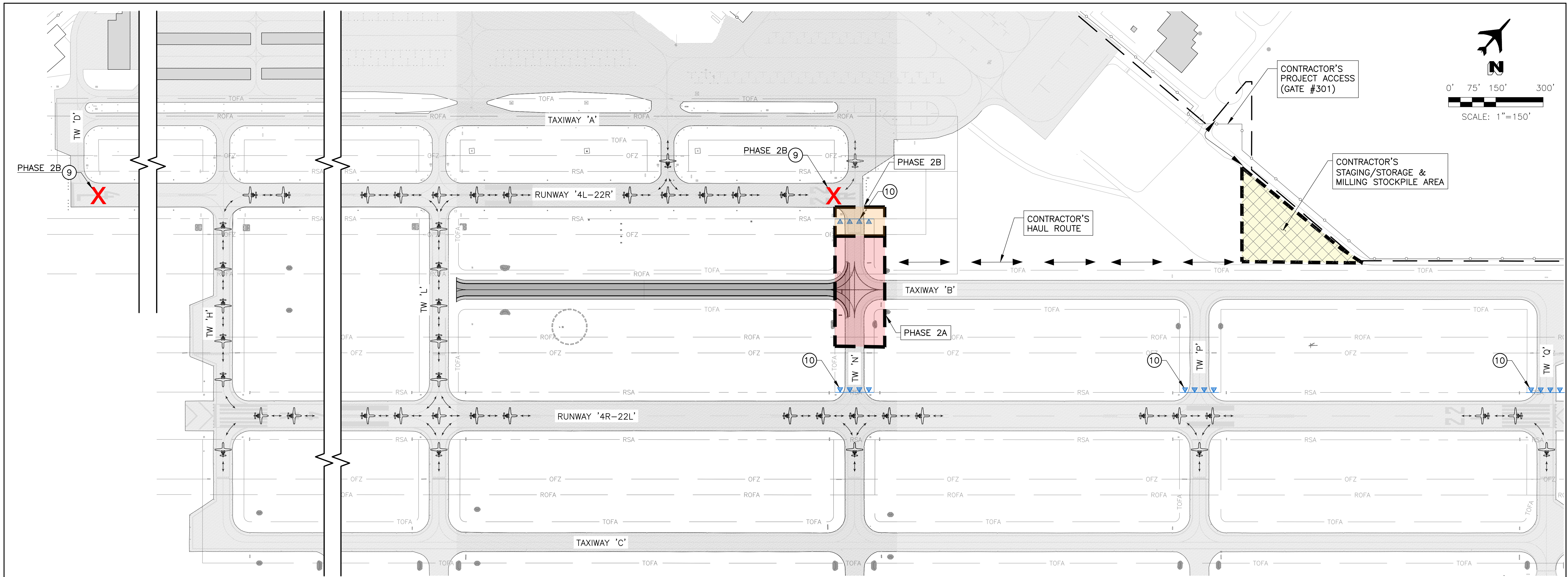


CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
**TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'**

**CONSTRUCTION PHASING &  
 BARRICADE PLAN  
 PHASE 1**

PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-031.DWG  
 DWG NO.  
**G3.1**  
 SHEET OF 39





- |   |   |     |
|---|---|-----|
| ○ | <b>CONSTRUCTION NOTES</b>   | ○   |
| ⑨ | LIGHTED X<br>FURNISHED BY AIRPORT<br>MAINTAINED & OPERATED BY CONTRACTOR<br>DET 2, DWG G3.0 | NPI |
| ⑩ | LOW PROFILE BARRICADES W/<br>OMNI-DIRECTIONAL LIGHTS<br>DET 1, DWG G3.0                     | NPI |

PROJECT ELEMENTS	DURATION
PHASE 2A CONSTRUCTION IMPACTING TAXIWAY 'N' & TAXIWAY 'B' REQUIRING THEIR CLOSURE. GRADING, PAVING & ELECTRICAL.	21 CALENDAR DAYS CONCURRENT W/PHASE 1
PHASE 2B CONSTRUCTION WITHIN THE RW '4L-22R' OFZ REQUIRING NIGHT WORK REQUIRING A RUNWAY CLOSURE. ELECTRICAL.	5 NIGHTS CONCURRENT W/PHASE 2A

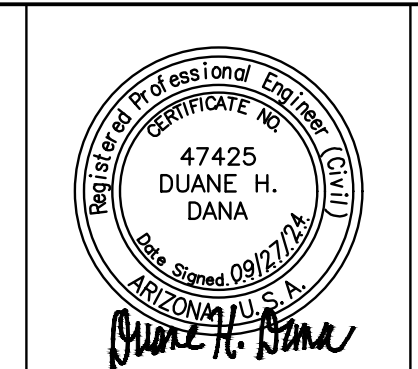
- |  |  |
|--|--|
|  | PHASE 2A   |
|  | PHASE 2B   |
|  | CONTRACTOR'S<br>STAGING/STORAGE &<br>MILLINGS STOCKPILE AREA |
|  | LOW-PROFILE BARRICADES                                       |
|  | CONTRACTOR'S HAUL ROUTE                                      |
|  | AIRCRAFT ROUTE DURING<br>CONSTRUCTION PHASE                  |
|  | LIGHTED 'X'  |

- |  |
|--|
| <b>PHASING NOTES</b>   |
| 1. ALL DURATIONS ARE CALENDAR DAYS.  |
| 2. BARRICADE PLACEMENT MAY BE ADJUSTED AT THE DISCRETION OF THE CITY AND/OR AIRPORT STAFF TO ACCOMMODATE SPECIFIC AIRCRAFT MOVEMENT NEEDS. |
| 3. CONTRACTOR SHALL COORDINATE WITH AIRPORT FOR TAXIWAY LIGHTING & SIGNAGE CIRCUIT LOCKOUT/TAGOUT PROCEDURE.                               |
| 4. NIGHTWORK SHALL OCCUR BETWEEN 9:00PM-6:00AM & SHALL BE COORDINATED W/AIRPORT OPERATIONS.  |
| 5. RUNWAY CLOSED DURING NIGHT OPERATIONS WHILE WORK IS TAKING PLACE IN THE ROFZ.   |

J:\2021\1021015.11\_CHD\_TW\_B\_IMPROVEMENTS\_PHASE\_1\CAD\21015\_11-G32.DWG Sep. 27, 2024 11:17 AM DARRIN OAKLEY

REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24

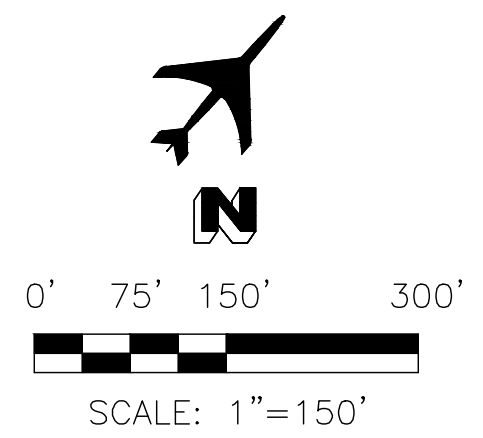
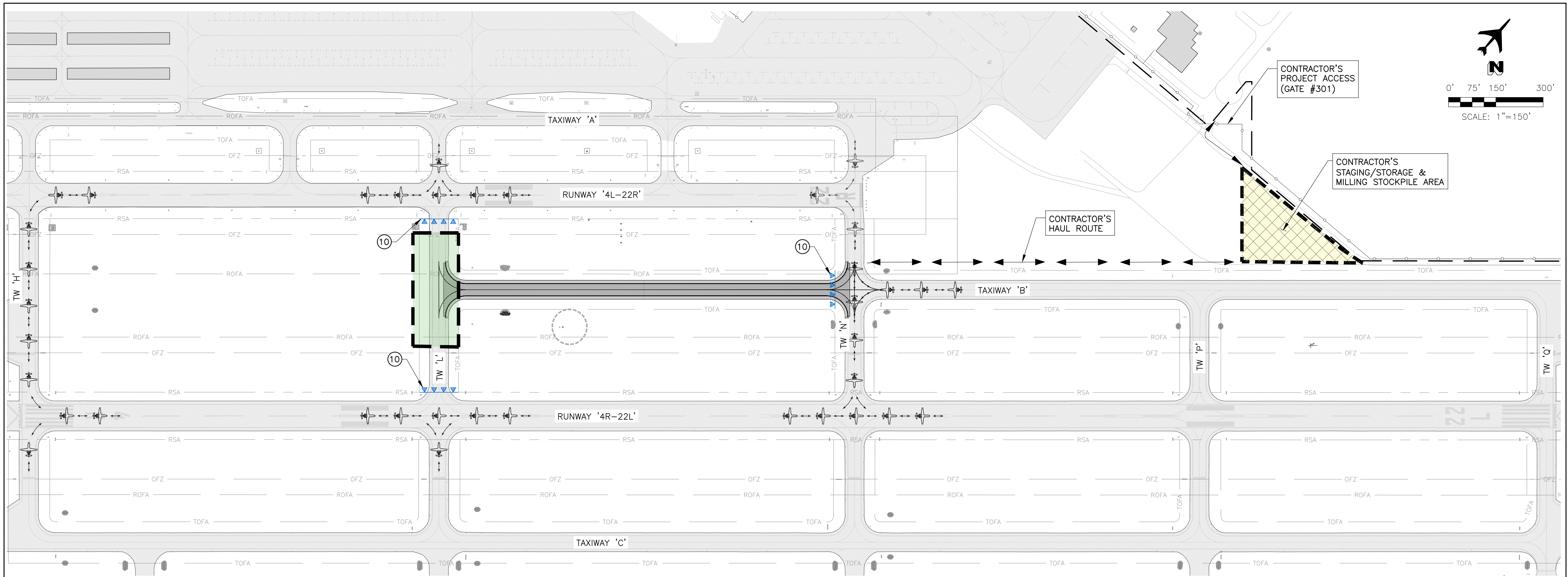


CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
**TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'**

**CONSTRUCTION PHASING &  
 BARRICADE PLAN  
 PHASE 2**

PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-G32.DWG  
 DWG NO.  
**G3.2**  
 SHEET 10 OF 39





**CONSTRUCTION NOTES**

⑩ LOW PROFILE BARRICADES W/ OMNI-DIRECTIONAL LIGHTS DET 1, DWG G3.0

<b>PROJECT ELEMENTS</b>	<b>DURATION</b>
PHASE 3 CONSTRUCTION IMPACTING TAXIWAY 'L' REQUIRING ITS CLOSURE. GRADING, PAVING & ELECTRICAL.	21 CALENDAR DAYS CONCURRENT W/PHASE 1 SEQUENTIAL W/PHASE 2

**LEGEND**

- PHASE 3
- CONTRACTOR'S STAGING/STORAGE & MILLINGS STOCKPILE AREA
- LOW-PROFILE BARRICADES
- CONTRACTOR'S HAUL ROUTE
- AIRCRAFT ROUTE DURING CONSTRUCTION PHASE

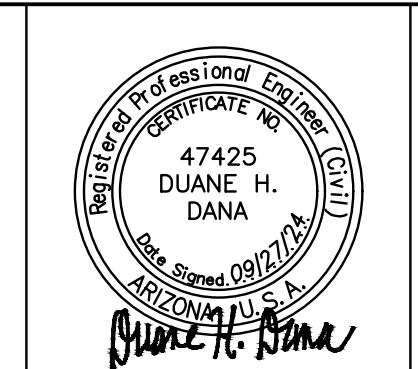
**PHASING NOTES**

- ALL DURATIONS ARE CALENDAR DAYS.
- BARRICADE PLACEMENT MAY BE ADJUSTED AT THE DISCRETION OF THE CITY AND/OR AIRPORT STAFF TO ACCOMMODATE SPECIFIC AIRCRAFT MOVEMENT NEEDS.
- CONTRACTOR SHALL COORDINATE WITH AIRPORT FOR TAXIWAY LIGHTING & SIGNAGE CIRCUIT LOCKOUT/TAGOUT PROCEDURE.

J:\2021\1021015.11\_CHD\_TW\_B\_IMPROVEMENTS\_PHASE1\CAD\21015\_11-G33.DWG Sep. 27, 2024 11:17 AM DIRIN, OAKLEY

REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24



CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
**TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'**

**CONSTRUCTION PHASING &  
 BARRICADE PLAN  
 PHASE 3**

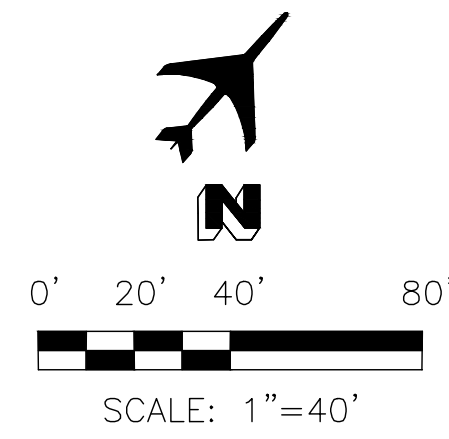
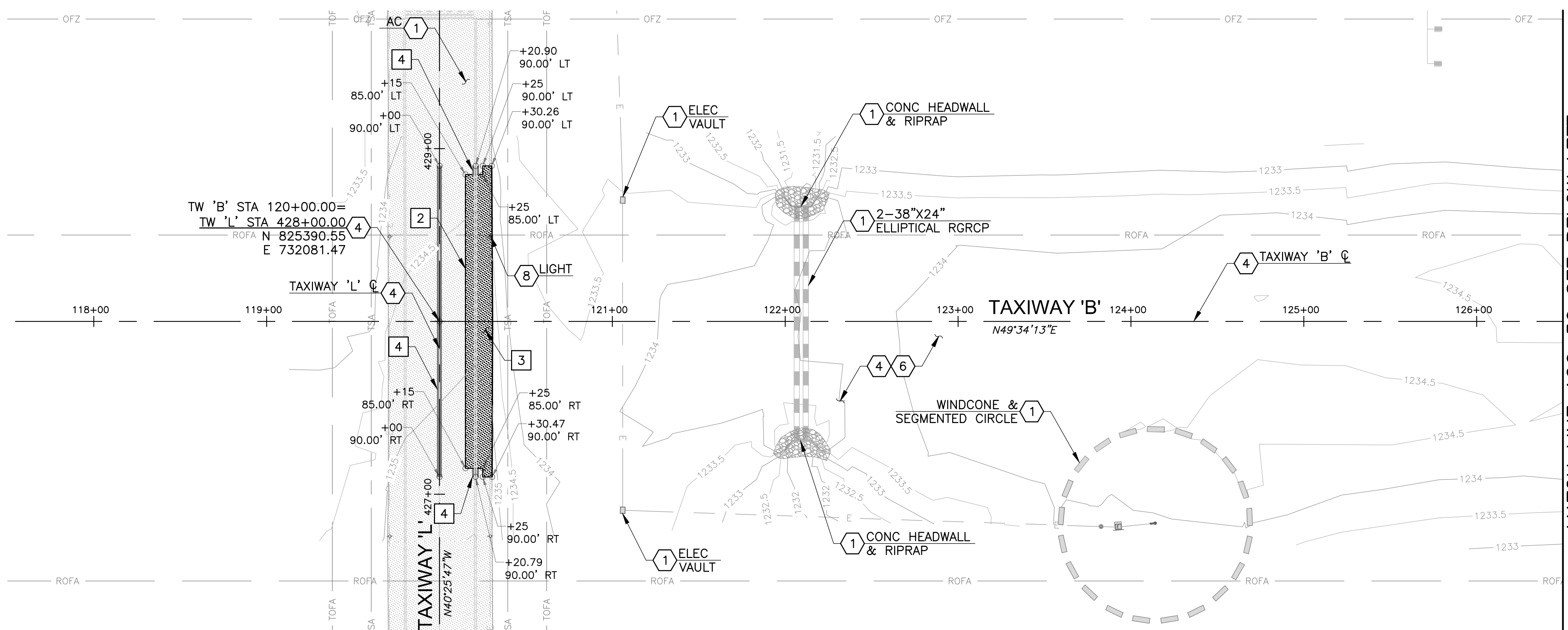
PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-G33.DWG  
**APPENDIX  
 A-3**



J:\2021\1021015.11\_Chd\_Tw\_B\_Improvements\_Phase\_1\CAD\21015\_11-CD1X.DWG Sep. 27, 2024 11:18 AM DARIN.DAWELEY

MATCH LINE 126+50 SEE ABOVE RIGHT

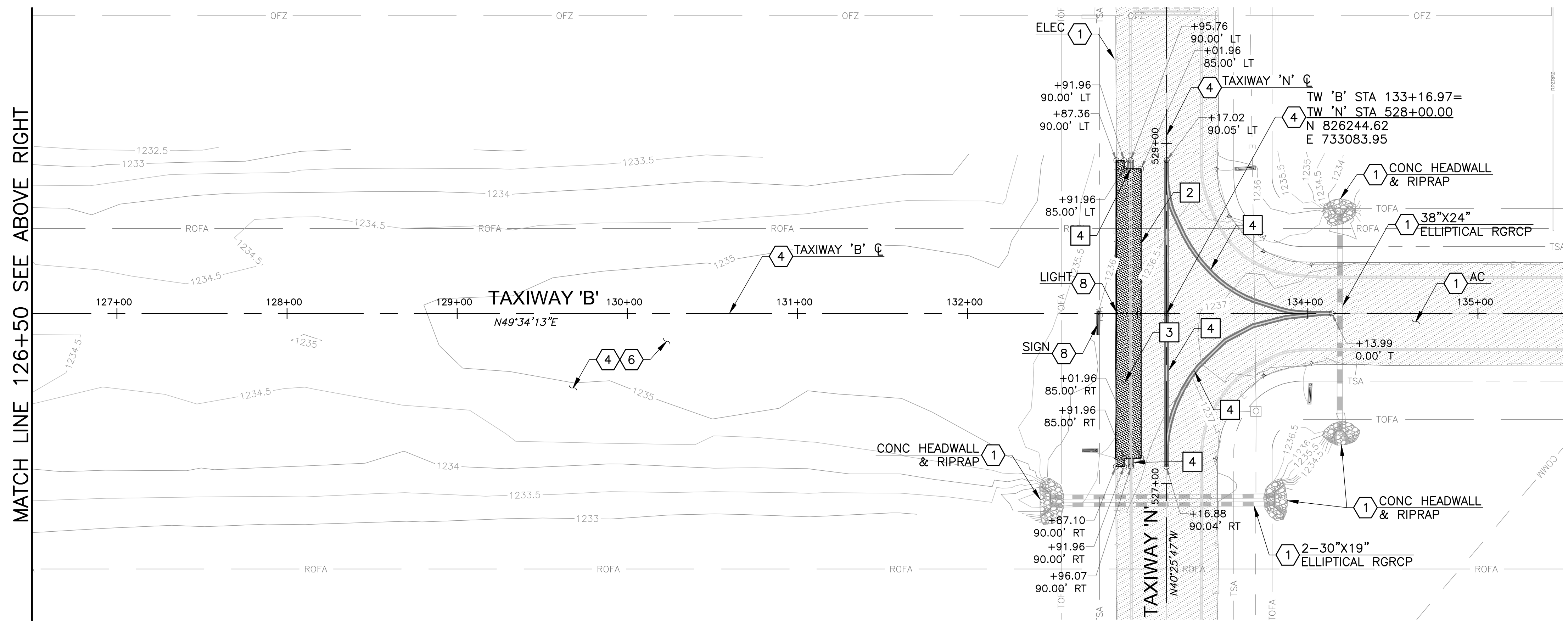
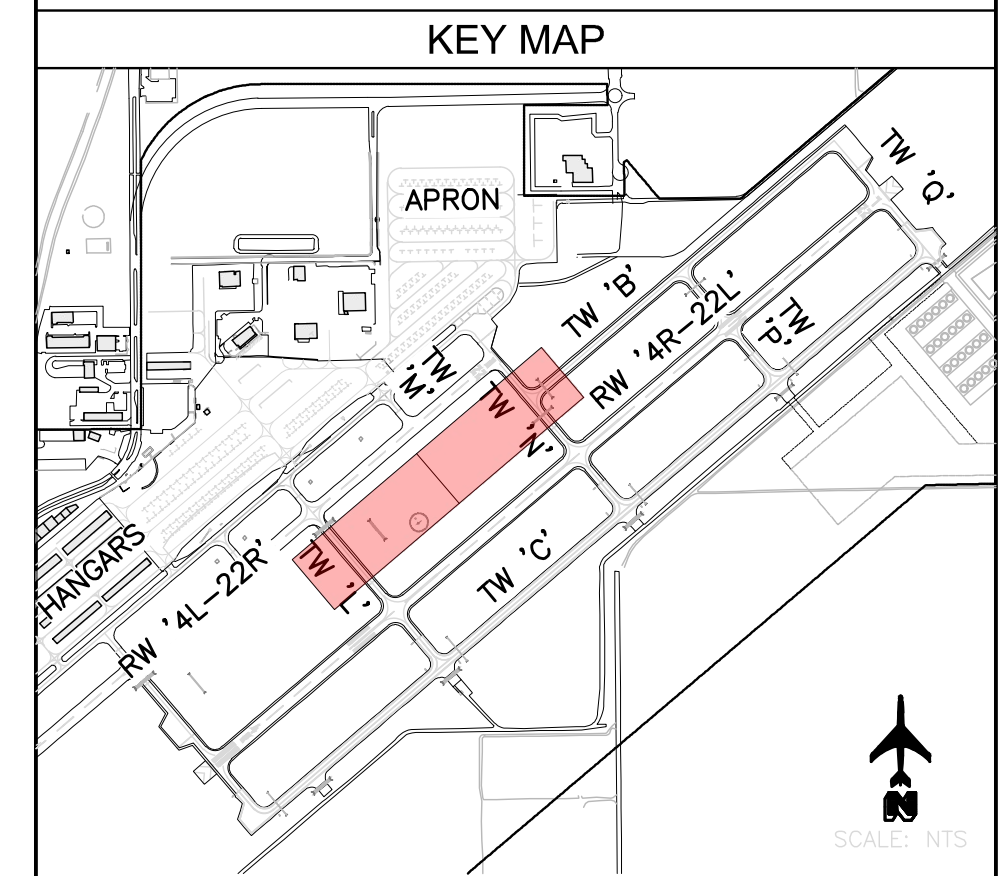
MATCH LINE 126+50 SEE BELOW LEFT



REMOVAL NOTES		
2	SAWCUT AC PAVEMENT (FULL DEPTH, ±4")	421 LF
3	REMOVE AC PAVEMENT (FULL DEPTH, ±4")	583 SY
4	OBLITERATE & SEAL PAVEMENT MARKINGS	1,583 SF

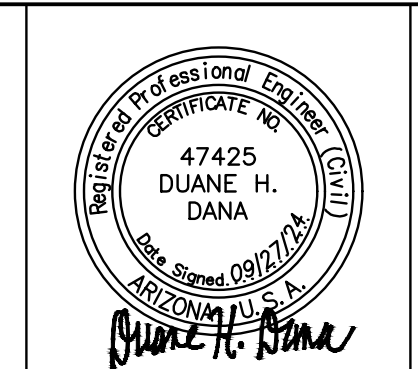
REFERENCE NOTES	
1	PROTECT IN PLACE
4	REFER TO DWG CP1.1-CP1.2 FOR GEOMETRIC CONTROL PLANS
6	REFER TO DWG CG1.1 FOR GRADING & DRAINAGE PLAN
8	REFER TO DWG E2.0-E2.1 FOR ELECTRICAL DEMOLITION PLANS

LEGEND	
	EXISTING AC PAVEMENT
	AC PAVEMENT REMOVAL



REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24

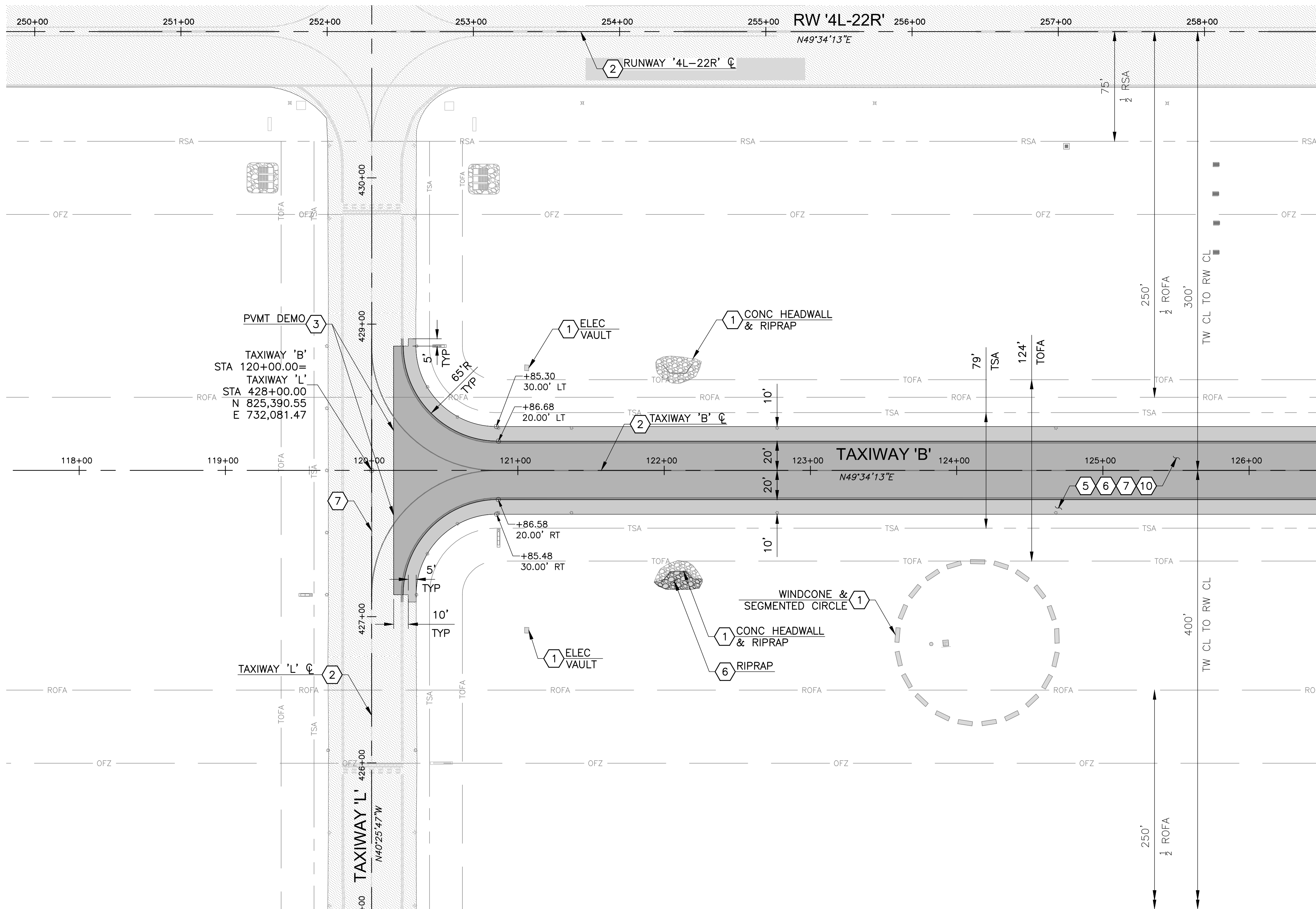


CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

DEMOLITION PLAN  
 STA 120+00 TO STA 135+00  
 DWG NO.  
**CD1.1**  
 SHEET 12 OF 39

PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-CD1X.DWG  
 DWG NO.  
**CD1.1**  
 SHEET 12 OF 39

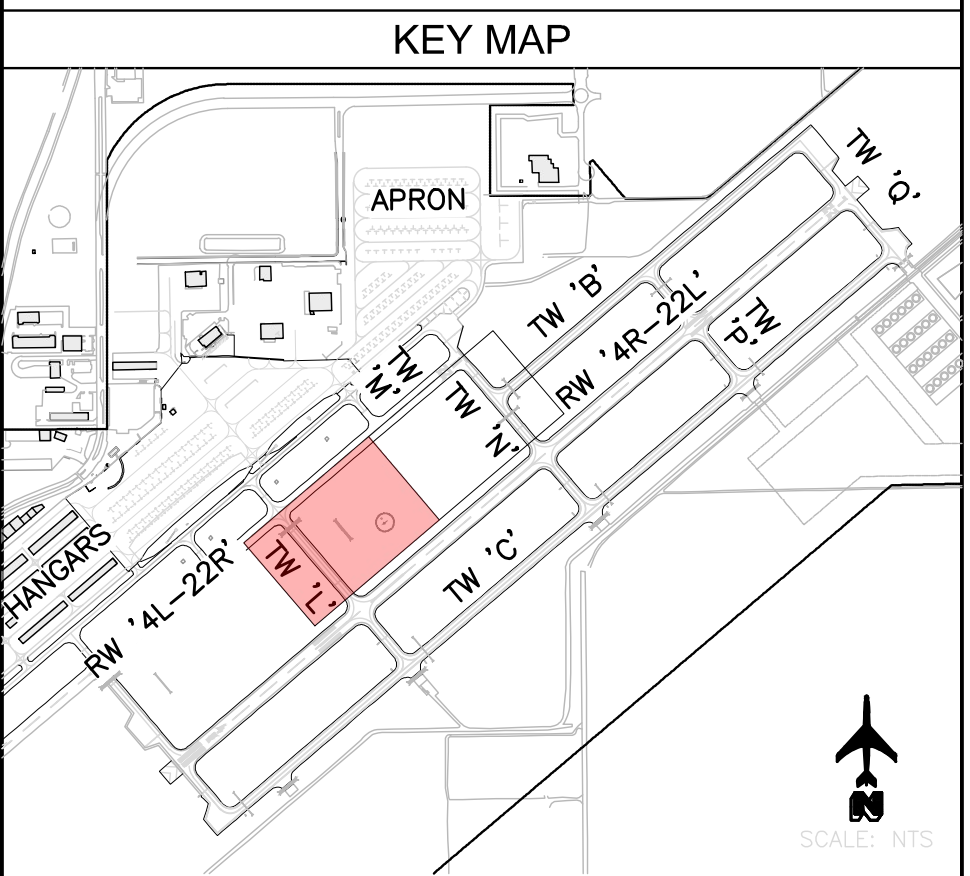
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MATCH LINE 126+50 SEE SHEET CP1.2

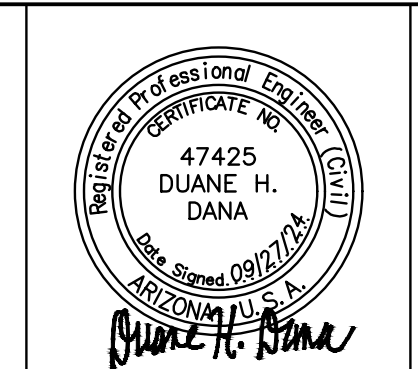
- REFERENCE NOTES**
- 1 PROTECT IN PLACE
  - 2 REFER TO G1.3 FOR SURVEY CONTROL PLAN
  - 3 REFER TO DWG CD1.1 FOR DEMOLITION PLANS
  - 5 REFER TO DWG CP2.1-CP2.2 FOR TAXIWAY PLAN & PROFILE
  - 6 REFER TO DWG CG1.1 FOR GRADING & DRAINAGE PLAN
  - 7 REFER TO DWG CM1.1 FOR PAVEMENT MARKING PLAN
  - 10 REFER TO DWG E3.0-E3.1 FOR AIRFIELD ELECTRICAL PLANS

- LEGEND**
- AC TAXIWAY PAVEMENT  
SECT A, DWG G2.1
  - AC TAXIWAY SHOULDER PAVEMENT  
SECT B, DWG G2.1
  - EXISTING AC PAVEMENT



REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24



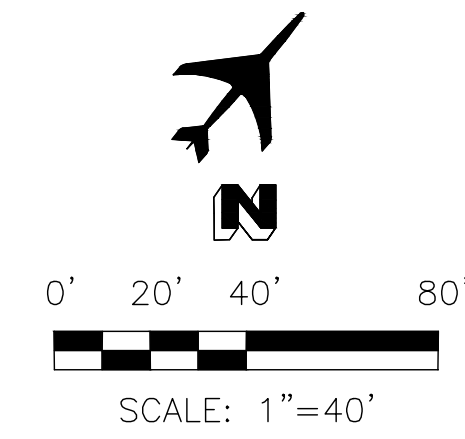
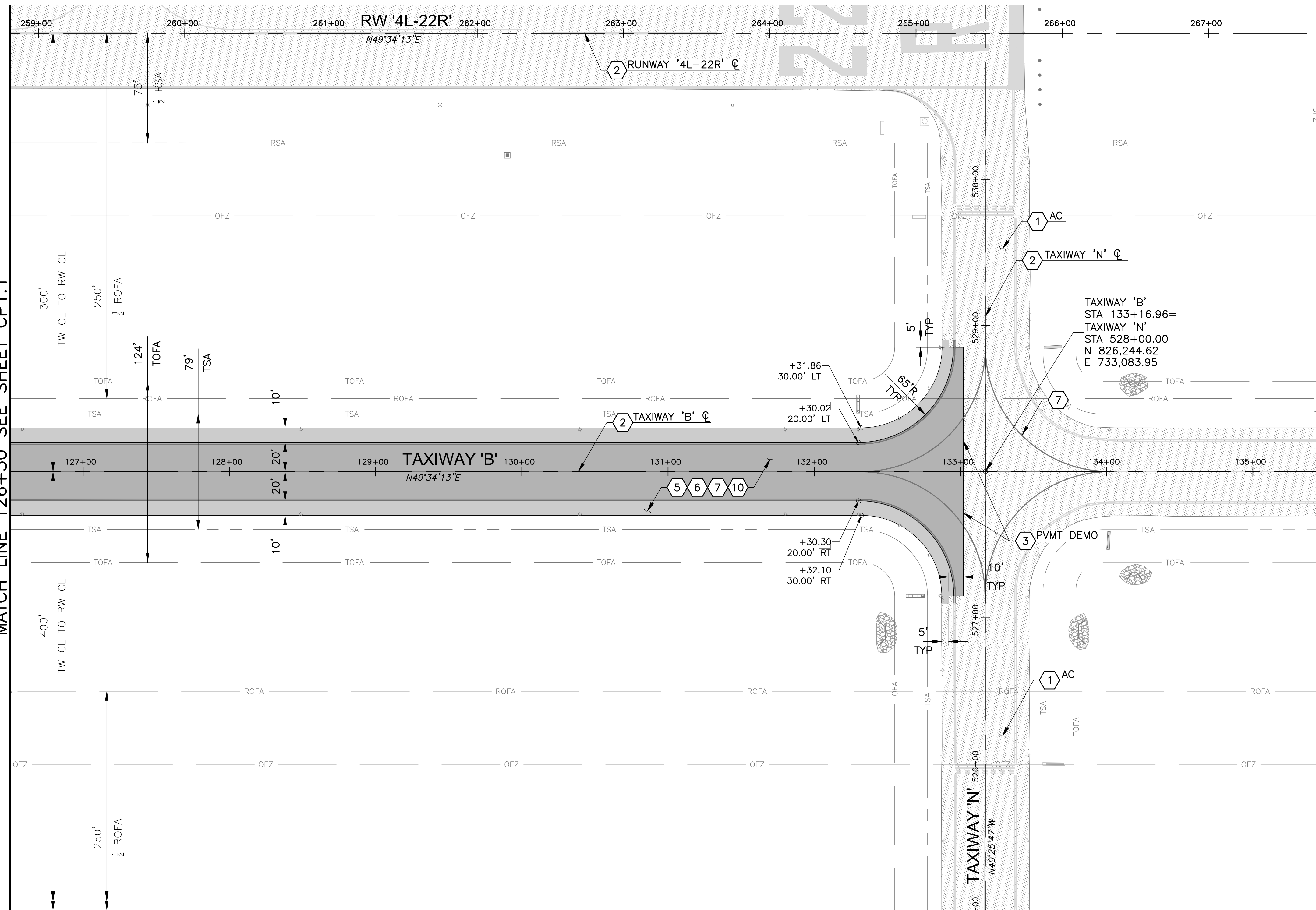
CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
**TAXIWAY 'B' IMPROVEMENTS PHASE 1:**  
**TAXIWAY 'L' TO TAXIWAY 'N'**

**GEOMETRIC CONTROL PLAN**  
 STA 120+00 TO STA 126+50  
**CP1.1**  
 SHEET 13 OF 39

PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-CP1X.DWG  
 DWG NO.  
**CP1.1**  
 SHEET 13 OF 39

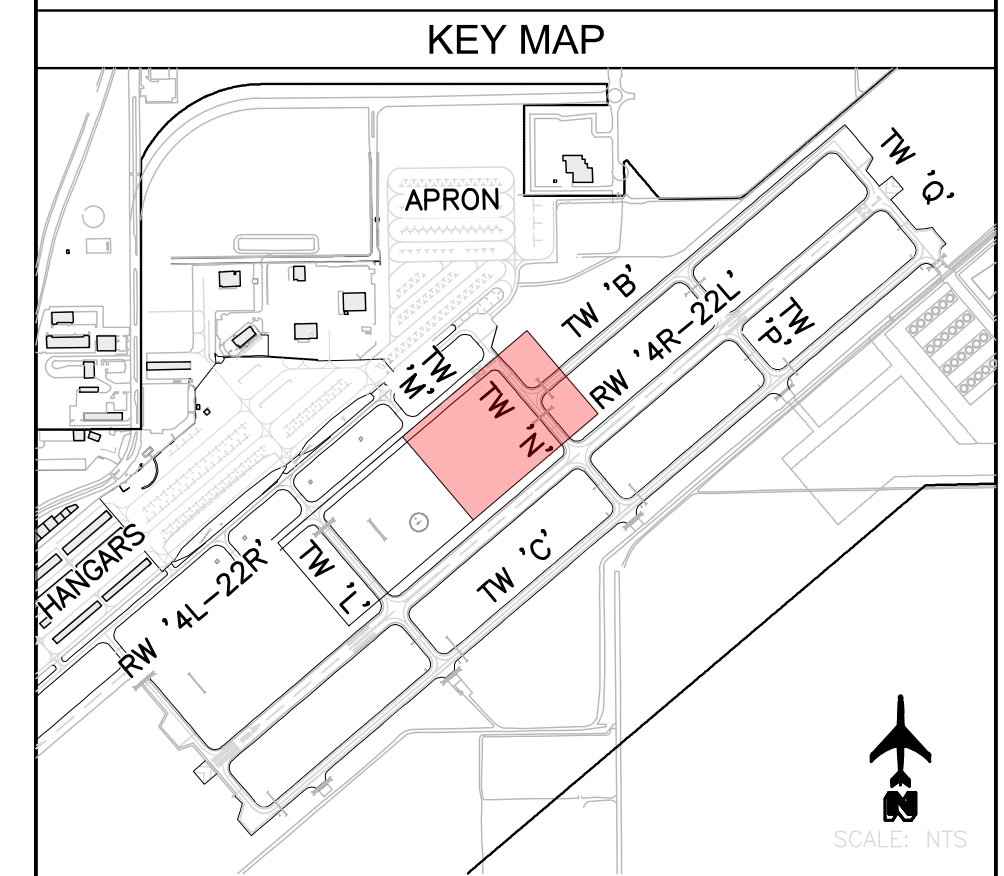
J:\2021\1021015.11\_CHD\_TW\_B\_IMPROVEMENTS\_PHASE1\CAD\21015\_11-CP1X.DWG Sep. 27, 2024 11:19 AM DARIN.DAWELEY

MATCH LINE 126+50 SEE SHEET CP1.1



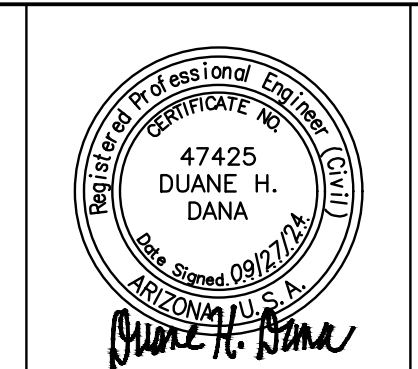
- REFERENCE NOTES
- 1 PROTECT IN PLACE
  - 2 REFER TO G1.3 FOR SURVEY CONTROL PLAN
  - 3 REFER TO DWG CD.1.1 FOR DEMOLITION PLANS
  - 5 REFER TO DWG CP2.1-CP2.2 FOR TAXIWAY PLAN & PROFILE
  - 6 REFER TO DWG CG.1.1 FOR GRADING & DRAINAGE PLAN
  - 7 REFER TO DWG CM.1.1 FOR PAVEMENT MARKING PLAN
  - 10 REFER TO DWG E3.0-E3.1 FOR AIRFIELD ELECTRICAL PLANS

- LEGEND
- AC TAXIWAY PAVEMENT SECT A, DWG G2.1
  - AC TAXIWAY SHOULDER PAVEMENT SECT B, DWG G2.1
  - EXISTING AC PAVEMENT



REV. NO.	DATE	DRWN	CHKD	REMARKS

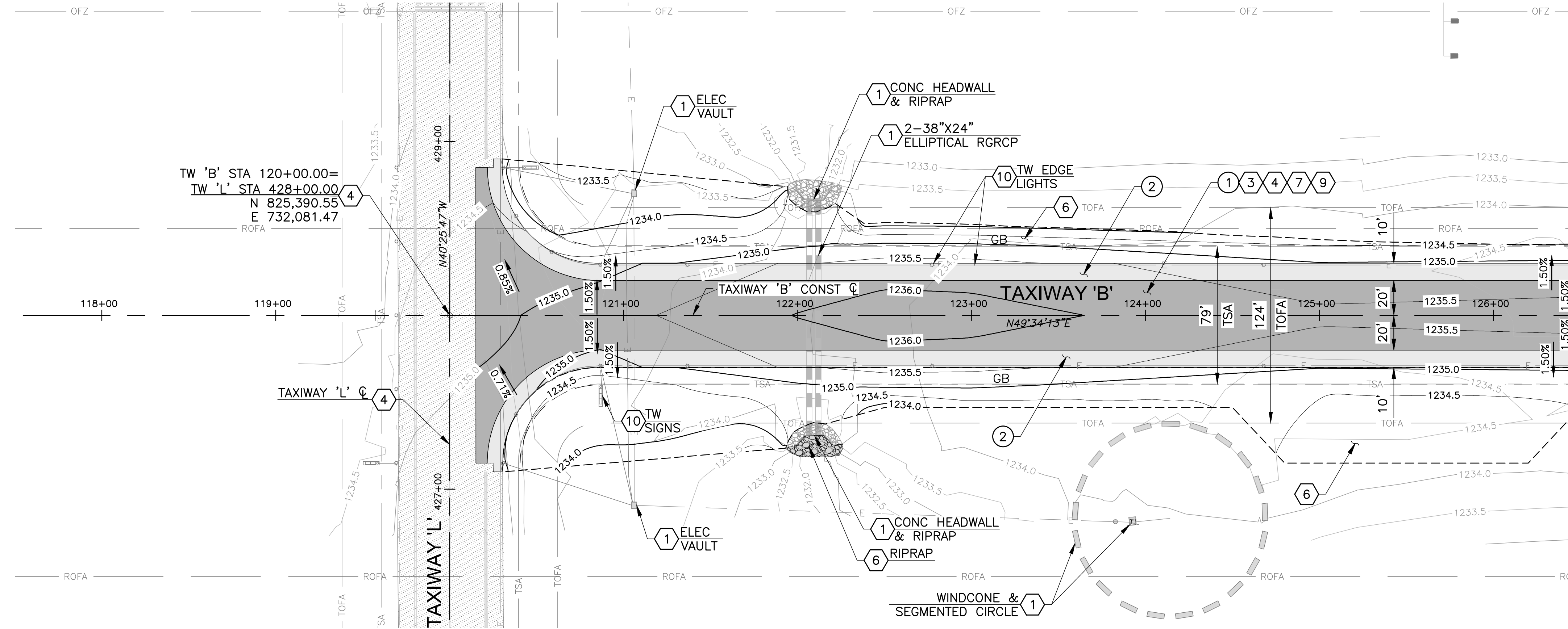
DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24



CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

GEOMETRIC CONTROL PLAN  
 STA 126+50 TO STA 135+00  
 DWG NO.  
**CP1.2**  
 SHEET 14 OF 39

PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-CP1X.DWG  
 DWG NO.  
**CP1.2**  
 SHEET 14 OF 39



MATCH LINE 126+50 SEE SHEET CP2.2

**CONSTRUCTION NOTES**

- ① AC TAXIWAY PAVEMENT 3,120 SY  
SECT A, DWG G2.1
- ② AC TAXIWAY SHOULDER PAVEMENT 1,453 SY  
SECT B, DWG G2.1

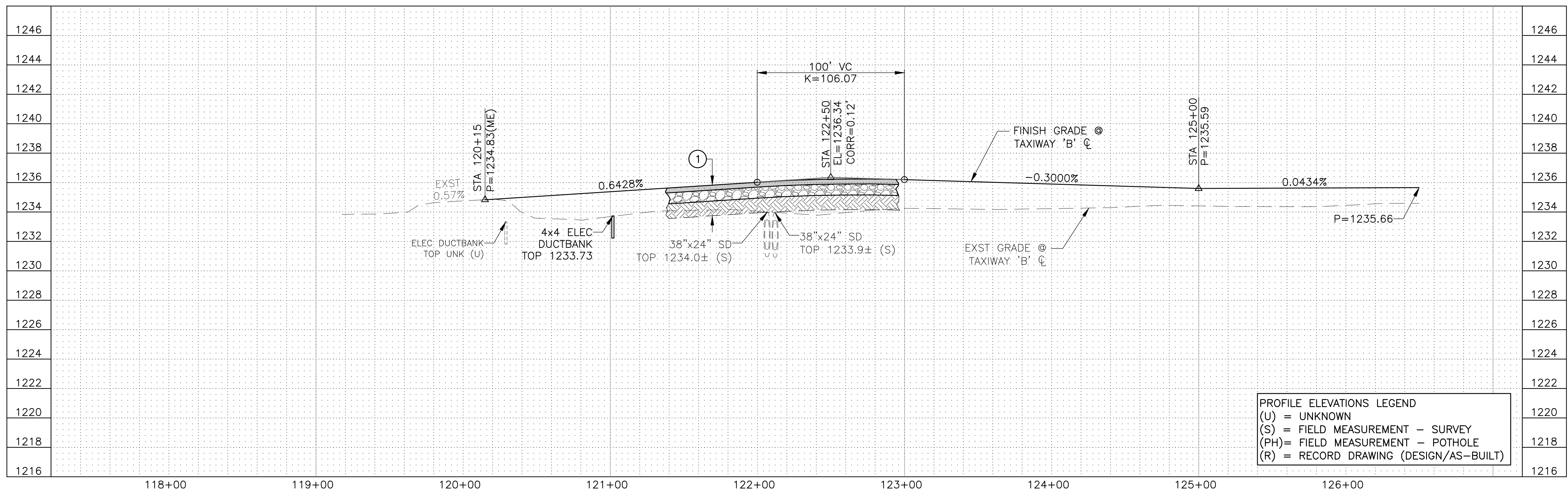
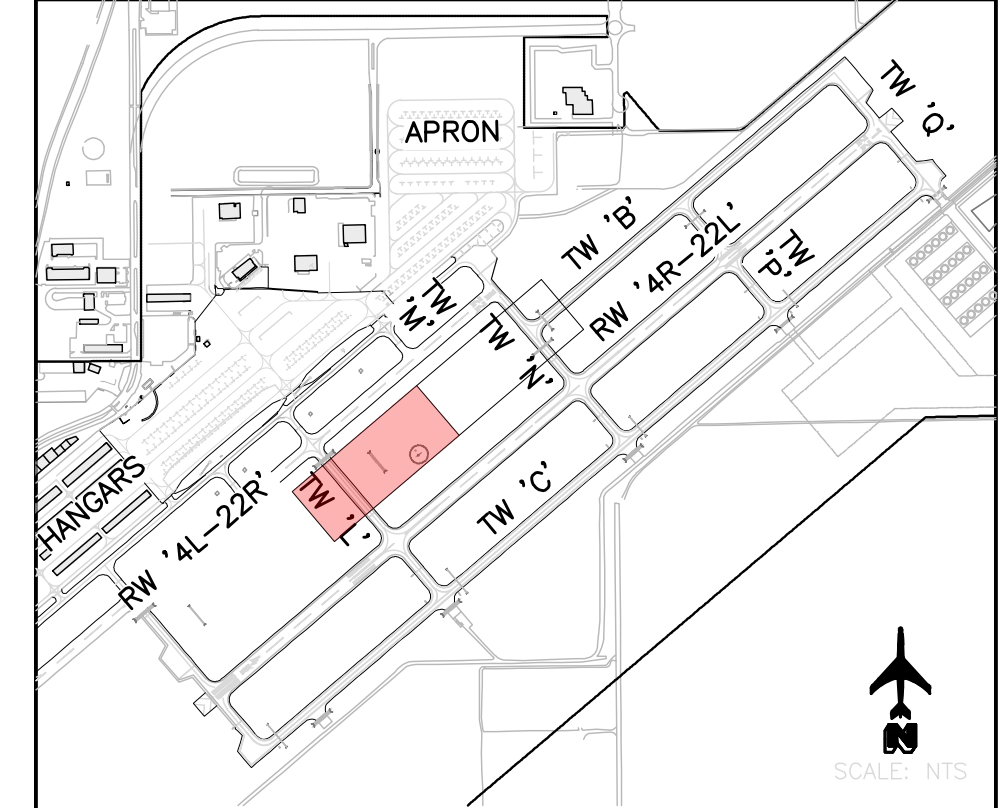
**REFERENCE NOTES**

- ① PROTECT IN PLACE
- ③ REFER TO DWG CD1.1 FOR DEMOLITION PLANS
- ④ REFER TO DWG CP1.1-CP1.2 FOR GEOMETRIC CONTROL PLANS
- ⑥ REFER TO DWG CG1.1 FOR GRADING & DRAINAGE PLAN
- ⑦ REFER TO DWG CM1.1 FOR PAVEMENT MARKING PLAN
- ⑨ REFER TO DWG CP3.1 FOR PAVEMENT ELEVATION PLAN
- ⑩ REFER TO DWG E3.0-E3.1 FOR AIRFIELD ELECTRICAL PLANS

**LEGEND**

- AC TAXIWAY PAVEMENT  
SECT A, DWG G2.1
- AC TAXIWAY SHOULDER PAVEMENT  
SECT B, DWG G2.1
- EXISTING AC PAVEMENT
- LIMITS OF GRADING
- GB — GRADE BREAK

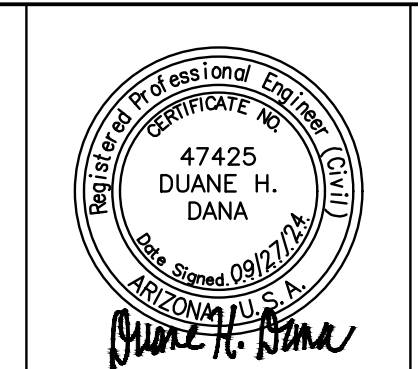
**KEY MAP**



J:\2021\1021015.11\_CND\_TW\_E\_IMPROVEMENTS\_PHASE1\CAD\21015\_11-CP2X.DWG Sep. 27, 2024 11:19 AM DARIN.DAWELEY

REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24

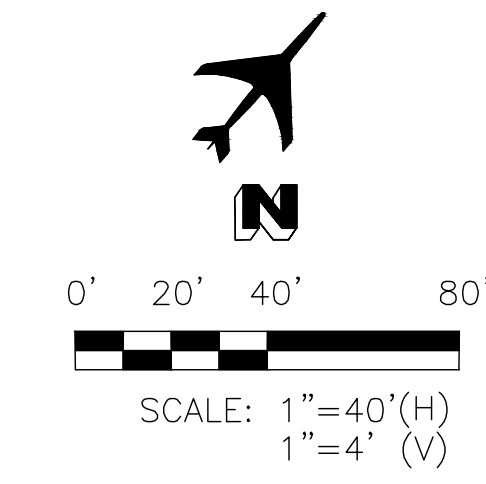
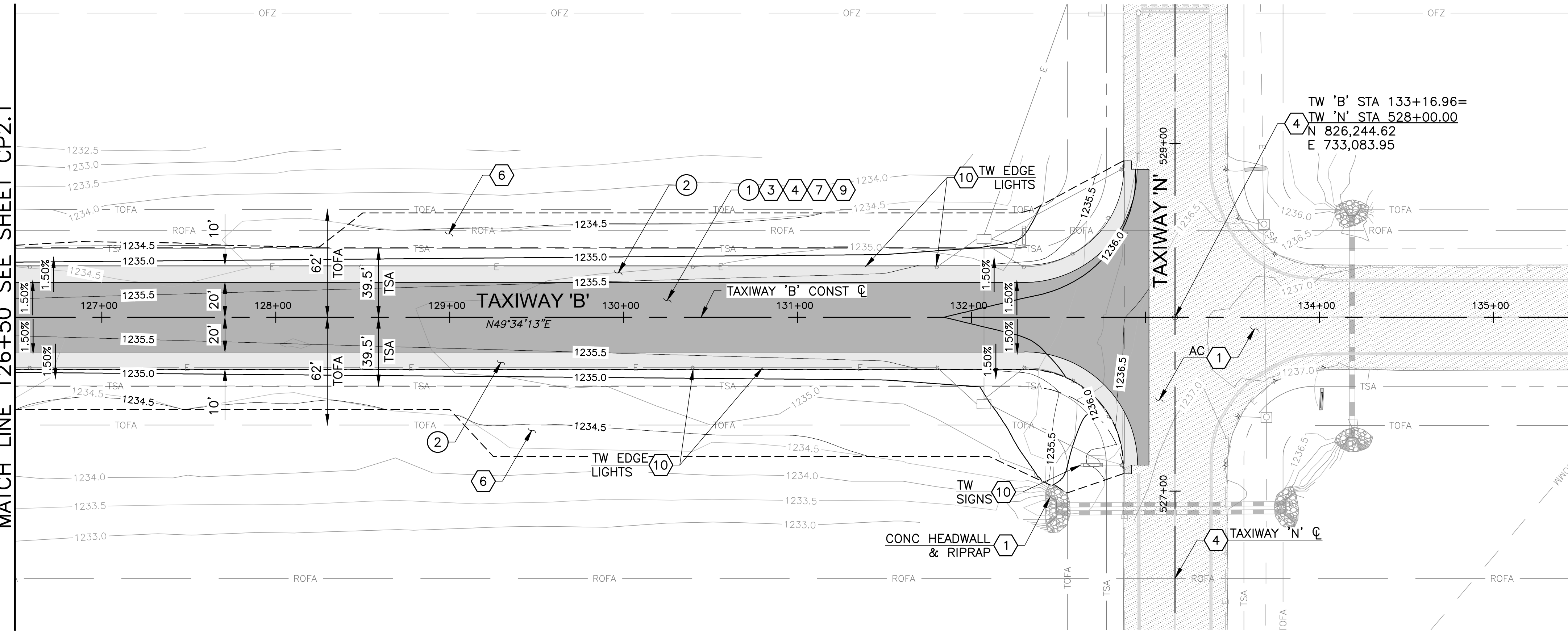


CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

PAVING PLAN & PROFILE  
 STA 120+00 TO STA 126+50  
 CP2.1  
 SHEET 15 OF 39

PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-CP2X.DWG  
 DWG NO.  
 CP2.1  
 SHEET 15 OF 39

MATCH LINE 126+50 SEE SHEET CP2.1



CONSTRUCTION NOTES

- ① AC TAXIWAY PAVEMENT 3,198 SY  
SECT A, DWG G2.1
- ② AC TAXIWAY SHOULDER PAVEMENT 1,480 SY  
SECT B, DWG G2.1

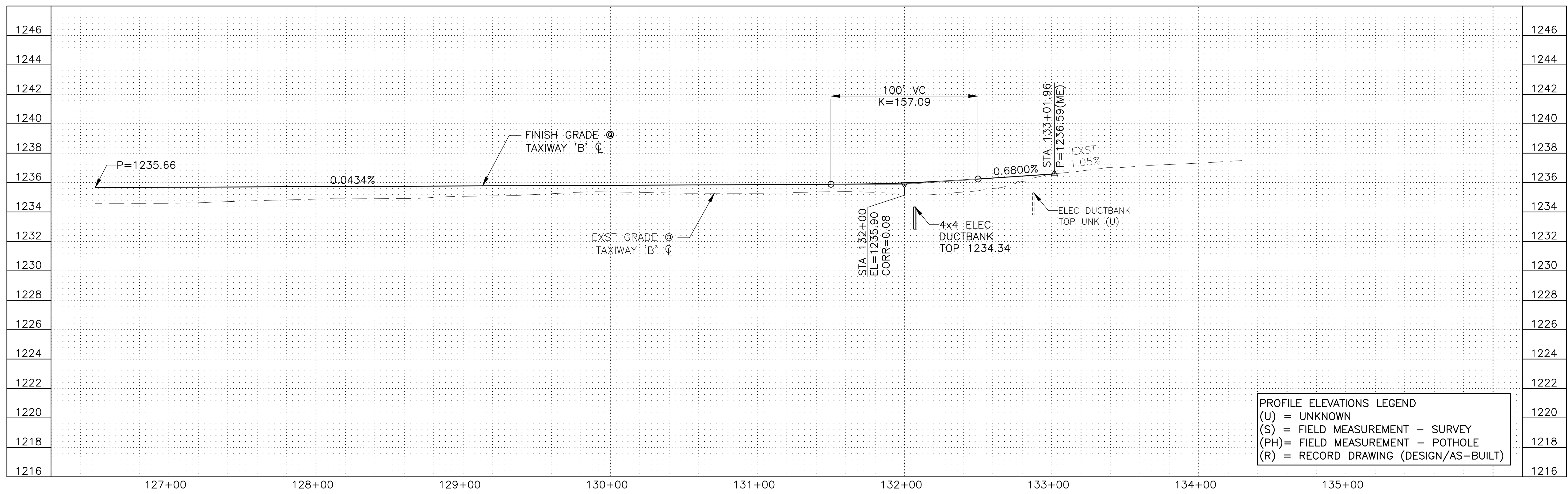
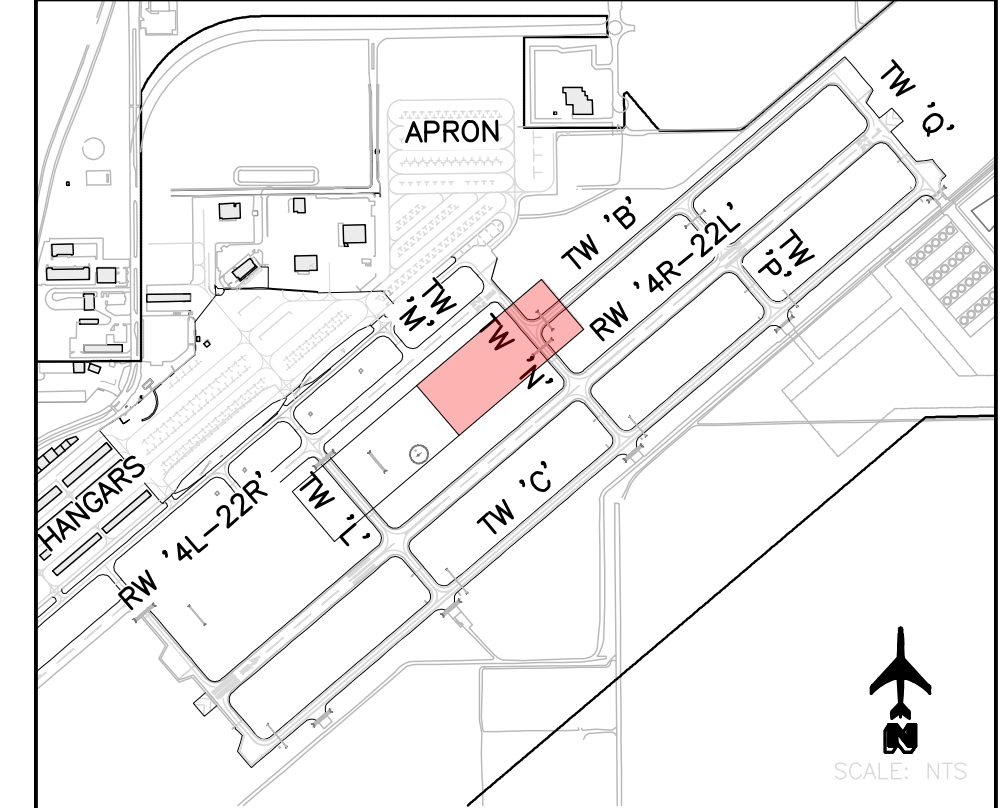
REFERENCE NOTES

- ① PROTECT IN PLACE
- ③ REFER TO DWG CD1.1 FOR DEMOLITION PLANS
- ④ REFER TO DWG CP1.1-CP1.2 FOR GEOMETRIC CONTROL PLANS
- ⑥ REFER TO DWG CG1.1 FOR GRADING & DRAINAGE PLAN
- ⑦ REFER TO DWG CM1.1 FOR PAVEMENT MARKING PLAN
- ⑨ REFER TO DWG CP3.1 FOR PAVEMENT ELEVATION PLAN
- ⑩ REFER TO DWG E3.0-E3.1 FOR AIRFIELD ELECTRICAL PLANS

LEGEND

- AC TAXIWAY PAVEMENT SECT A, DWG G2.1
- AC TAXIWAY SHOULDER PAVEMENT SECT B, DWG G2.1
- EXISTING AC PAVEMENT
- LIMITS OF GRADING
- GRADE BREAK

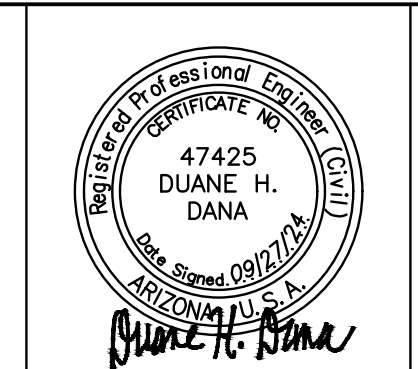
KEY MAP



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REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24



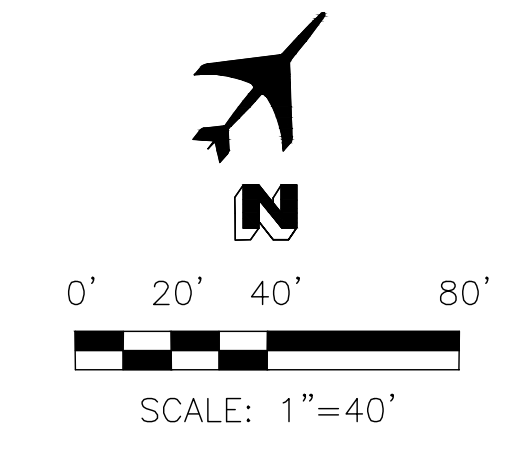
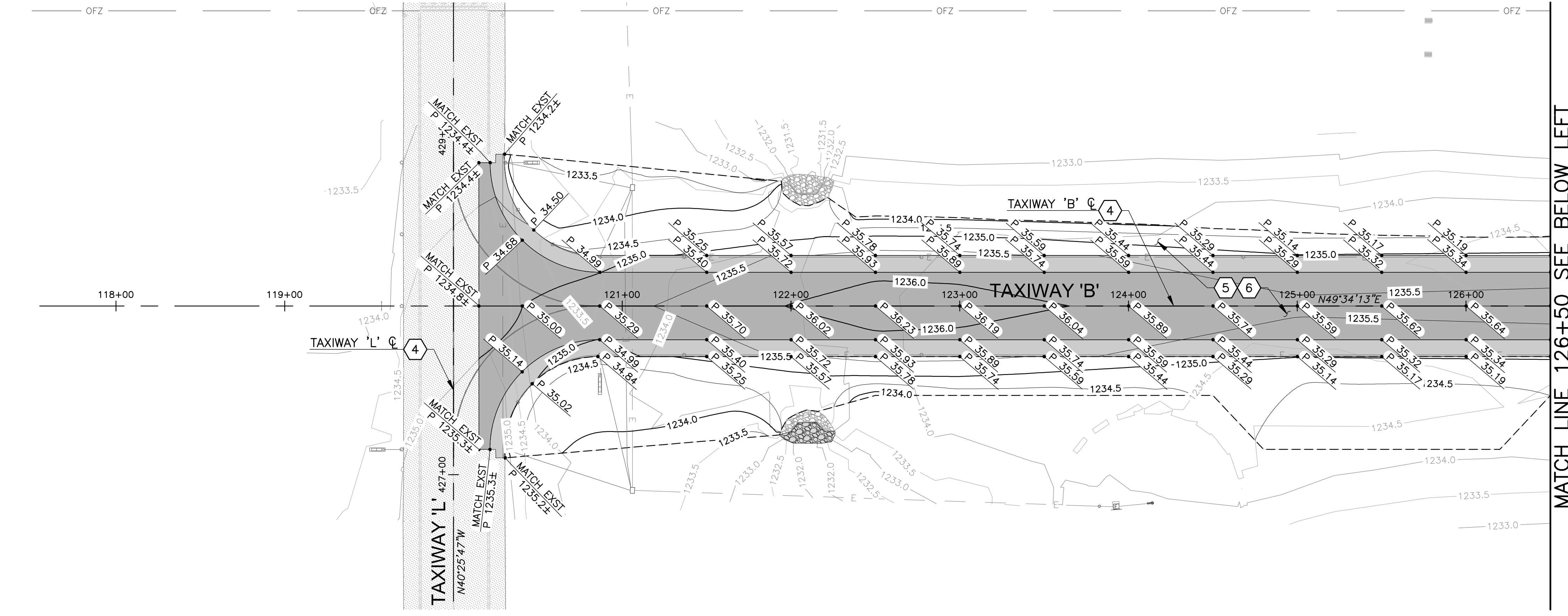
CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

PAVING PLAN & PROFILE  
 STA 126+50 TO STA 135+00  
 CP2.2  
 SHEET 16 OF 39

PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-CP2X.DWG  
 DWG NO.  
 CP2.2  
 SHEET 16 OF 39

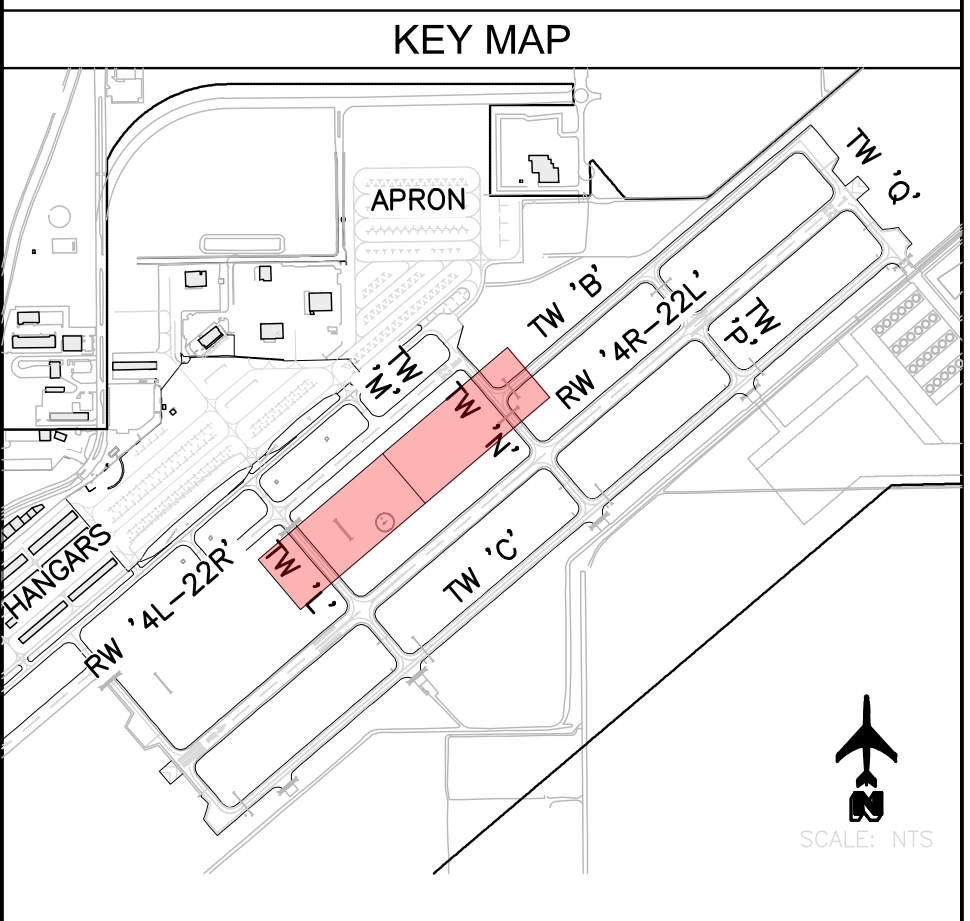
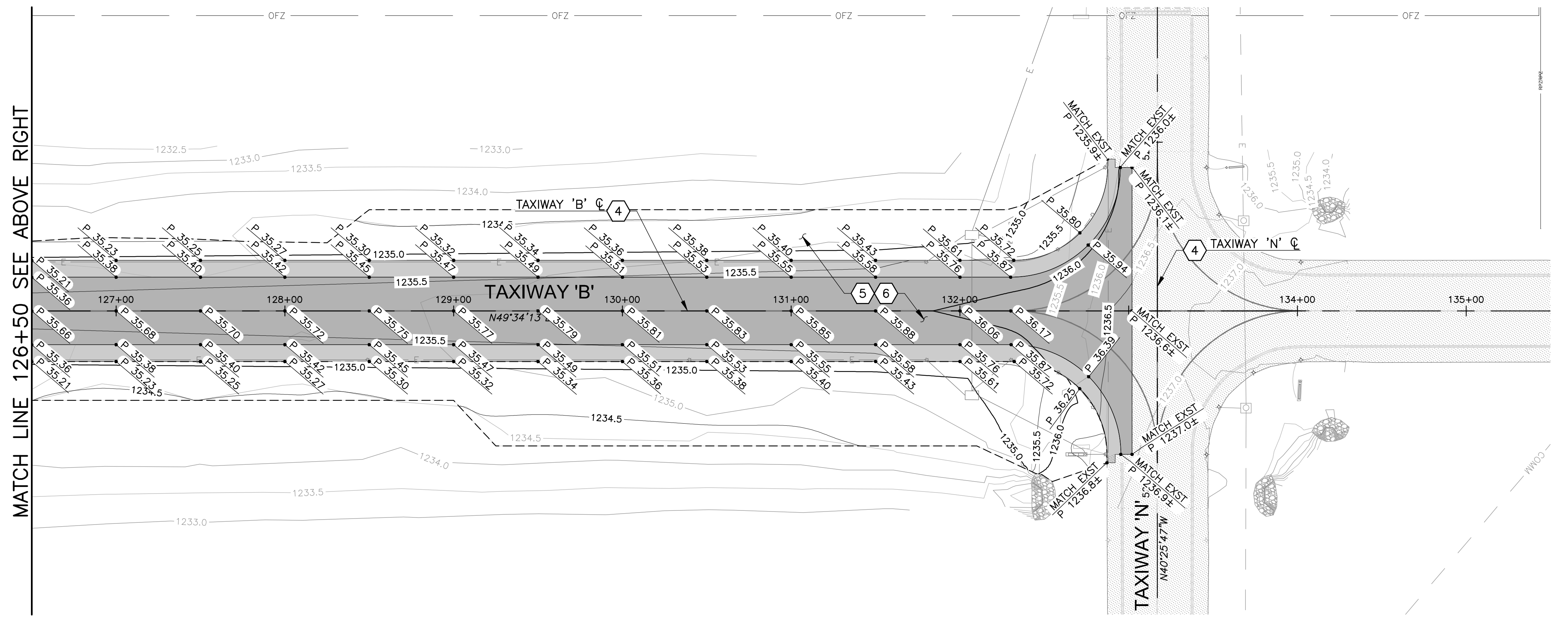


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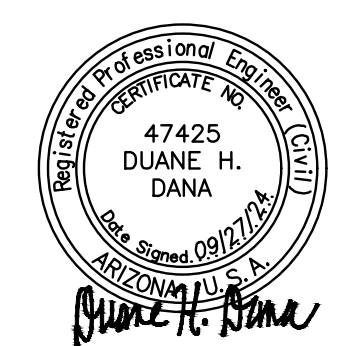
- REFERENCE NOTES
- 4 REFER TO DWG CP1.1-CP1.2 FOR GEOMETRIC CONTROL PLANS
  - 5 REFER TO DWG CP2.1-CP2.2 FOR TAXIWAY PLAN & PROFILE
  - 6 REFER TO DWG CG1.1 FOR GRADING & DRAINAGE PLAN

MATCH LINE 126+50 SEE BELOW LEFT



REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24

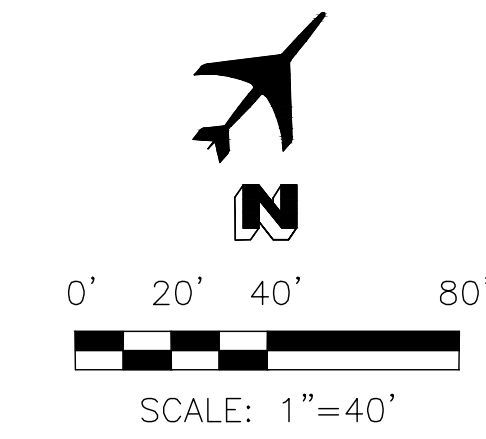
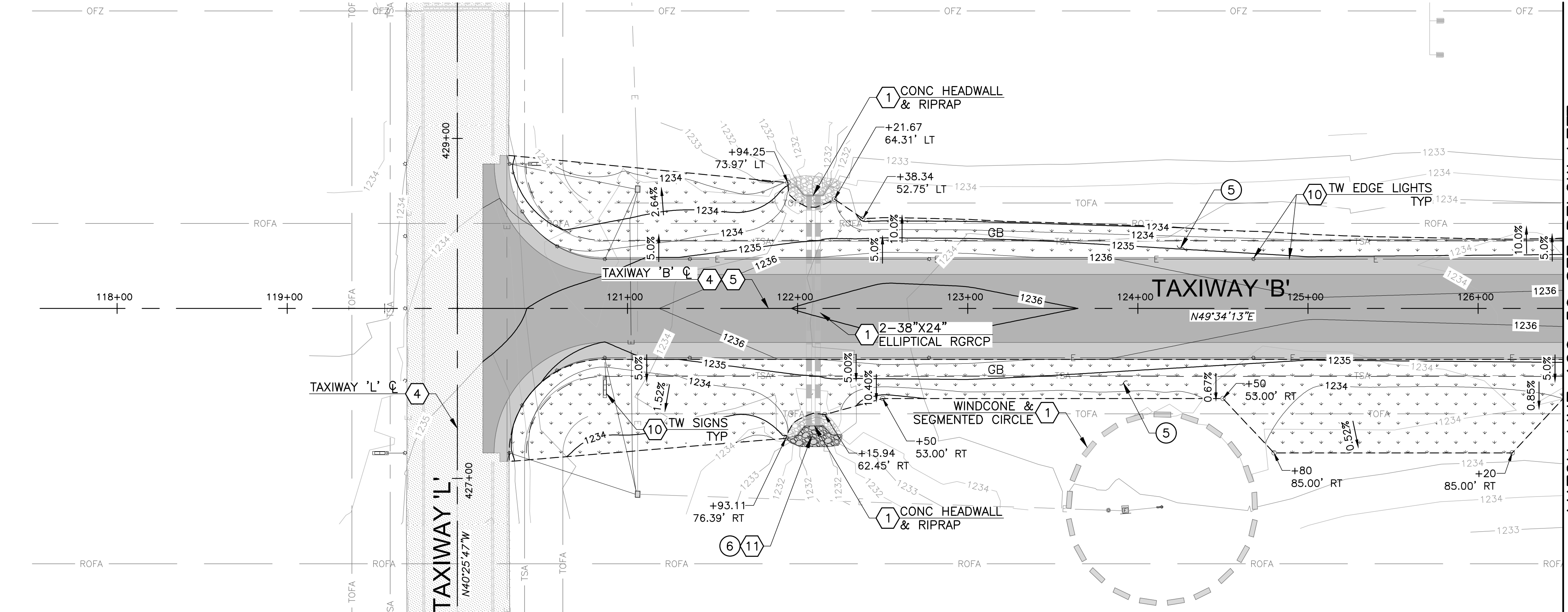


CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

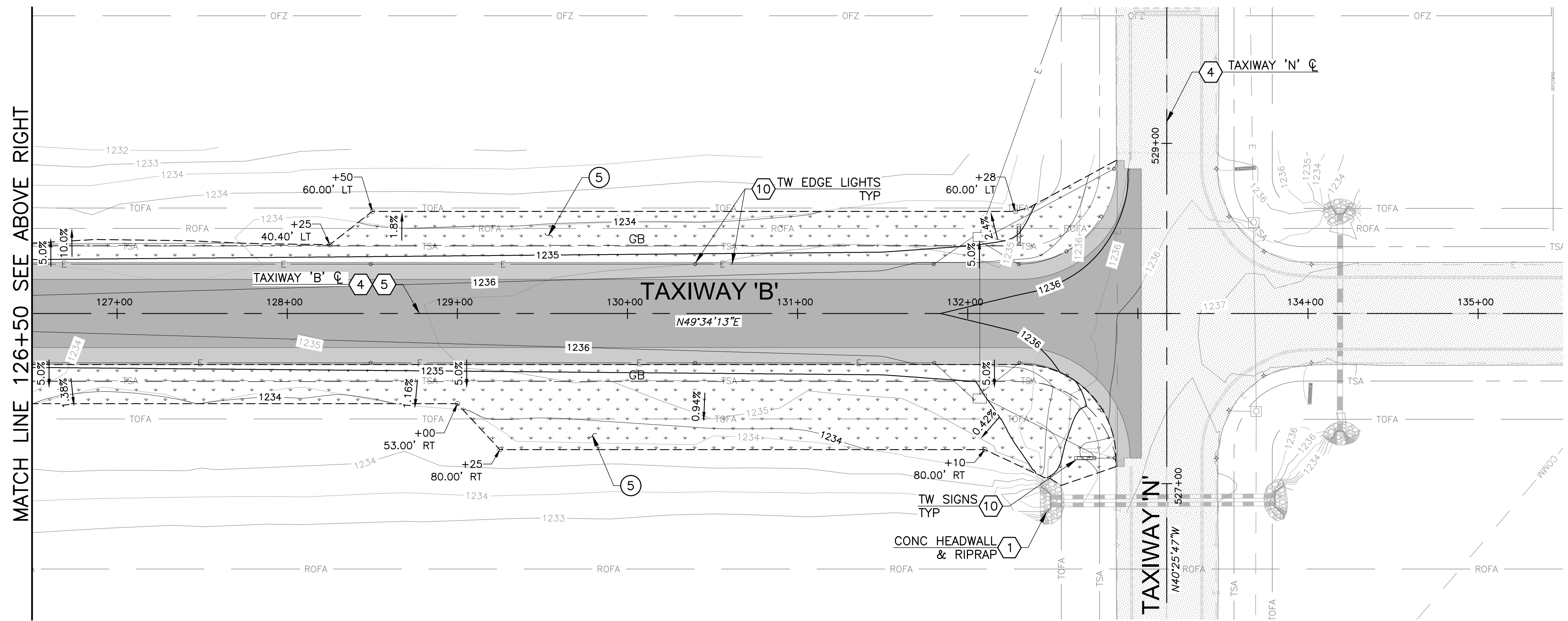
PAVEMENT ELEVATION PLAN  
 STA 120+00 TO STA 135+00  
 DWG NO.  
**CP3.1**  
 SHEET 17 OF 39

PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-CP3X.DWG  
 DWG NO.  
**CP3.1**  
 SHEET 17 OF 39

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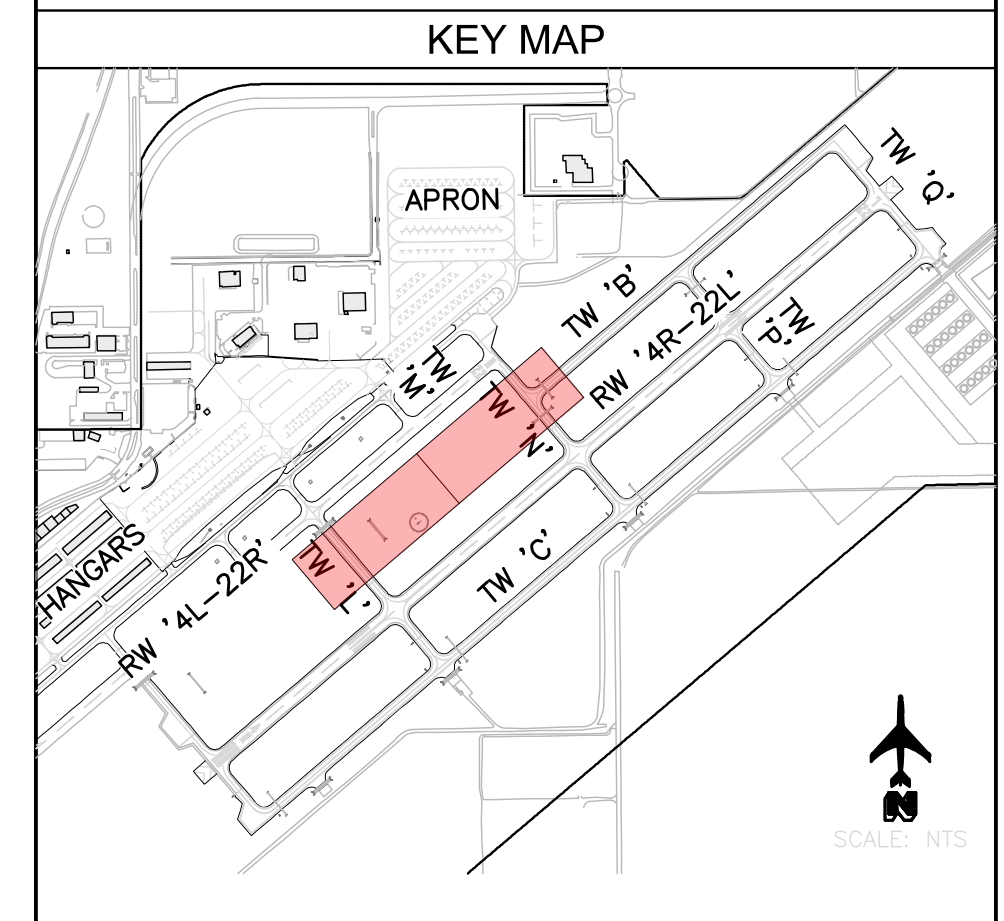
MATCH LINE 126+50 SEE BELOW LEFT



MATCH LINE 126+50 SEE ABOVE RIGHT

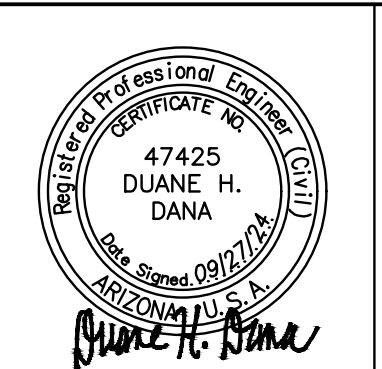
- CONSTRUCTION NOTES**
- ⑤ SEEDING 1.9 AC
  - ⑥ RIPRAP D50=6", T=12" 31 SY
- REFERENCE NOTES**
- ① PROTECT IN PLACE
  - ④ REFER TO DWG CP2.1-CP2.2 FOR GEOMETRIC CONTROL PLANS
  - ⑤ REFER TO DWG CP2.1-CP2.2 FOR TAXIWAY PLAN & PROFILE
  - ⑩ REFER TO DWG E3.0-E3.1 FOR AIRFIELD ELECTRICAL PLANS
  - ⑪ CONTRACTOR TO SALVAGE & REPLACE RIPRAP AS NEEDED TO MAINTAIN EXISTING GRADE

- LEGEND**
- SEEDING
  - AC TAXIWAY PAVEMENT SECT A, DWG G2.1
  - AC TAXIWAY SHOULDER PAVEMENT SECT B, DWG G2.1
  - EXISTING AC PAVEMENT
  - LIMITS OF GRADING
  - GRADE BREAK



REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24

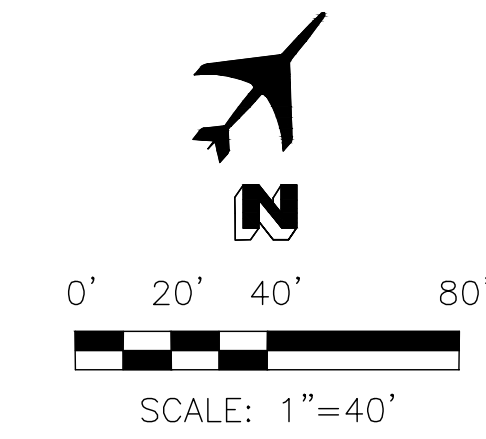
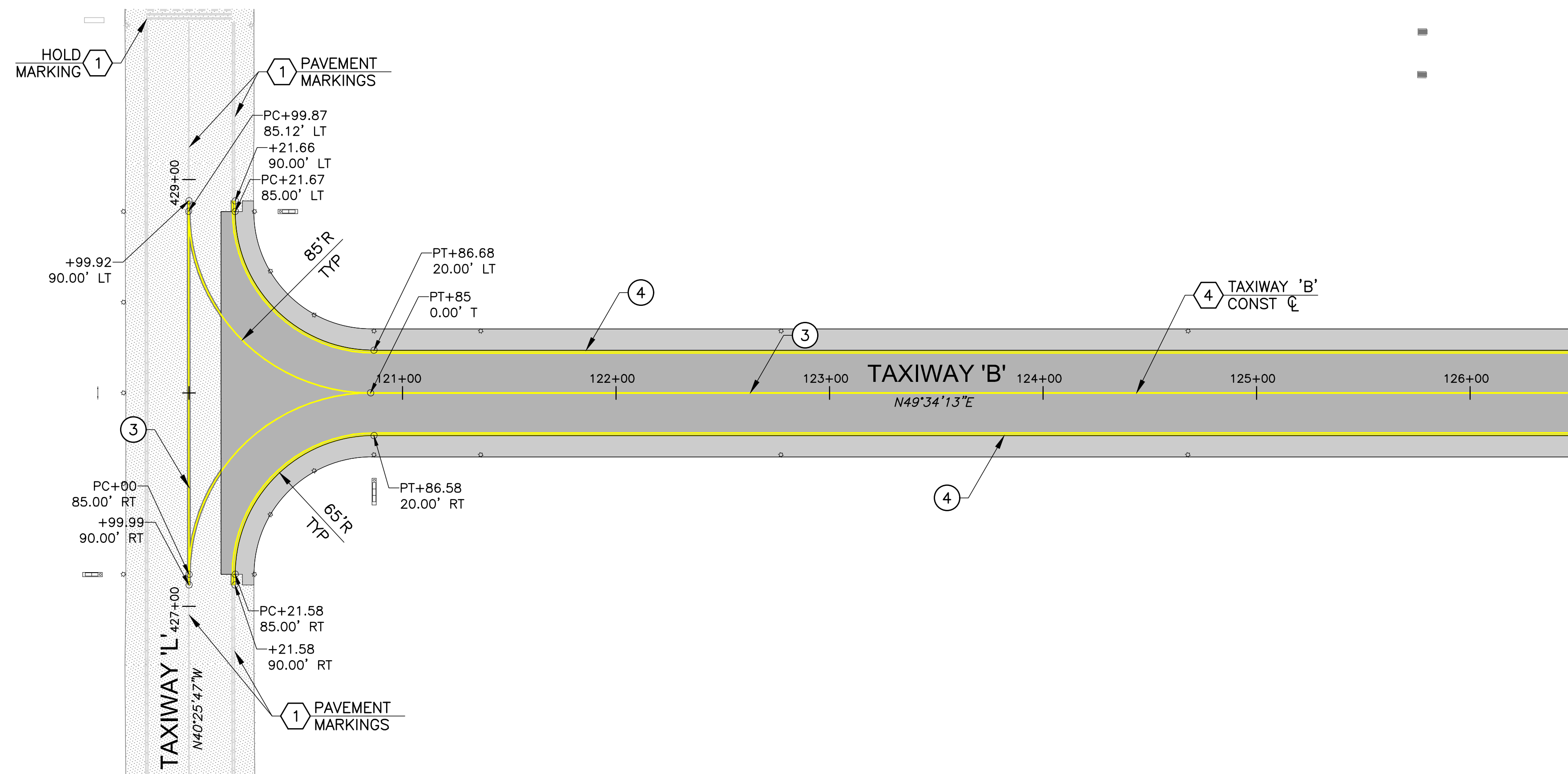


CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

GRADING & DRAINAGE PLAN  
 STA 120+00 TO STA 135+00  
 DWG NO.  
**CG1.1**  
 SHEET 18 OF 39

PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-CG1X.DWG  
 DWG NO.  
**CG1.1**  
 SHEET 18 OF 39

J:\2021\1021015\_11\_CHD\_TW\_B\_IMPROVEMENTS\_PHASE1\CAD\21015\_11-CM1X-DWG-Sep. 27, 2024 11:21 AM DARRIN OAKLEY



MATCH LINE 126+50 SEE BELOW LEFT

CONSTRUCTION NOTES

- ③ TAXIWAY CENTERLINE MARKING 2,515 LF  
DET 1, DWG CM2.1
- ④ TAXIWAY EDGE MARKING 2,717 LF  
DET 2, DWG CM2.1

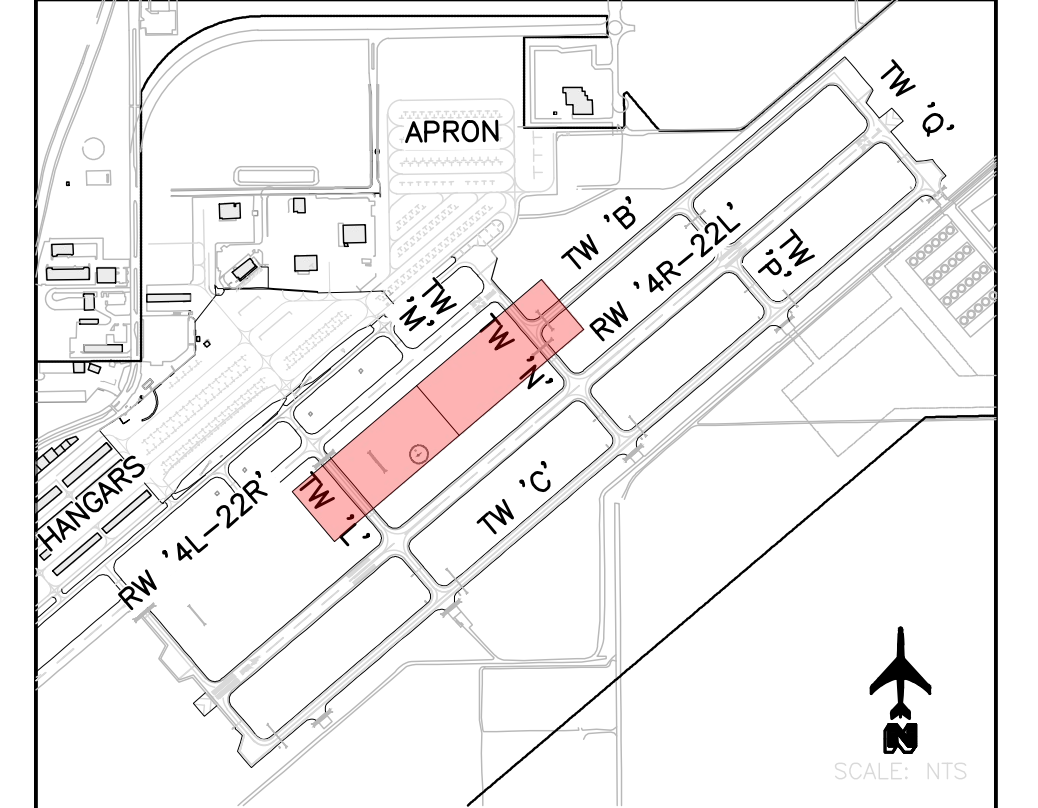
REFERENCE NOTES

- ① PROTECT IN PLACE
- ④ REFER TO DWG CP1.1-CP1.2 FOR GEOMETRIC CONTROL PLANS

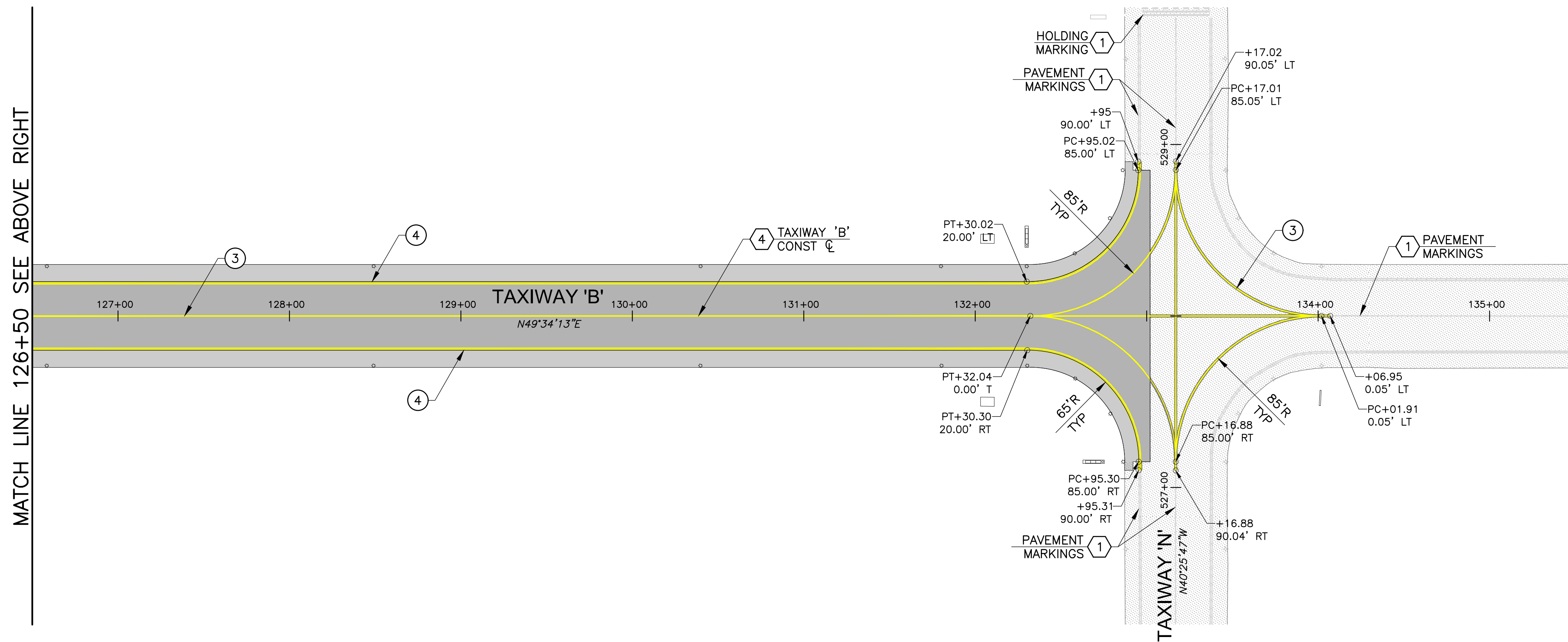
PAVEMENT MARKING NOTES

1. ALL MARKINGS SHALL COMPLY WITH FAA ADVISORY CIRCULAR (AC) 150/5340-1M (MOST CURRENT VERSION). IF, IN THE OPINION OF THE AIRPORT, A CONFLICT EXISTS BETWEEN THE AC AND THE PLANS, THE AC SHALL GOVERN.
2. ALL PERMANENT PAVEMENT MARKINGS SHALL RECEIVE GLASS BEADS.
3. ALL BLACK PAINT IS CONSIDERED INCIDENTAL TO THE PROJECT. THERE WILL BE NO SEPARATE MEASUREMENT FOR BLACK PAINT.

KEY MAP

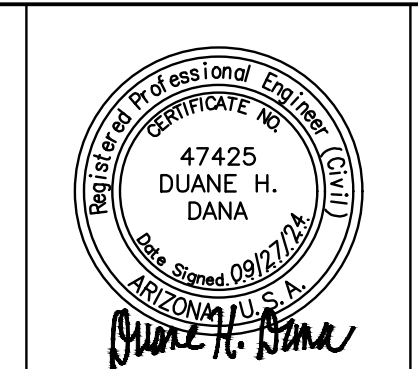


MATCH LINE 126+50 SEE ABOVE RIGHT



REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24



CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

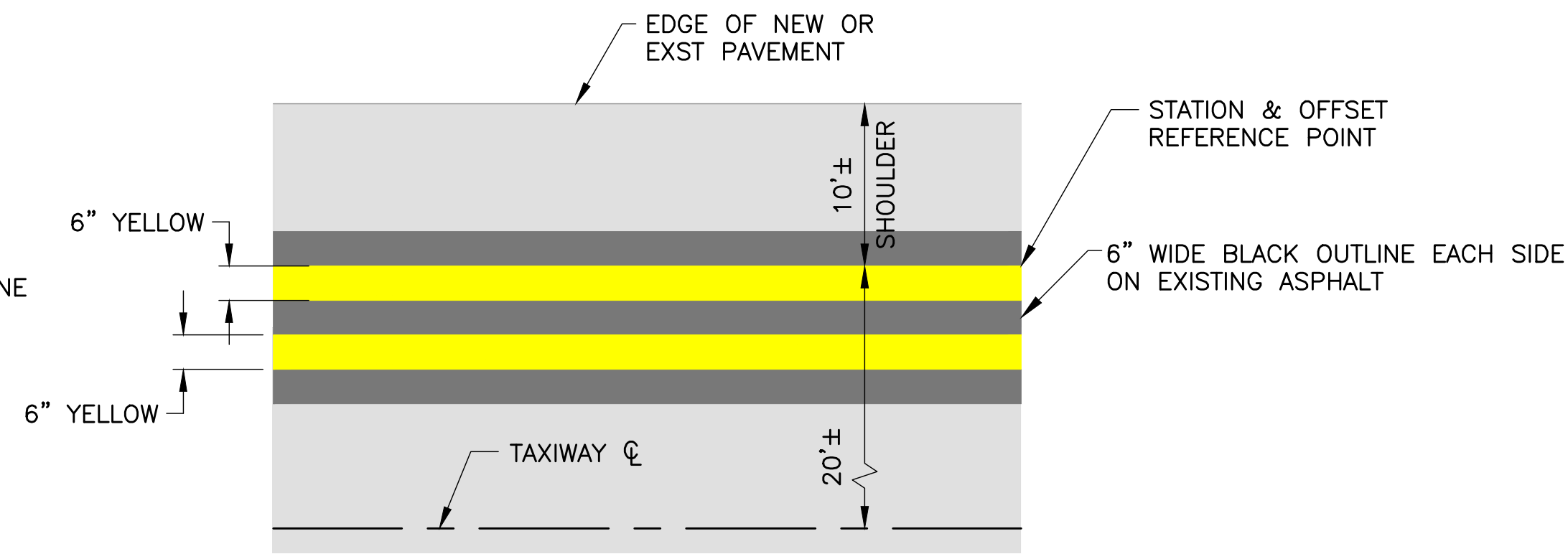
PAVEMENT MARKING PLAN  
 STA 120+00 TO STA 135+00

PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-CM1X.DWG  
 DWG NO.  
**CM1.1**  
 SHEET 19 OF 39

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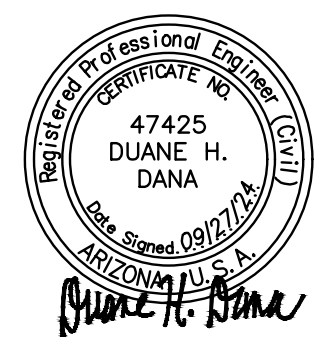
1 TAXIWAY CENTERLINE MARKING  
NTS



2 TAXIWAY EDGE MARKING  
NTS

REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24



CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

PAVEMENT MARKING DETAILS

PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-CM1X.DWG  
 DWG NO.  
**CM2.1**  
 SHEET 20 OF 39



GENERAL ELECTRICAL REQUIREMENTS

- 1. THE COMPLETED INSTALLATION SHALL CONFORM TO ALL APPLICABLE FEDERAL, STATE AND LOCAL CODE ORDINANCES AND REGULATIONS. CONTRACTOR SHALL OBTAIN NECESSARY PERMITS AND INSPECTIONS REQUIRED BY THE AUTHORITIES HAVING JURISDICTION. ALL WORK SHALL BE DONE IN A NEAT, WORKMANLIKE, FINISHED, AND SAFE MANNER, ACCORDING TO THE LATEST PUBLISHED N.E.C.A. STANDARDS OF INSTALLATION, UNDER COMPETENT SUPERVISION. INSTALL GROUNDING AS REQUIRED BY THE CODE(S) AND FAA ADVISORY CIRCULAR REQUIREMENTS AND DETAILED PER THIS PLAN SET.
2. VISIT THE SITE PRIOR TO BIDDING TO BECOME FAMILIAR WITH EXISTING CONDITIONS AND ALL OTHER FACTORS WHICH MAY AFFECT THE EXECUTION OF THIS WORK. INCLUDE ALL RELATED COSTS IN THE INITIAL BID PROPOSAL.
3. ALL MATERIALS SHALL BE NEW AND OF THE BEST QUALITY, MANUFACTURED IN ACCORDANCE WITH NEMA, ANSI, U.L. OR OTHER APPLICABLE STANDARDS. THE USE OF MANUFACTURER'S NAMES, MODELS, AND NUMBERS IS INTENDED TO ESTABLISH STYLE, QUALITY, APPEARANCE, USEFULNESS AND BID PRICE. PROPOSED SUBSTITUTIONS SHALL BE SUBMITTED IN WRITING AND REVIEWED BY THE ENGINEER BEFORE ORDERING.
4. PROTECT ALL ELECTRICAL MATERIAL AND EQUIPMENT INSTALLED UNDER THIS DIVISION AGAINST DAMAGE BY OTHER TRADES, WEATHER CONDITIONS OR ANY OTHER CAUSES. EQUIPMENT FOUND DAMAGED OR IN OTHER THAN NEW CONDITION WILL BE REJECTED AS DEFECTIVE.
5. LEAVE THE SITE CLEAN, REMOVE ALL DEBRIS, EMPTY CARTONS, TOOLS, CONDUIT, WIRE SCRAPS AND ALL MISCELLANEOUS SPARE EQUIPMENT AND MATERIALS USED IN THE WORK DURING CONSTRUCTION. ALL COMPONENTS SHALL BE FREE OF DUST, GRIT AND FOREIGN MATERIALS, LEFT AS NEW BEFORE FINAL ACCEPTANCE OF WORK.
6. EXPOSED CONDUITS TO BE GALVANIZED RIGID STEEL, MINIMUM SIZE 3/4", UNLESS OTHERWISE NOTED ON THE PLANS.
7. ALL SAFETY SWITCHES AND OTHER DISTRIBUTION AND CONTROL ELECTRICAL EQUIPMENT SHALL BE U.L. LISTED AND RATED FOR HEAVY DUTY SERVICE.
8. ALL WIRING DEVICES SHALL BE SPECIFICATION GRADE.
9. ALL ELECTRICAL EQUIPMENT, CONDUIT, WIRING, BOXES, ETC. SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW PRIOR TO ORDERING. THE SUBMITTALS SHALL BE NEATLY GROUPED AND ORGANIZED. PERTINENT INFORMATION SHALL BE HIGHLIGHTED, AND THE SPECIFIC PRODUCT SHALL BE IDENTIFIED. ALL SUBMITTALS SHALL BE COMPLETE, AND PRESENTED IN ONE PACKAGE. THE SUBMITTAL SHALL INCLUDE A COMPLETE LIST OF THE EQUIPMENT AND MATERIALS, INCLUDING THE MANUFACTURER'S NAME, PRODUCT SPECIFICATION, DESCRIPTIVE DATA, TECHNICAL LITERATURE, PERFORMANCE CHARTS, CATALOG CUTS, INSTALLATION INSTRUCTIONS, AND SPARE PART RECOMMENDATIONS FOR EACH DIFFERENT ITEM OF THE EQUIPMENT SPECIFIED.
10. CONDUIT/CONDUCTOR RUNS SHOWN ARE DIAGRAMMATICAL ONLY. THE BEST FINAL CONDUIT ROUTING SHALL BE AS DETERMINED BY THE ELECTRICAL CONTRACTOR AT THE TIME OF CONSTRUCTION. THE CITY INSPECTOR AND AIRPORT REPRESENTATIVE MUST FIELD REVIEW CONTRACTOR'S PROPOSED CONDUIT ROUTING AND HANDHOLE LOCATIONS PRIOR TO ANY SAW CUTTING OR EXCAVATIONS.
11. IT IS THE OBLIGATION OF THE CONTRACTOR TO ORGANIZE HIS WORK, SO THAT A COMPLETE ELECTRICAL, INSTRUMENTATION, AND CONTROL SYSTEM FOR THE FACILITY WILL BE PROVIDED, AND WILL BE SUPPORTED BY ACCURATE SHOP AND RECORD DRAWINGS, AND O & M MANUALS.
12. PRIOR TO PERFORMING UNDERGROUND EXCAVATION, THE CONTRACTOR SHALL CONTACT BLUE STAKE TO OBTAIN FIELD UNDERGROUND UTILITIES LOCATION MARKING. THE CONTRACTOR SHALL UNCOVER, AHEAD OF CONSTRUCTION, ALL LINES BEING TIED INTO AND ALL INTERSECTING ELECTRICAL LINES AND UTILITIES AS SHOWN ON THE PLANS OR MARKED BY BLUE STAKE TO VERIFY THEIR LOCATION AND DEPTH. CONTRACTOR SHALL ASSUME THAT ALL EXISTING DUCTBANKS THAT CROSS EXISTING AIRFIELD PAVEMENTS OR ARE WITHIN RUNWAY OR TAXIWAY SAFETY AREAS ARE ENCASED. UTILITY SERVICES AND ALL EXISTING ELECTRICAL LINES SHALL BE LOCATED AND PROTECTED BY THE CONTRACTOR. COORDINATE WITH ENGINEER AND/OR APPROPRIATE UTILITY COMPANY. ALL PRECAUTIONS SHALL BE USED WHILE WORKING NEAR ALL UTILITIES, TO AVOID INJURY OR DEATH TO PERSONNEL, PROPERTY DAMAGE AND/OR INTERRUPTION OF SERVICE.
13. THE CONTRACTOR SHALL ALLOW TWO WEEKS FOR REVIEW OF THE SUBMITTALS BY THE ENGINEER. THIS PERIOD SHALL BE CALCULATED FROM THE TIME THE ENGINEER HAS RECEIVED THE SUBMITTALS TO THE TIME THE SUBMITTALS LEAVE THE ENGINEER'S OFFICE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DELAYS RESULTING FROM "FURNISH AS NOTED", "REVISE AND RESUBMIT", "NOT REVIEWED" AND/OR "REJECTED" SUBMITTALS. INCOMPLETE SUBMITTALS WILL BE RETURNED FOR RESUBMITTAL WITH NO CHANGE BEING ALLOWED TO THE CONSTRUCTION SCHEDULE. SUBMITTALS SHALL INCLUDE COMPREHENSIVE INFORMATION AND TECHNICAL DATA, CLEARLY IDENTIFYING AND SUBSTANTIATING THE CHARACTERISTICS REQUIRED BY THE CONSTRUCTION DOCUMENTS.
14. ALL REQUESTS FOR INFORMATION (RFIs) SHALL BE PRESENTED TO THE ENGINEER ALLOWING TWO WEEKS FOR RESPONSE. THIS PROCESS SHALL BE CONSIDERED IN THE SCHEDULING PROCESS AND SHALL NOT CAUSE A DELAY IN THE COMPLETION OF THE PROJECT.

ELECTRICAL ABBREVIATIONS:

- ATCT AIR TRAFFIC CONTROL TOWER
BCC BARE COPPER GROUND
CE CONCRETE ENCASED
CKT CIRCUIT
COC CITY OF CHANDLER
FAA FEDERAL AVIATION ADMINISTRATION
HH HANDHOLE
LF LINEAR FEET
N.E.C.A. NATIONAL ELECTRICAL CONTRACTORS ASSOCIATION
NEMA NATIONAL ELECTRICAL MANUFACTURERS ASSOCIATION
NPI NON PAY ITEM
OFZ OBSTACLE FREE ZONE
PAPI PRECISION APPROACH PATH INDICATOR
PVC POLY-VINYL CHLORIDE
ROFA RUNWAY OBJECT FREE AREA
RSA RUNWAY SAFETY AREA
SE SLURRY ENCASED
SGN SIGN
TOFA TAXIWAY OBJECT FREE AREA
TSA TAXIWAY SAFETY AREA
TYP TYPICAL

ELECTRICAL LEGEND:

(UNLESS OTHERWISE NOTED ON PLANS)

- / --- PVC W/ #8-5KV L-824 TYPE "C" CABLE PER INDICATED LIGHTING CIRCUIT. (QUANTITY AND SIZE AS INDICATED ON PLAN SHEETS) (/ - INDICATES NUMBER OF CONDUCTORS)
--- --- EXISTING CONDUIT/CONDUCTOR
--- / --- EXISTING CONDUIT WITH NEW CONDUCTOR (/ - INDICATES NUMBER OF CONDUCTORS)
● SPLICE POINT OF EXISTING AND NEW CONDUIT AND CONDUCTOR
⊗ EXISTING TAXIWAY EDGE LIGHT
⊗ EXISTING RUNWAY EDGE LIGHT
⊕ EXISTING RUNWAY THRESHOLD/END LIGHT
□/□ EXISTING HANDHOLE
MH EXISTING MANHOLE
EXISTING AIRFIELD GUIDANCE SIGN
○ NEW L-867D JUNCTION CAN
⊗ NEW L-867E JUNCTION CAN
□ NEW 2'x3'x3' CONCRETE HANDHOLE
HH NEW 4'x4'x4' CONCRETE HANDHOLE
NEW AIRFIELD GUIDANCE SIGN
NEW L-861T INCANDESCENT (QUARTZ) ELEVATED TAXIWAY EDGE LIGHT ON NEW L-867 BASE
NEW L-853 RETROREFLECTIVE EDGE MARKER
NEW TAXIWAY END MARKER - UNLIGHTED SIGN

SELECTED FAA ADVISORY CIRCULARS FOR AIRPORT PROJECTS (MOST RECENT VERSION):

- 150/5340-18 STANDARDS FOR AIRPORT SIGN SYSTEMS
150/5340-30 DESIGN & INSTALLATION DETAILS FOR AIRPORT VISUAL AIDS
150/5345-7 SPECIFICATION FOR L-824 UNDERGROUND ELECTRICAL CABLE FOR AIRPORT LIGHTING CIRCUITS
150/5345-26 SPECIFICATION FOR L-823 PLUG AND RECEPTACLE, CABLE CONNECTORS
150/5345-42 SPECIFICATION FOR AIRPORT LIGHT BASES, TRANSFORMER HOUSINGS, JUNCTION BOXES, AND ACCESSORIES
150/5345-44 SPECIFICATION FOR RUNWAY AND TAXIWAY SIGNS
150/5345-46 SPECIFICATION OF RUNWAY AND TAXIWAY LIGHT FIXTURES
150/5345-47 SPECIFICATION FOR SERIES TO SERIES ISOLATION TRANSFORMERS FOR AIRPORT LIGHTING SYSTEMS
150/5345-53 AIRPORT LIGHTING EQUIPMENT CERTIFICATION PROGRAM
150/5370-2 OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION
150/5370-10 STANDARDS FOR SPECIFYING CONSTRUCTION OF AIRPORTS

SHEET INDEX

- E1.0 ELECTRICAL LEGEND
E1.1 ELECTRICAL NOTES
E2.0 - E2.1 AIRFIELD ELECTRICAL DEMOLITION PLANS
E3.0 - E3.1 AIRFIELD ELECTRICAL PLANS
E4.0 AIRFIELD GUIDANCE SIGNAGE SCHEDULE
E5.0 - E5.5 AIRFIELD ELECTRICAL DETAILS
E6.0 EQUIPMENT DATA TABLES

Table with 5 columns: REV. NO., DATE, DRWN, CHKD, REMARKS. Row 1: REV. NO., DATE, DRWN, CHKD, REMARKS. Row 2: REV. NO., DATE, DRWN, CHKD, REMARKS.

DESIGNED BY: RD
DRAWN BY: JBW
SHEET CHK'D BY: CA
CROSS CHK'D BY: SW
APPROVED BY: CA
DATE: 09/27/2024



CITY OF CHANDLER, ARIZONA
CHANDLER MUNICIPAL AIRPORT
TAXIWAY 'B' IMPROVEMENTS PHASE 1:
TAXIWAY 'L' TO TAXIWAY 'N'

ELECTRICAL LEGEND & NOTES

PROJECT NO. 1021015.11
FILE NAME: 23011 E1.0.DWG
DWG NO.
E1.0
SHEET 21 OF 39

ELECTRICAL CONSTRUCTION PHASING NOTES

1. CONTRACTOR SHALL COORDINATE ALL WORK WITH AIRPORT MAINTENANCE, AIRPORT OPERATIONS, AND THE ENGINEER AS NECESSARY.
2. CONTRACTOR SHALL GIVE 72 HOURS NOTICE PRIOR TO WORKING ON OR AROUND ANY DUCTBANKS, HANDHOLES, ETC.
3. CONTRACTOR SHALL MAINTAIN OR HAVE SUFFICIENT MATERIAL/EQUIPMENT REQUIRED TO PROVIDE TEMPORARY LIGHTING AND CIRCUIT EXTENSIONS. THIS INCLUDES, BUT IS NOT LIMITED TO FIXTURES, TRANSFORMERS, BASES, CONDUIT, L-824 CABLE & L-823 SPLICE KITS. THESE ITEMS WILL NOT BE AVAILABLE FROM THE AIRPORT MAINTENANCE SHOP.
4. THE CONTRACTOR SHALL MAINTAIN QUALIFIED PERSONNEL WITH THE APPROPRIATE EQUIPMENT, FOR THE INSTALLATION AND SPLICING OF AIRFIELD LIGHTING. SUCH PERSONNEL SHALL BE CAPABLE OF 60 MINUTE RESPONSE TIME IF THEY ARE NOT ALREADY PRESENT ON THE AIRFIELD.
5. TURN OFF AND COVER EXISTING SIGNAGE THAT MAY POTENTIALLY MISDIRECT AIRCRAFT MOVEMENT INTO CLOSED AREAS BARRICADED FOR CONSTRUCTION. SIGNAGE REQUIRING PARTIAL COVERAGE WITH TAXIWAY LOCATION PANELS REMAINING VISIBLE SHALL REMAIN ON WITH ONLY DIRECTIONAL PORTIONS COVERED WITH SECTIONS OF DARK COLORED TARP OR DOUBLE-LAYERED BURLAP THAT DOES NOT PERMIT VISIBILITY OF COVERED PORTION OF ARRAY DAY OR NIGHT. COVERS SHALL BE SECURELY HELD IN PLACE BY RATCHETING LASHING STRAPS, NO TAPE OR ADHESIVES WILL BE PERMITTED. SEE PHASING PLANS FOR PLACEMENT OF SIGN COVERS.
6. COVER EXISTING ELEVATED EDGE LIGHT FIXTURES IN CLOSED AREA BARRICADED FOR CONSTRUCTION WITH 4" PVC PIPE. COVER SHALL EXTEND 2" MIN. ABOVE TOP OF FIXTURE.
7. PROVIDE ANY TEMPORARY AIRFIELD CIRCUIT JUMPERS REQUIRED TO MAINTAIN OPERATION OF ALL CIRCUITS AFFECTED BY CONSTRUCTION PRIOR TO START OF DEMOLITION. TEMPORARY CIRCUIT JUMPERS SHALL BE SLEEVED IN 2" CONDUIT, SANDBAGGED OR SECURED TO LOW-LEVEL BARRICADES. TEMPORARY CIRCUIT JUMPERS MAY BE ROUTED THROUGH NEW TAXIWAY CROSSINGS OR EXISTING SPARE CONDUITS AS REQUIRED AND SHALL BE COMPLETELY REMOVED WHEN NO LONGER REQUIRED FOR OPERATION. TEMPORARY JUMPER PLACEMENT SHALL NOT AFFECT AIRCRAFT MOVEMENT OR AIRPORT OPERATIONS.
8. UNCOVER SIGNS AND EDGE LIGHT FIXTURES, REMOVE TEMPORARY JUMPERS, AND VERIFY OPERATION AT THE END OF PROJECT.

ELECTRICAL DEMOLITION GENERAL NOTES:

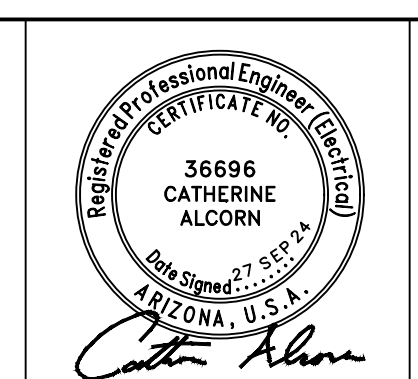
1. WITHIN AREAS OF DEMOLITION, AND AS OTHERWISE SHOWN, CAREFULLY REMOVE IDENTIFIED AIRFIELD SIGNS, BASES, AND ISOLATION TRANSFORMERS, AND OTHER INDICATED ITEMS.
2. CABLE REMOVAL
  - A. DIRECT BURIAL: REMOVE FROM WITHIN MANHOLES AND HANDHOLES AND REMOVE IN AREAS WHERE EXCAVATION REQUIRES DISTURBING.
  - B. IN CONDUIT: REMOVE COMPLETELY BETWEEN NEAREST BASES, HANDHOLES OR MANHOLES.
3. AT INDICATED DEVICES TO BE REMOVED OR IN DEMOLITION AREAS INDICATED, REMOVE ALL CONDUCTORS BACK TO NEAREST FIXTURE BASE OR HANDHOLE OUTSIDE DEMOLITION AREA.
4. FIXTURES, CABLES, CONDUITS, DUCTS, ETC. WHICH ARE NOT SPECIFICALLY INDICATED TO BE REMOVED (OR WHICH ARE SHOWN TO REMAIN WITHIN AREAS OF GENERAL DEMOLITION) SHALL REMAIN IN-PLACE AND FUNCTIONAL.
5. CONTRACTOR SHALL VERIFY EQUIPMENT AND CABLE DESIGNATIONS AND STATUS PRIOR TO REMOVAL OR DISCONNECTING.
6. ALL REMOVED LIGHTS, BASES, AND ISOLATION TRANSFORMERS, SHALL BE SALVAGED AND DELIVERED TO CHANDLER MUNICIPAL AIRPORT OPERATIONS AND MAINTENANCE. ALL OTHER REMOVED MATERIAL SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE REMOVED FROM THE SITE AND PROPERLY DISPOSED OF IN ACCORDANCE WITH LOCAL ORDINANCES.
7. DRAWINGS MAY NOT DETAIL ALL EXISTING FACILITIES IN AREAS OF DEMOLITION. CONTRACTOR SHALL REVIEW THE SITE AND RECORD DRAWINGS TO VERIFY THE DEMOLITION EFFORT INVOLVED.
8. CONTRACTOR SHALL BE REQUIRED TO HAVE A PRIVATE "ON-SITE" UTILITY LOCATING COMPANY AND POTHOLE EXISTING UTILITY LINES AS NEEDED.

ELECTRICAL GENERAL NOTES

1. GROUNDS RODS AND COUNTERPOISE WIRE ARE CONSIDERED INCIDENTAL TO FIXTURE AND CONDUIT INSTALLATION.
2. ELECTRICAL CONTRACTOR SHALL COORDINATE ALL DEMOLITION AND CONSTRUCTION WITH CIVIL CONTRACTOR TO REDUCE CONFLICTS THAT AFFECT CONSTRUCTION PHASING AND SCHEDULING.
3. CONTRACTOR SHALL FIELD VERIFY ALL FIXTURE STATION AND OFFSETS AND CONFIRM ALL LOCATIONS ARE WITHIN SPECIFICATION TOLERANCES.

REV. NO.	DATE	DRWN	CHKD	REMARKS

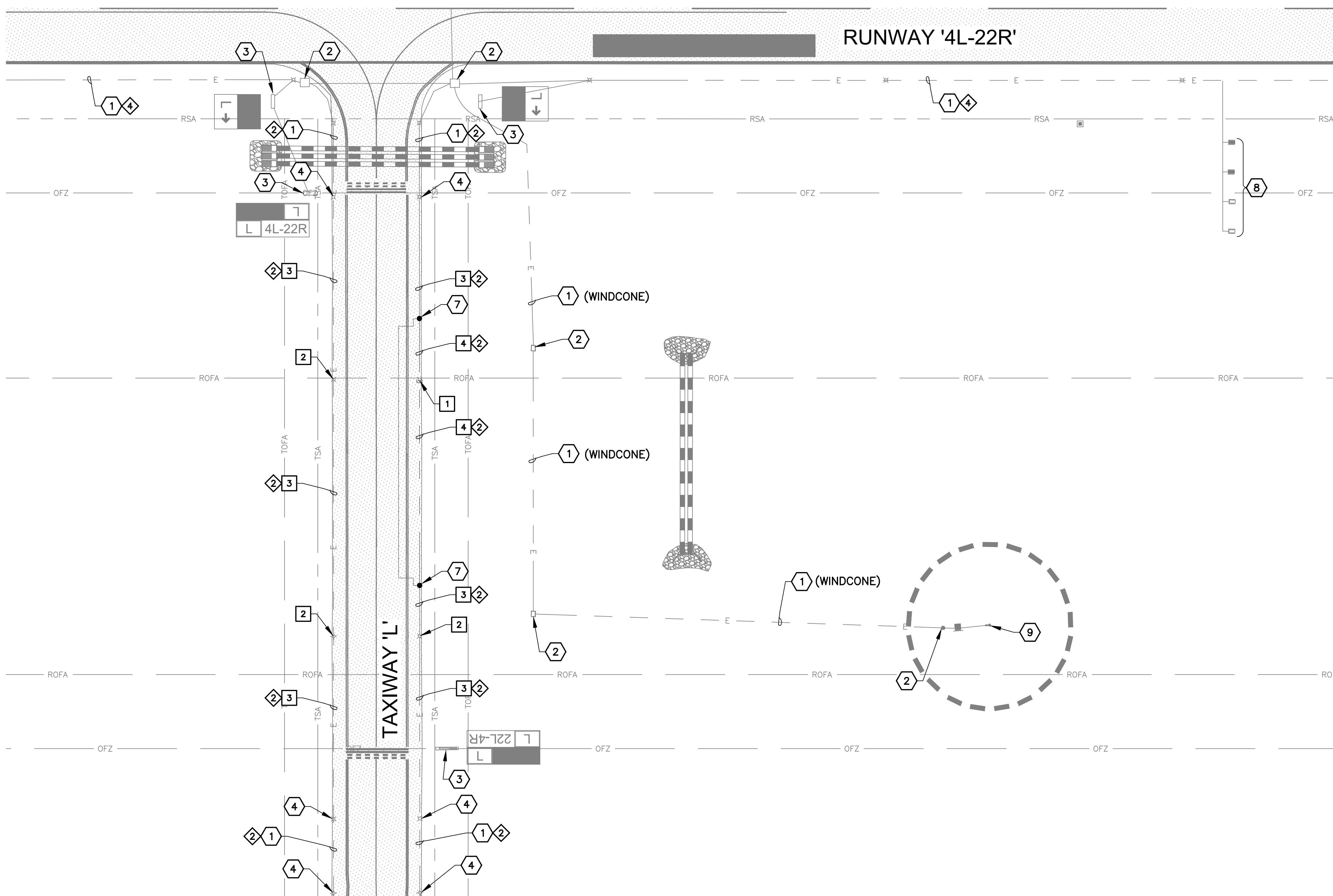
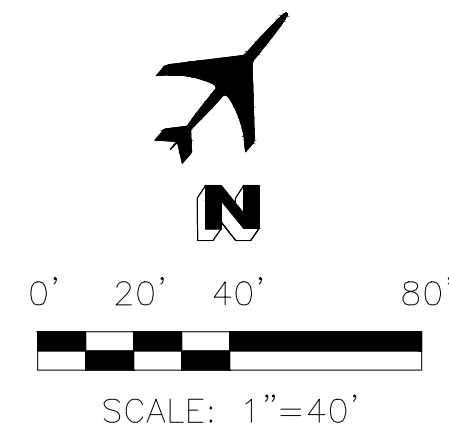
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 SHEET CHK'D BY: \_\_\_\_\_ CA  
 CROSS CHK'D BY: \_\_\_\_\_ SW  
 APPROVED BY: \_\_\_\_\_ CA  
 DATE: 09/27/2024



CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

ELECTRICAL NOTES

PROJECT NO. 1021015.11  
 FILE NAME: 23011 E1.1.DWG  
 DWG NO.  
**E1.1**  
 SHEET 22 OF 39

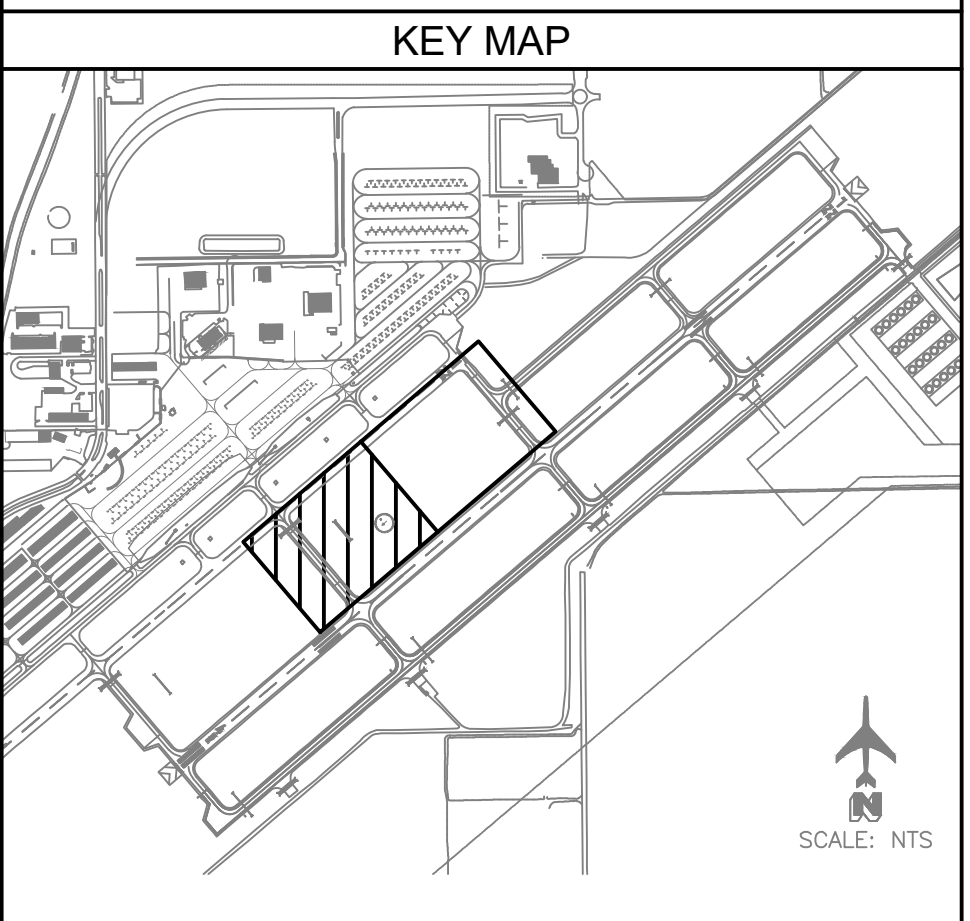


MATCH LINE 126+50 SEE SHEET E2.1

- REMOVAL NOTES**
- 1 REMOVE AND SALVAGE EXISTING TAXIWAY EDGE LIGHT AND ISOLATION TRANSFORMER, RETURN TO OWNER. DEMOLISH BASE CAN. (1 TOTAL)
  - 2 REMOVE AND SALVAGE EXISTING TAXIWAY EDGE LIGHT AND ISOLATION TRANSFORMER, RETURN TO OWNER. BASE CAN TO REMAIN. (3 TOTAL)
  - 3 REMOVE EXISTING CONDUCTORS BACK TO NEXT ADJACENT LIGHT FIXTURE OR HANDHOLE. CONDUIT TO REMAIN. (669 LF)
  - 4 EXCAVATE AND REMOVE EXISTING CONDUIT AND CONDUCTOR(S). (233 LF TOTAL)

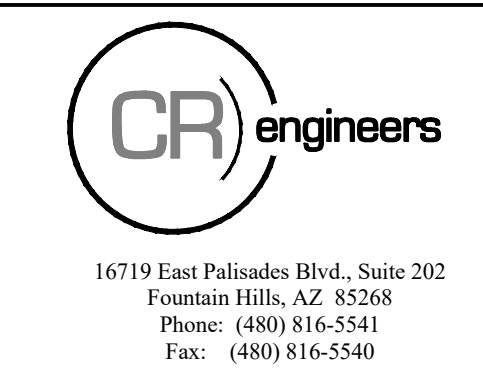
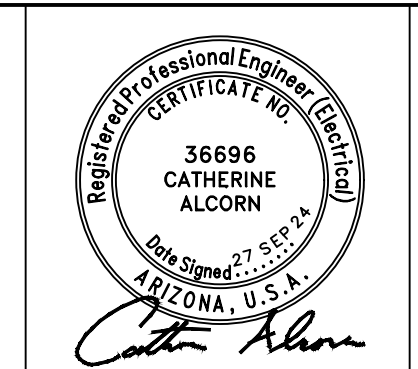
- REFERENCE NOTES**
- 1 EXISTING CONDUIT AND CONDUCTOR TO REMAIN - PROTECT IN PLACE
  - 2 EXISTING HANDHOLE/JUNCTION CAN TO REMAIN - PROTECT IN PLACE
  - 3 EXISTING AIRFIELD GUIDANCE SIGN TO REMAIN - PROTECT IN PLACE
  - 4 EXISTING EDGE LIGHT FIXTURE TO REMAIN - PROTECT IN PLACE
  - 7 APPROXIMATE SPLICE POINT OF EXISTING CONDUIT AND CONDUCTOR
  - 8 EXISTING PAPI TO REMAIN - PROTECT IN PLACE
  - 9 EXISTING WINDCONE TO REMAIN - PROTECT IN PLACE

- CIRCUIT IDENTIFICATION**
- 2 TAXIWAY C
  - 4 RW 4L-22R



REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: RD  
 DRAWN BY: JBW  
 SHEET CHK'D BY: CA  
 CROSS CHK'D BY: SW  
 APPROVED BY: CA  
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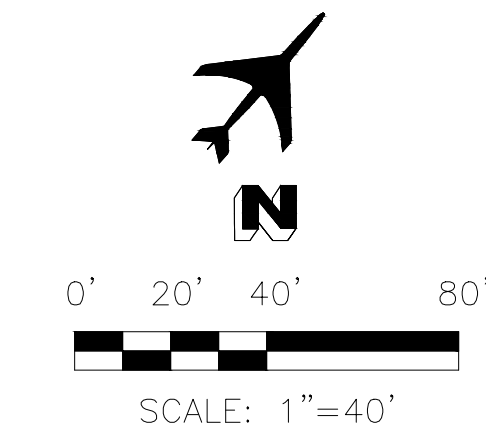
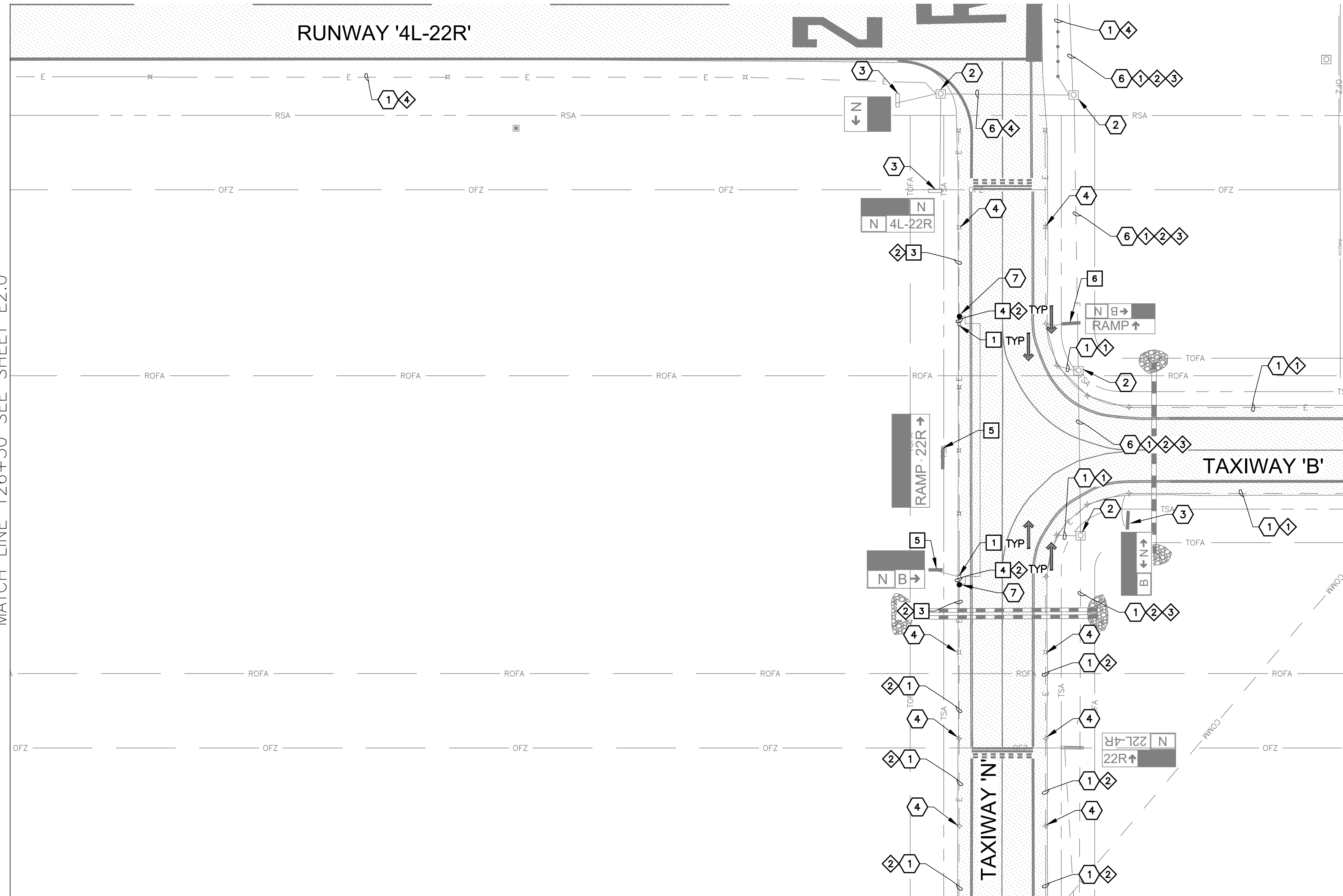


CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
**TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'**

**AIRFIELD ELECTRICAL  
 DEMOLITION PLAN**

PROJECT NO. 1021015.11  
 FILE NAME: 23011 E2.0.DWG  
 DWG NO.  
**E2.0**  
 SHEET 23 OF 39

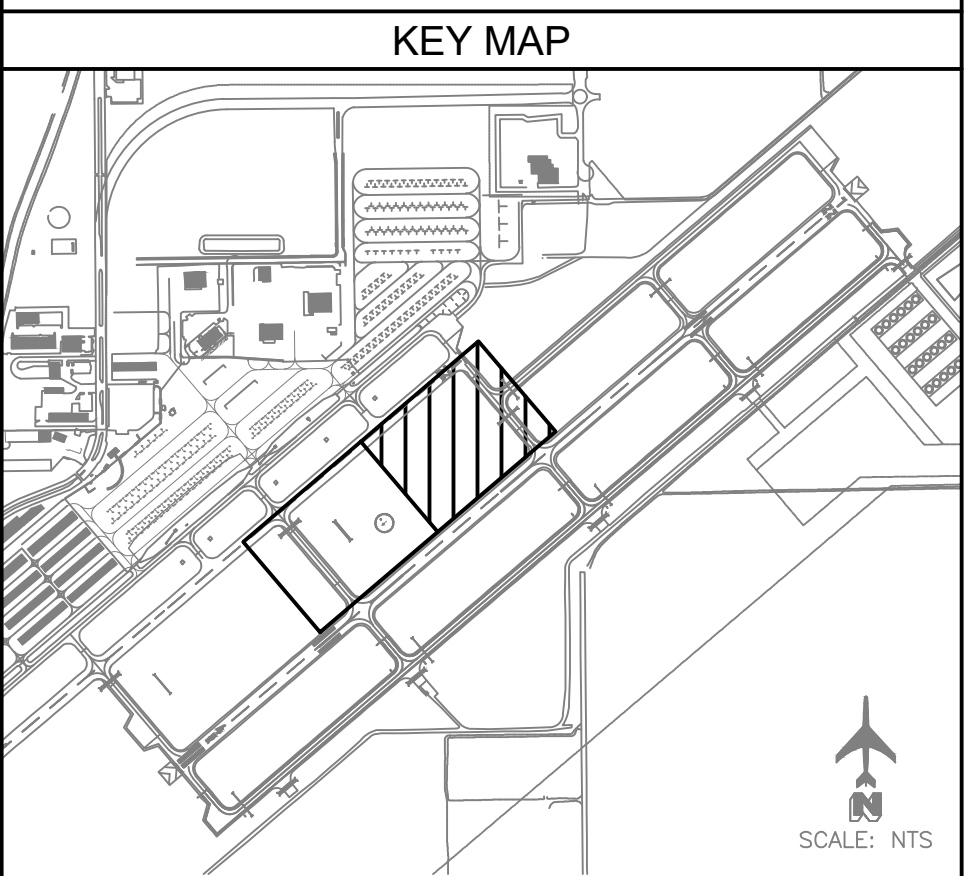
MATCH LINE 126+50 SEE SHEET E2.0



- REMOVAL NOTES**
- 1 REMOVE AND SALVAGE EXISTING TAXIWAY EDGE LIGHT AND ISOLATION TRANSFORMER, RETURN TO OWNER. DEMOLISH BASE CAN. (5 TOTAL)
  - 3 REMOVE EXISTING CONDUCTORS BACK TO NEXT ADJACENT LIGHT FIXTURE OR HANDHOLE. CONDUIT TO REMAIN. (115 LF)
  - 4 EXCAVATE AND REMOVE EXISTING CONDUIT AND CONDUCTOR(S). (185 LF)
  - 5 REMOVE AND SALVAGE EXISTING AIRFIELD GUIDANCE SIGN AND ISOLATION TRANSFORMER. RETURN TO OWNER. REMOVE CONCRETE SIGN BASE. (2 TOTAL)
  - 6 REMOVE AND SALVAGE EXISTING AIRFIELD GUIDANCE SIGN AND ISOLATION TRANSFORMER. RETURN TO OWNER. CONCRETE SIGN BASE TO REMAIN. (1 TOTAL)

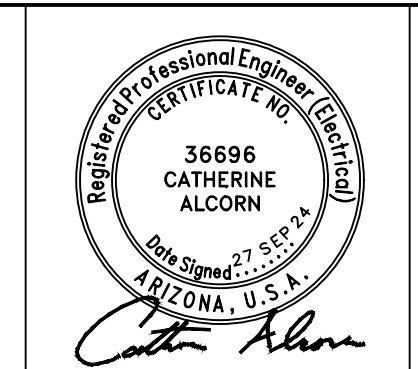
- REFERENCE NOTES**
- 1 EXISTING CONDUIT AND CONDUCTOR TO REMAIN - PROTECT IN PLACE
  - 2 EXISTING HANDHOLE/JUNCTION CAN TO REMAIN - PROTECT IN PLACE
  - 3 EXISTING AIRFIELD GUIDANCE SIGN TO REMAIN - PROTECT IN PLACE
  - 4 EXISTING EDGE LIGHT FIXTURE TO REMAIN - PROTECT IN PLACE
  - 6 EXISTING DUCTBANK TO REMAIN - PROTECT IN PLACE
  - 7 APPROXIMATE SPLICE POINT OF EXISTING CONDUIT AND CONDUCTOR

- CIRCUIT IDENTIFICATION**
- 1 TAXIWAY B
  - 2 TAXIWAY C
  - 3 RW 4R-22L
  - 4 RW 4L-22R



REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: RD  
 DRAWN BY: JBW  
 SHEET CHK'D BY: CA  
 CROSS CHK'D BY: SW  
 APPROVED BY: CA  
 DATE: 09/27/2024



**CR engineers**  
 16719 East Palisades Blvd., Suite 202  
 Fountain Hills, AZ 85268  
 Phone: (480) 816-5541  
 Fax: (480) 816-5540

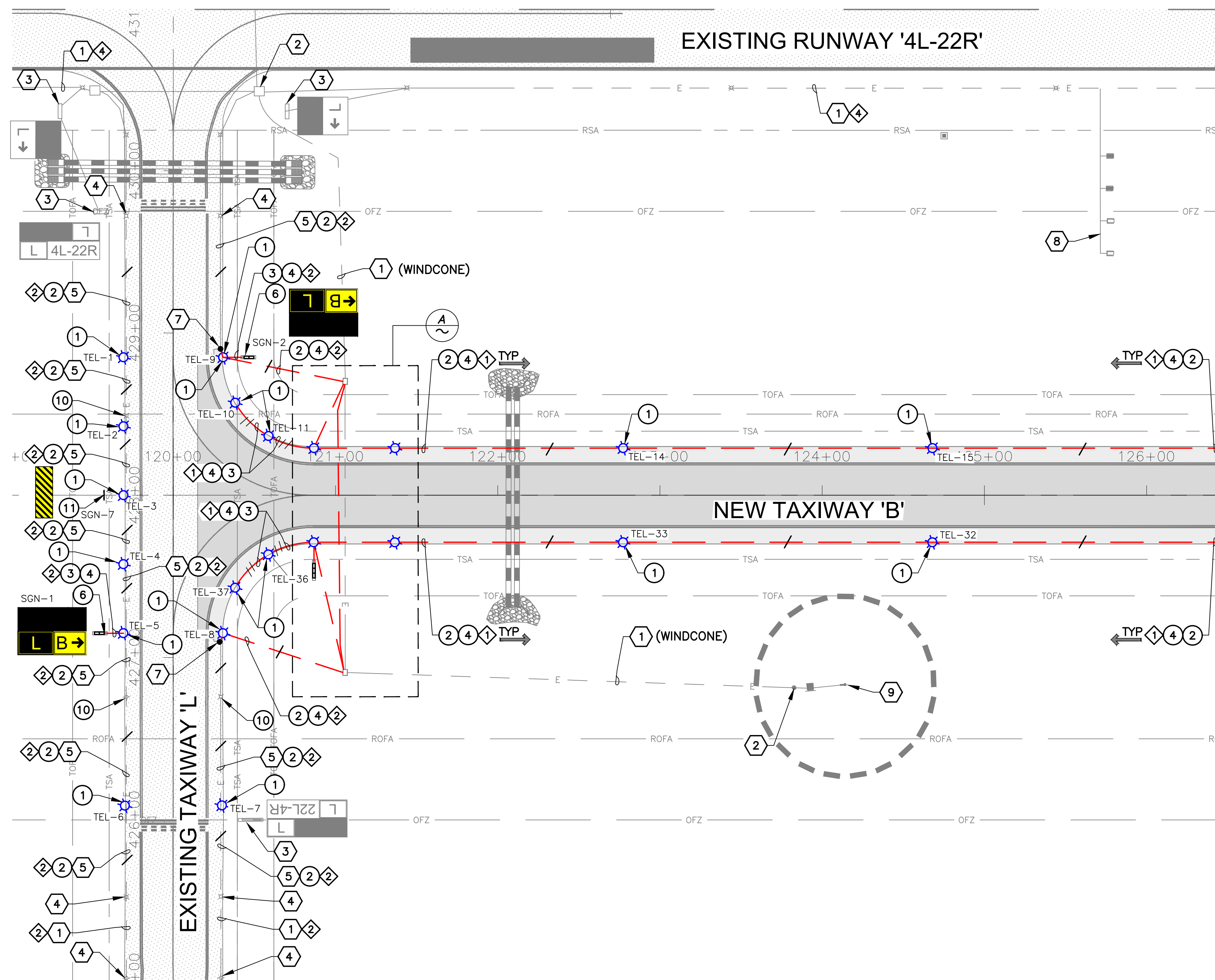
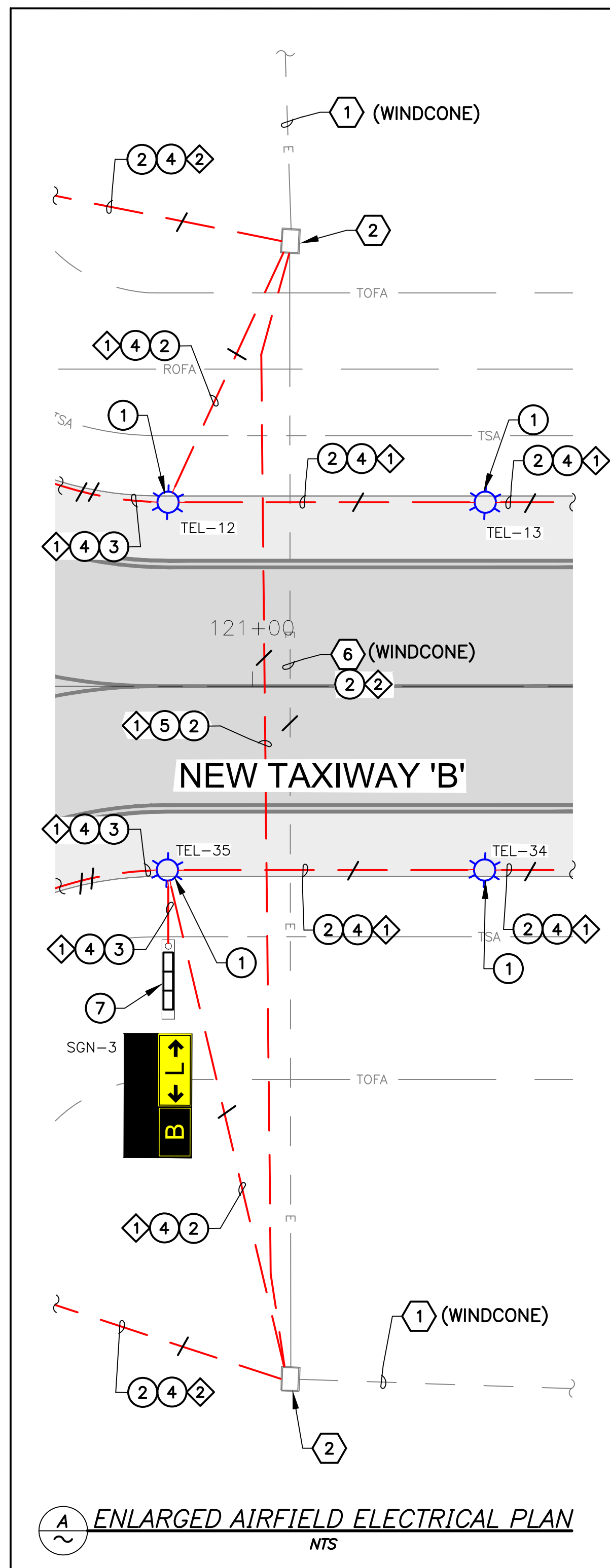


CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
**TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'**

**AIRFIELD ELECTRICAL  
 DEMOLITION PLAN**

PROJECT NO. 1021015.11  
 FILE NAME: 23011 E2.0.DWG  
 DWG NO.  
**E2.1**  
 SHEET 24 OF 39

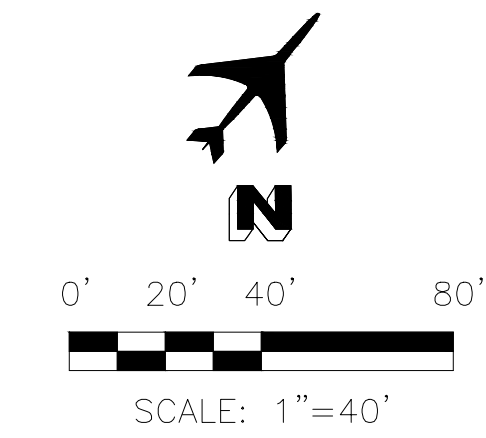
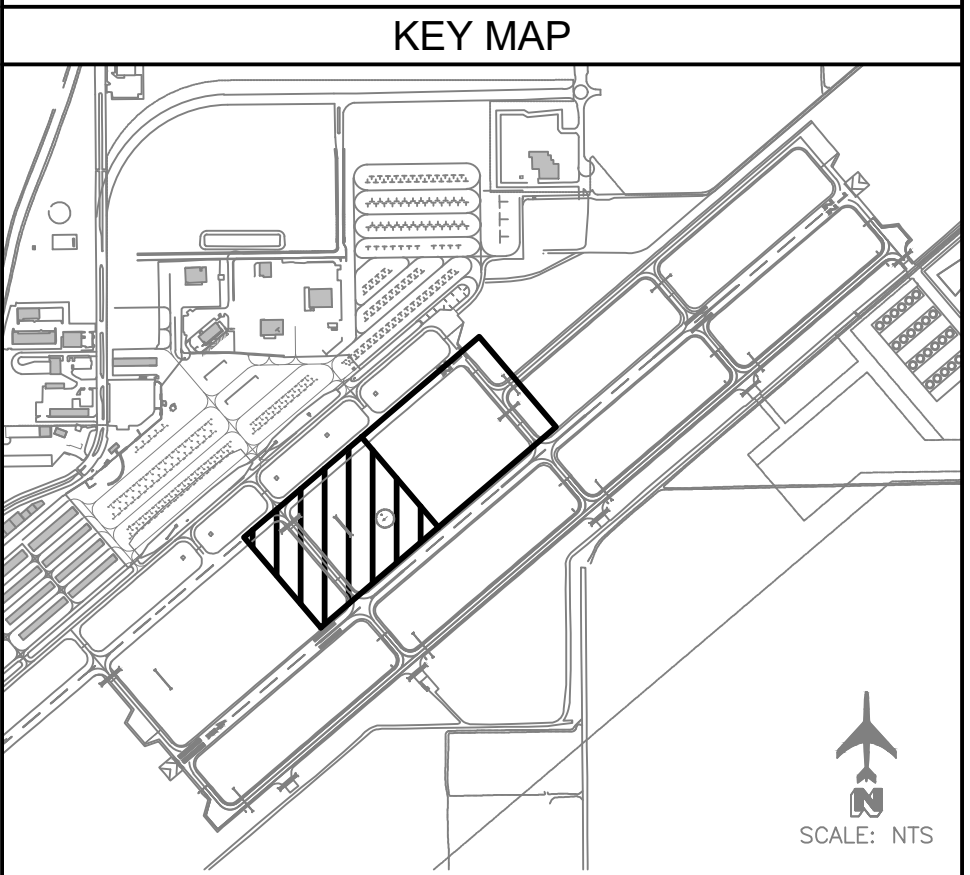




MATCH LINE 126+50 SEE SHEET E3.1

- CONSTRUCTION NOTES**
- 1 NEW L-861T QUARTZ TAXIWAY EDGE LIGHT AND ISOLATION TRANSFORMER ON NEW L-867 BASE (21 TOTAL)
  - 2 NEW 1/C, #8-5KV, TYPE C AIRFIELD LIGHTING CABLE (2,660 LF)
  - 3 NEW 2/C, #8-5KV, TYPE C AIRFIELD LIGHTING CABLE (281 LF)
  - 4 1-2" CONDUIT DIRECT BURIED (1,704 LF)
  - 5 2-2" CONDUIT CONCRETE ENCASED (180 LF)
  - 6 NEW L-858(L) LED SIZE 2, STYLE 3, CLASS 2, 2-MODULE AIRFIELD GUIDANCE SIGN ON NEW CONCRETE SIGN BASE. (2 TOTAL)
  - 7 NEW L-858(L) LED SIZE 2, STYLE 3, CLASS 2, 3-MODULE AIRFIELD GUIDANCE SIGN ON NEW CONCRETE SIGN BASE. (1 TOTAL)
  - 10 INSTALL NEW STEEL BLANK COVER ON EXISTING L-867 BASE CAN WITH NEW BOLTS. (3 TOTAL)
  - 11 NEW SIZE 2 UNLIGHTED SIGN ON NEW CONCRETE BASE (1 TOTAL)

- REFERENCE NOTES**
- 1 EXISTING CONDUIT AND CONDUCTOR
  - 2 EXISTING HANDHOLE/JUNCTION CAN
  - 3 EXISTING AIRFIELD GUIDANCE SIGN
  - 4 EXISTING EDGE LIGHT FIXTURE
  - 5 EXISTING CONDUIT
  - 6 EXISTING DUCTBANK
  - 7 APPROXIMATE SPLICE POINT OF EXISTING CONDUIT AND CONDUCTOR
  - 8 EXISTING PAPI
  - 9 EXISTING WINDCONE



- CIRCUIT IDENTIFICATION**
- ◇ TAXIWAY B
  - ◇ TAXIWAY C
  - ◇ RW 4R-22L
  - ◇ RW 4L-22R



REV. NO.	DATE	DRWN	CHKD	REMARKS

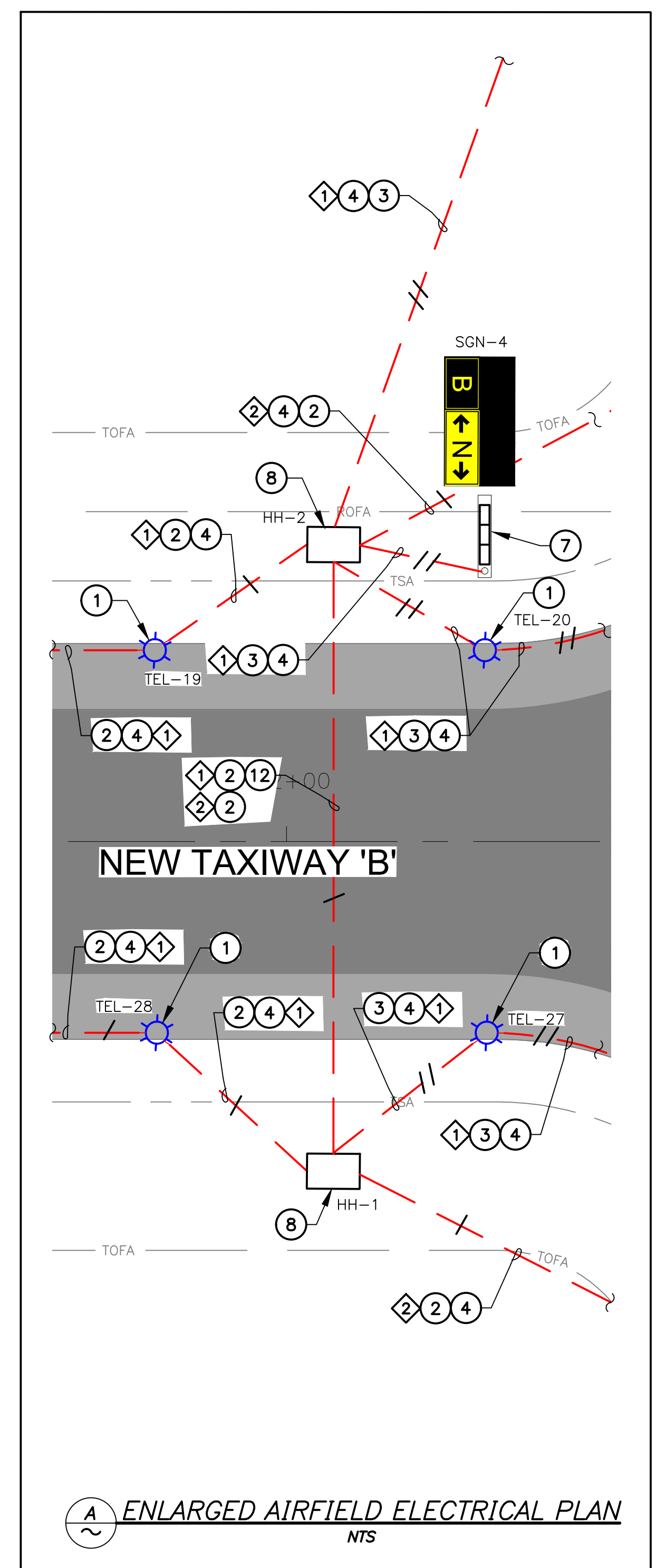
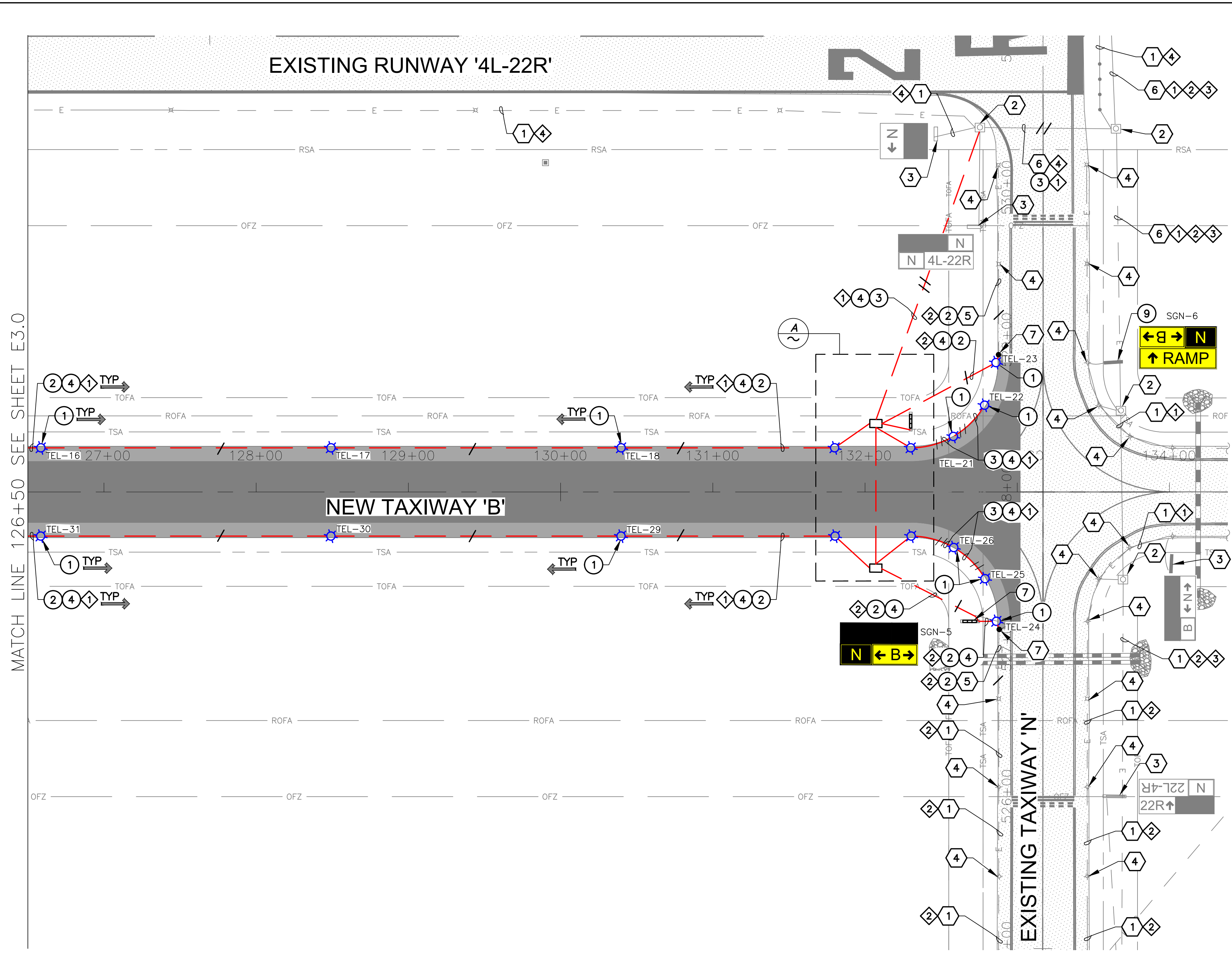
DESIGNED BY: RD  
 DRAWN BY: JBW  
 SHEET CHK'D BY: CA  
 CROSS CHK'D BY: SW  
 APPROVED BY: CA  
 DATE: 09/27/2024



CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
**TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'**

**AIRFIELD ELECTRICAL PLAN**

PROJECT NO. 1021015.11  
 FILE NAME: 23011 E3.0.DWG  
 DWG NO.  
**E3.0**  
 SHEET 25 OF 39



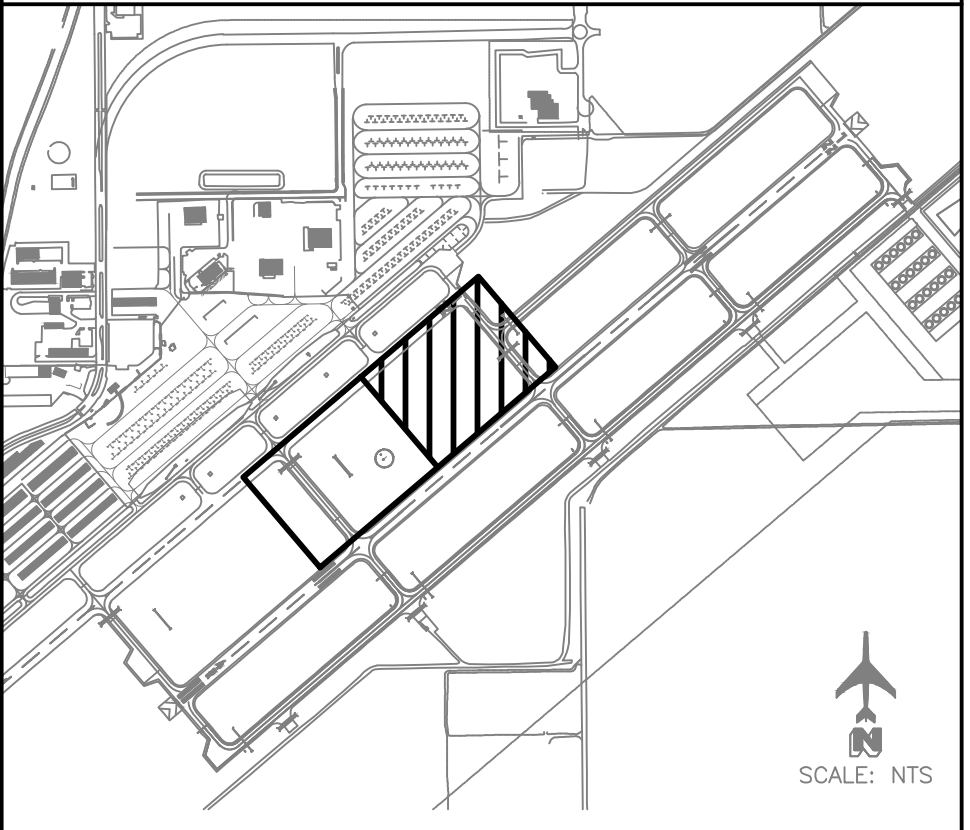
CONSTRUCTION NOTES

- 1 NEW L-861T QUARTZ TAXIWAY EDGE LIGHT AND ISOLATION TRANSFORMER ON NEW L-867 BASE (16 TOTAL)
- 2 NEW 1/C, #8-5KV, TYPE C AIRFIELD LIGHTING CABLE (1,865 LF)
- 3 NEW 2/C, #8-5KV, TYPE C AIRFIELD LIGHTING CABLE (660 LF)
- 4 1-2" CONDUIT DIRECT BURIED (1,720 LF)
- 7 NEW L-858(L) LED SIZE 2, STYLE 3, CLASS 2, 3-MODULE AIRFIELD GUIDANCE SIGN ON NEW CONCRETE SIGN BASE. (2 TOTAL)
- 8 NEW 2'x3'x3' HAND HOLE WITH AIRCRAFT-RATED LID WITH SPRING ASSISTED OPENING. (2 TOTAL)
- 9 NEW L-858(L) LED SIZE 2, STYLE 3, CLASS 2, 3-MODULE AIRFIELD GUIDANCE SIGN ON EXISTING SIGN BASE. (1 TOTAL)
- 12 4-2" CONDUIT CONCRETE ENCASED (95 LF)

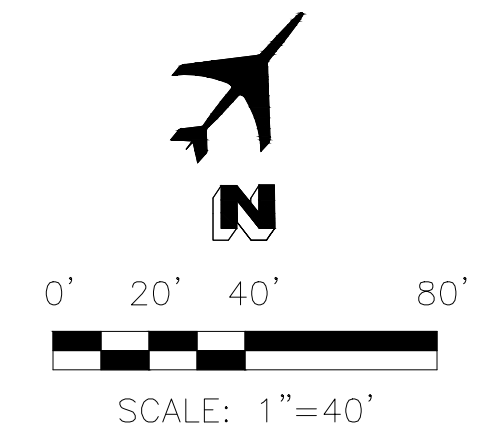
REFERENCE NOTES

- 1 EXISTING CONDUIT AND CONDUCTOR
- 2 EXISTING HANDHOLE/JUNCTION CAN
- 3 EXISTING AIRFIELD GUIDANCE SIGN
- 4 EXISTING EDGE LIGHT FIXTURE
- 5 EXISTING CONDUIT
- 6 EXISTING DUCTBANK
- 7 APPROXIMATE SPLICE POINT OF EXISTING CONDUIT AND CONDUCTOR

KEY MAP



MATCH LINE 126+50 SEE SHEET E3.0

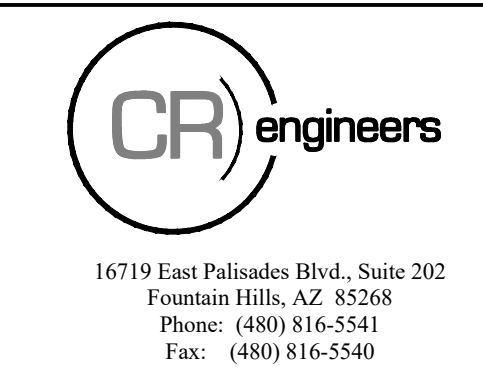
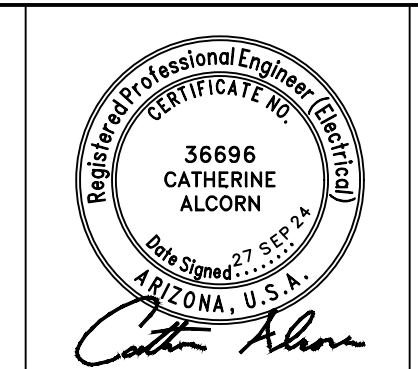


- ◇ CIRCUIT IDENTIFICATION ◇
- 1 TAXIWAY B
  - 2 TAXIWAY C
  - 3 RW 4R-22L
  - 4 RW 4L-22R



REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: RD  
 DRAWN BY: JBW  
 SHEET CHK'D BY: CA  
 CROSS CHK'D BY: SW  
 APPROVED BY: CA  
 DATE: 09/27/2024



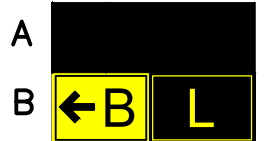
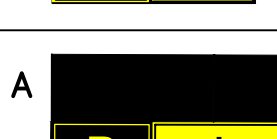

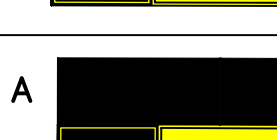

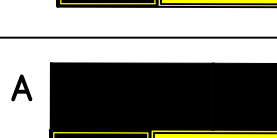

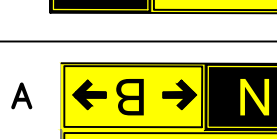
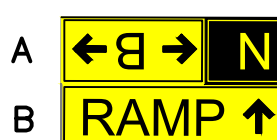
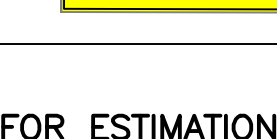


CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

AIRFIELD ELECTRICAL PLAN



PROJECT NO. 1021015.11  
 FILE NAME: 23011 E3.0.DWG  
 DWG NO.  
**E3.1**  
 SHEET 26 OF 39


CHANDLER MUNICIPAL AIRPORT GUIDANCE SIGNAGE SCHEDULE

SIGN NUMBER	CIRCUIT	SIGN WITH FACE DESIGNATIONS	FACE A MESSAGE	FACE B MESSAGE	FACE A COLOR	FACE B COLOR	SIZE	STYLE	CLASS	SHEET NUMBER	SCOPE OF WORK
SGN-1	TW C	A  B 	BLANK	L   B →	B	Y/B   B/Y	2	3	2	E3.0	NEW L-858(L) LED SIZE 2, 2-MODULE AIRFIELD GUIDANCE SIGN AND ISOLATION TRANSFORMER ON NEW SIGN BASE
SGN-2	TW C	A  B 	BLANK	← B   A	B	B/Y   Y/B	2	3	2	E3.0	NEW L-858(L) LED SIZE 2, 2-MODULE AIRFIELD GUIDANCE SIGN AND ISOLATION TRANSFORMER ON NEW SIGN BASE
SGN-3	TW B	A  B 	BLANK	B   ← L →	B	Y/B   B/Y   B/Y	2	3	2	E3.0	NEW L-858(L) LED SIZE 2, 3-MODULE AIRFIELD GUIDANCE SIGN AND ISOLATION TRANSFORMER ON NEW SIGN BASE
SGN-4	TW B	A  B 	BLANK	B   ← N →	B	Y/B   B/Y   B/Y	2	3	2	E3.1	NEW L-858(L) LED SIZE 2, 3-MODULE AIRFIELD GUIDANCE SIGN AND ISOLATION TRANSFORMER ON NEW SIGN BASE
SGN-5	TW C	A  B 	BLANK	N ← B →	B	Y/B   B/Y   B/Y	2	3	2	E3.1	NEW L-858(L) LED SIZE 2, 3-MODULE AIRFIELD GUIDANCE SIGN AND ISOLATION TRANSFORMER ON NEW SIGN BASE
SGN-6	TW C	A  B 	N   ← B →	RA   MP   ↑	Y/B   B/Y   B/Y	B/Y   B/Y   B/Y	2	3	2	E3.1	NEW L-858(L) LED SIZE 2, 3-MODULE AIRFIELD GUIDANCE SIGN AND ISOLATION TRANSFORMER ON EXISTING SIGN BASE

1. MODULE SIZES ARE GIVEN FOR ESTIMATION ONLY AND ARE SUBJECT TO CHANGE BY SIGN MANUFACTURER
2. CONTRACTOR SHALL PERFORM AND SUBMIT COMPLETE FIELD SURVEY/RECORD DRAWINGS FOR ALL EQUIPMENT PRIOR TO FINAL PAYMENT.

CHANDLER MUNICIPAL AIRPORT AIRFIELD GUIDANCE SIGN SCHEDULE - UNLIGHTED SIGN

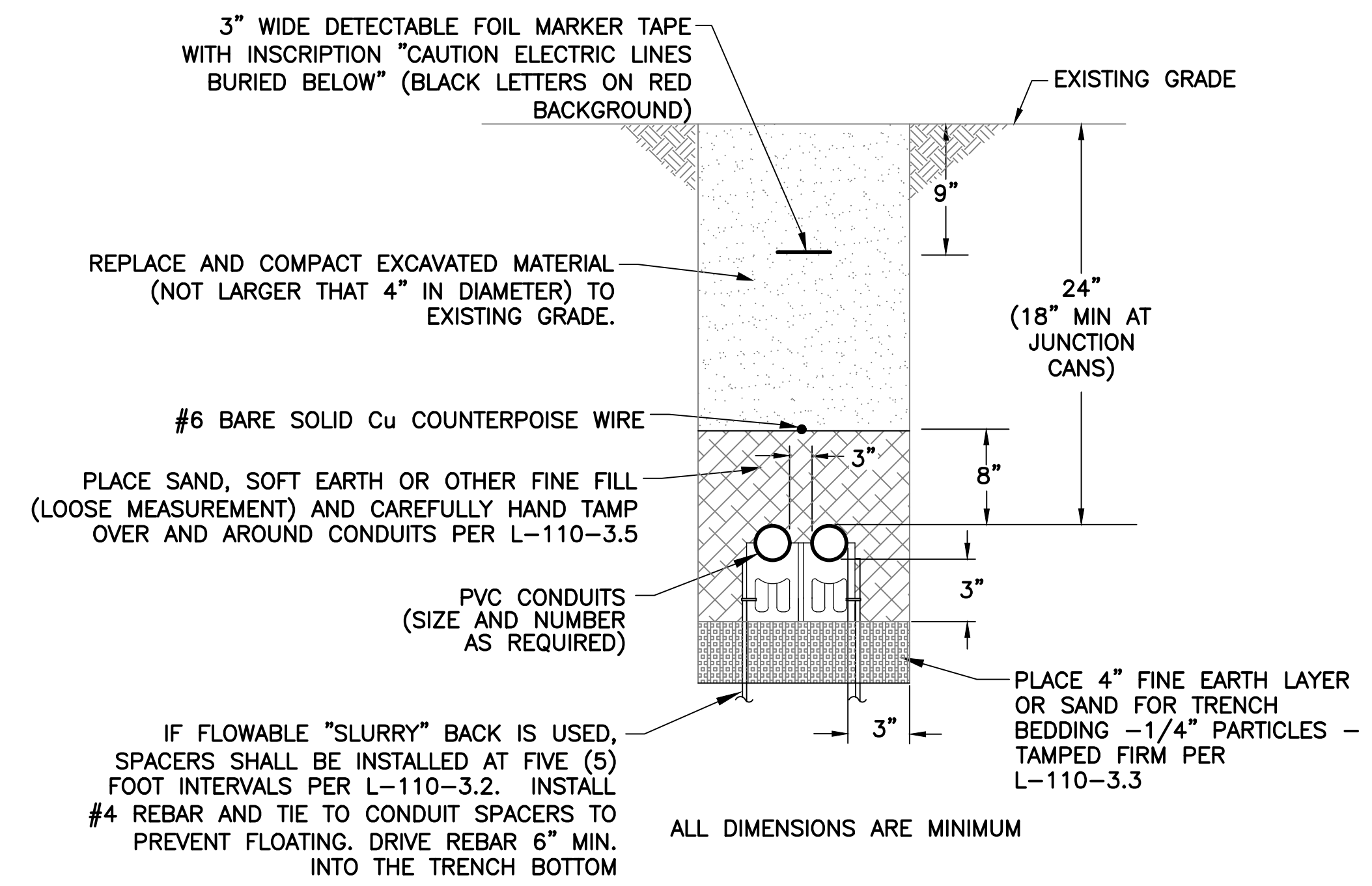
SIGN NUMBER	CIRCUIT	SIGN	MESSAGE	COLOR	SIZE	STYLE	MODE	SHEET NUMBER	SCOPE OF WORK
SGN-7	N/A			B/Y	2	4	1	E3.0	NEW SIZE 2 UNLIGHTED AIRFIELD GUIDANCE SIGN ON NEW BASE.

DESIGNED BY: RD				CITY OF CHANDLER, ARIZONA CHANDLER MUNICIPAL AIRPORT TAXIWAY 'B' IMPROVEMENTS PHASE 1: TAXIWAY 'L' TO TAXIWAY 'N'	AIRFIELD GUIDANCE SIGNAGE SCHEDULE	PROJECT NO. 1021015.11 FILE NAME: 23011 E4.0.DWG DWG NO. <b>E4.0</b> SHEET 27 OF 39
DRAWN BY: JBW						
SHEET CHK'D BY: CA						
CROSS CHK'D BY: SW						
APPROVED BY: CA						
DATE: 09/27/2024	16719 East Palisades Blvd., Suite 202 Fountain Hills, AZ 85268 Phone: (480) 816-5541 Fax: (480) 816-5540	C.O.C. LOG NO. 24-0054, C.O.C. PROJECT NO. AI2302.401				

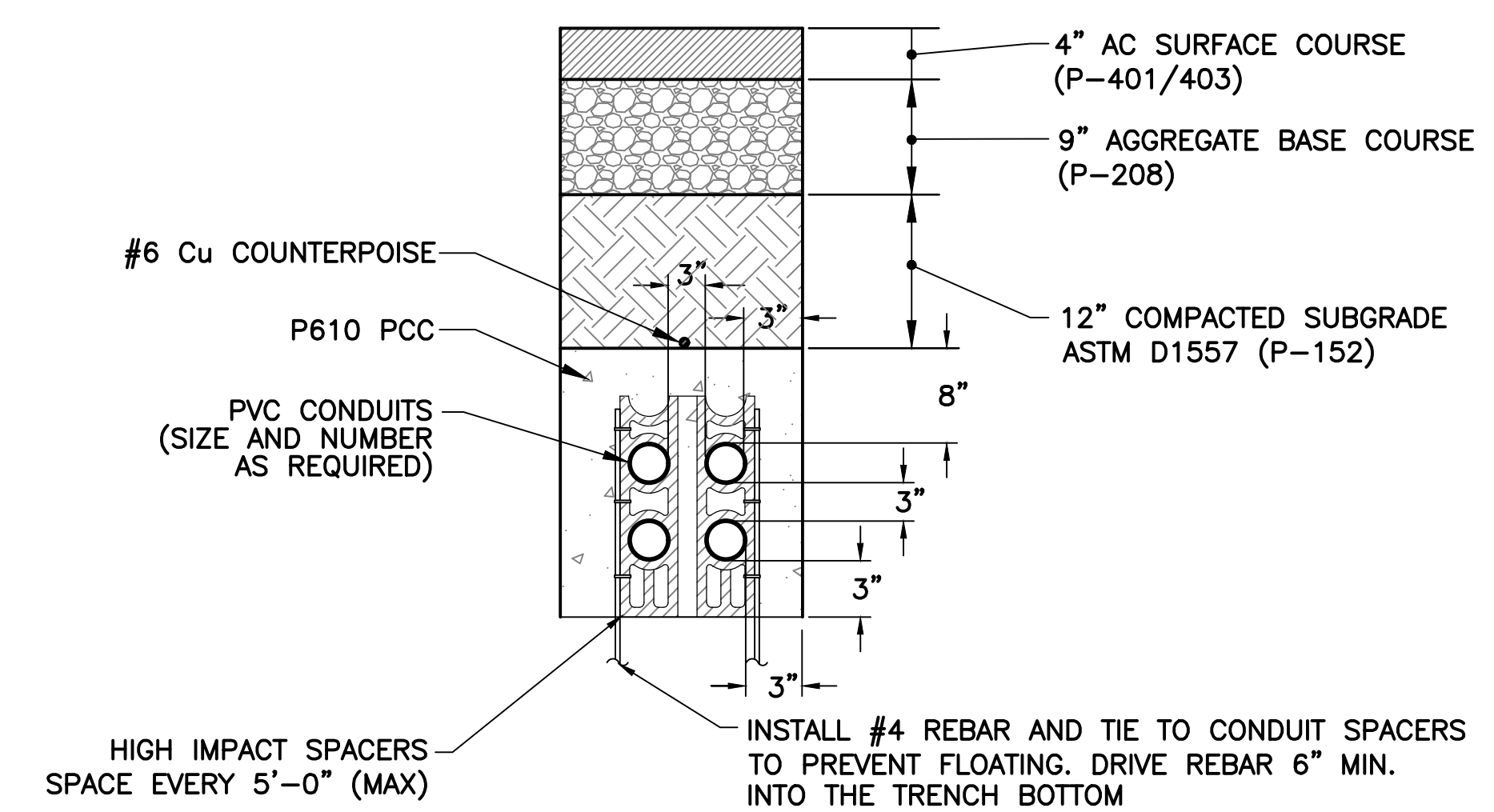
REV. NO.	DATE	DRWN	CHKD	REMARKS

**GENERAL NOTES FOR CONDUIT INSTALLATION**

- CONCRETE ENCASED DUCTBANKS SHALL BE USED WHEN CROSSING UNDER PAVEMENTS EXPECTED TO CARRY AIRCRAFT LOADS SUCH AS RUNWAYS, TAXIWAYS, TAXI-LANES, RAMPS, AND APRONS OR VEHICULAR TRAFFIC SUCH AS SERVICE ROADS. WHEN UNDER PAVED SHOULDERS AND OTHER PAVED AREAS, CONDUIT AND DUCTBANKS SHALL BE ENCASED USING FLOWABLE FILL FOR PROTECTION.
- UNDER PAVEMENT, THE TOP OF THE DUCTBANKS SHALL NOT BE LESS THAN 18 INCHES BELOW THE SUBGRADE; IN OTHER LOCATION THE TOP OF THE DUCTBANK OR UNDERGROUND CONDUIT SHALL BE NOT LESS THAN 18 INCHES BELOW FINISHED GRADE.
- DUCTBANKS SHALL BE INSTALLED SO THAT THE TOP OF THE CONCRETE ENVELOPE IS NOT LESS THAN 18 INCHES BELOW THE BOTTOM OF THE BASE OR STABILIZED BASE COURSE LAYERS WHERE INSTALLED UNDER RUNWAY, TAXIWAYS, APRONS, OR OTHER PAVED AREAS, AND NOT LESS THAN 18 INCHES BELOW FINISHED GRADE WHERE INSTALLED IN UNPAVED AREAS.
- ALL DUCTBANKS SHALL BE PLACED ON A LAYER OF CONCRETE NOT LESS THAN 3 INCHES THICK PRIOR TO ITS INITIAL SET. THE CONTRACTOR SHALL SPACE THE CONDUITS NOT LESS THAN 3 INCHES APART (MEASURED FROM OUTSIDE WALL TO OUTSIDE WALL).
- ALL SUCH MULTIPLE CONDUITS SHALL BE PLACED USING CONDUIT SPACERS, APPLICABLE TO THE TYPE OF CONDUIT. CONCRETE SHALL BE PLACED AROUND AND ON TOP OF THE CONDUITS NOT LESS THAN 3 INCHES THICK.
- CONDUIT FORMING THE DUCTBANK SHALL BE INSTALLED USING CONDUIT SPACERS. #4 REINFORCING BARS SHALL BE DRIVEN VERTICALLY INTO THE SOIL A MINIMUM OF 6 INCHES TO ANCHOR THE ASSEMBLY INTO THE EARTH PRIOR TO PLACING THE CONCRETE ENCASEMENT. SPACERS SHALL BE INSTALLED AT 5 FEET INTERVALS.
- ALL PAVEMENT SURFACES THAT ARE TO HAVE DUCTS INSTALLED THEREIN SHALL BE NEATLY SAW CUT TO FORM VERTICAL FACE. ALL EXCAVATION AND BACKFILL SHALL BE INCLUDED IN THE CONTRACT WITH PRICE FOR THE DUCT.
- BARE SOLID #6 AWG COPPER COUNTERPOISE WIRE SHALL BE INSTALLED FOR LIGHTNING PROTECTION OF THE UNDERGROUND CABLES. THE COUNTERPOISE CONDUCTOR SHALL BE INSTALLED NO LESS THAN 8 INCHES MINIMUM OR 12 INCHES MAXIMUM ABOVE THE RACEWAY OR CABLE TO BE PROTECTED, EXCEPT AS PERMITTED BELOW:
  - THE MINIMUM COUNTERPOISE CONDUCTOR HEIGHT ABOVE THE RACEWAY OR CABLE TO BE PROTECTED SHALL BE PERMITTED TO BE ADJUSTED SUBJECT TO COORDINATION WITH AIRFIELD LIGHTING AND PAVEMENT DESIGNS.
  - THE COUNTERPOISE CONDUCTOR HEIGHT ABOVE THE PROTECTED RACEWAY(S) OR CABLE(S) SHALL BE CALCULATED TO ENSURE THAT THE RACEWAY OR CABLE IS WITHIN A 45-DEGREE AREA OF PROTECTION (45 DEGREES ON EACH SIDE OF VERTICAL CREATING A 90-DEGREE ANGLE).
- INSTALL A PLASTIC, DETECTABLE, RED, 3 INCH WIDE TAPE LOCATED 8 INCHES (MINIMUM) BELOW GRADE ABOVE ALL UNDERGROUND CONDUIT OR DUCT LINES NOT INSTALLED UNDER PAVEMENT. UNDERGROUND ELECTRICAL WARNING (CAUTION) TAPE SHALL BE INSTALLED IN THE TRENCH ABOVE ALL UNDERGROUND DUCTBANKS AND CONDUIT IN UNPAVED AREAS. THE WARNING TAPE SHALL BE LOCATED 6 INCHES ABOVE THE DUCT/CONDUIT OR THE COUNTERPOISE WIRE IF PRESENT.
- PROVIDE MULE TAPE IN ALL NEW (UNUSED) CONDUITS. PLUG ENDS IN HANDHOLES
- P-610 CONCRETE ENCASED CONDUIT UNDER PAVEMENT PER SPECIFICATION L-110.
- INSTALL A #6 BARE COPPER COUNTERPOISE ABOVE EACH DUCT ASSEMBLY FROM HANDHOLE TO HANDHOLE AND EXOTHERMICALLY WELD TO GROUND RODS AT EACH HANDHOLE.
- INSTALL LIGHTING SERIES CIRCUITS AS FOLLOWS:
  - ONE CIRCUIT (1 OR 2 CONDUCTORS) PER 2°C. LIMIT 4°C TO NO MORE THAN EIGHT (8) CONDUCTORS.
  - START INSTALLATION IN BOTTOM CONDUITS OF DUCT ARRAY, LEAVING UPPER CONDUITS EMPTY.
- ALL UNDERGROUND CONDUITS SHALL MAINTAIN A 12" (MIN.) SEPARATION FROM ALL OTHER (EXISTING OR NEW) UNDERGROUND FACILITIES UNLESS NOTED OTHERWISE ON DRAWINGS
- CONDUIT IN DUCTBANK(S) ARE TO BE STACKED NO MORE THAN FOUR (4) CONDUITS. IF MORE CONDUITS ARE NEEDED, THE WIDTH OF THE TRENCH IS TO BE INCREASED
- WHERE PAVEMENT IS EXISTING AND REPAIR/REPLACEMENT IS REQUIRED, REPLACE IN KIND. ANTICIPATE THREE (3) INCHES OF ASPHALT ON CLSM SLURRY ENCASED DUCT SECTION. THIS SHALL BE INCIDENTAL TO THE COST OF RETROFIT DUCTBANK INSTALLATION.
- ADJUST DEPTH TO 18"-24" TOTAL FOR CONDUIT INSTALLATION BETWEEN OR INTO L-867/L-868 BASE CAN.
- INSTALL ONLY ONE CIRCUIT PER CONDUIT.
- WHERE DUCTBANK CONTAINS BOTH AIRFIELD LIGHTING CONDUIT AND COMMUNICATION CONDUITS, MAINTAIN 12" (MIN.) HORIZONTAL OR VERTICAL SEPARATION BETWEEN SYSTEMS.
- DUCTBANK MAY BE ADJUSTED UP TO A MINIMUM OF 18" BELOW FINISHED GRADE SUBJECT TO COORDINATION WITH AIRFIELD LIGHTING AND STORM DRAIN INFRASTRUCTURE WITHIN INFIELD AREAS.
- SUBJECT TO COORDINATION WITH AIRFIELD LIGHTING AND PAVEMENT DESIGNS, THE P-610 OR CLSM PLACED AROUND AND ON TOP OF THE CONDUITS MAY BE ADJUSTED TO NOT LESS THAN 3" THICK.



501 DIRECT EARTH BURIED (DB) CONDUIT DUCTBANK  
NTS

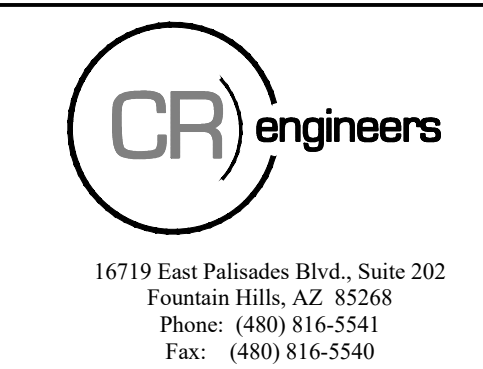
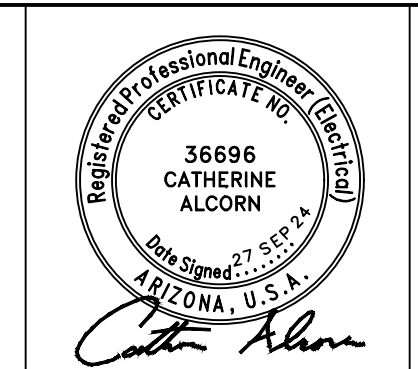


501 CONCRETE ENCASED (CE) CONDUIT DUCTBANK  
NTS

REFER TO CIVIL PLANS FOR PAVEMENT SECTIONS

REV. NO.	DATE	DRWN	CHKD	REMARKS

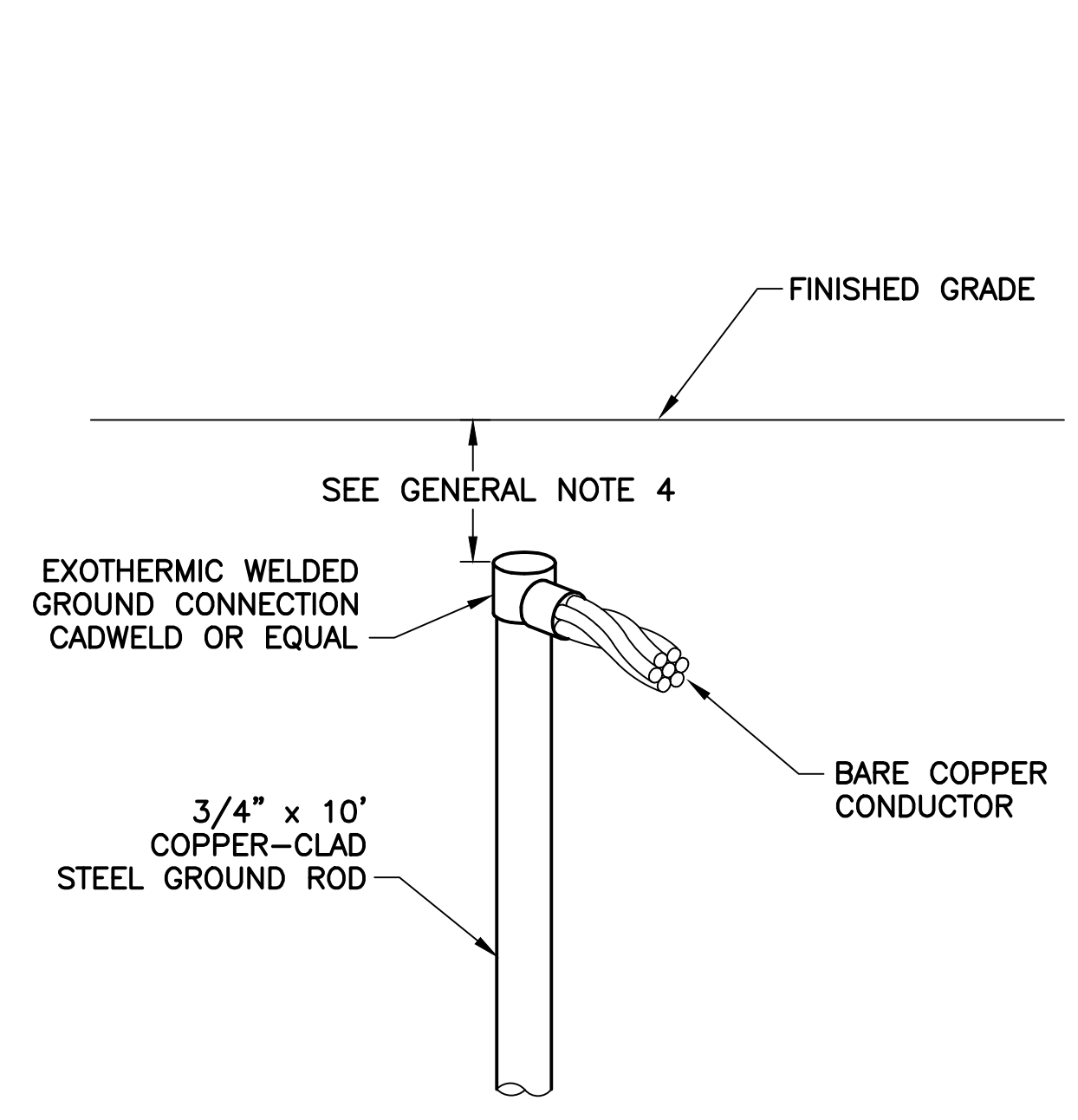
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 DRAWN BY: JBW  
 SHEET CHK'D BY: CA  
 CROSS CHK'D BY: SW  
 APPROVED BY: CA  
 DATE: 09/27/2024



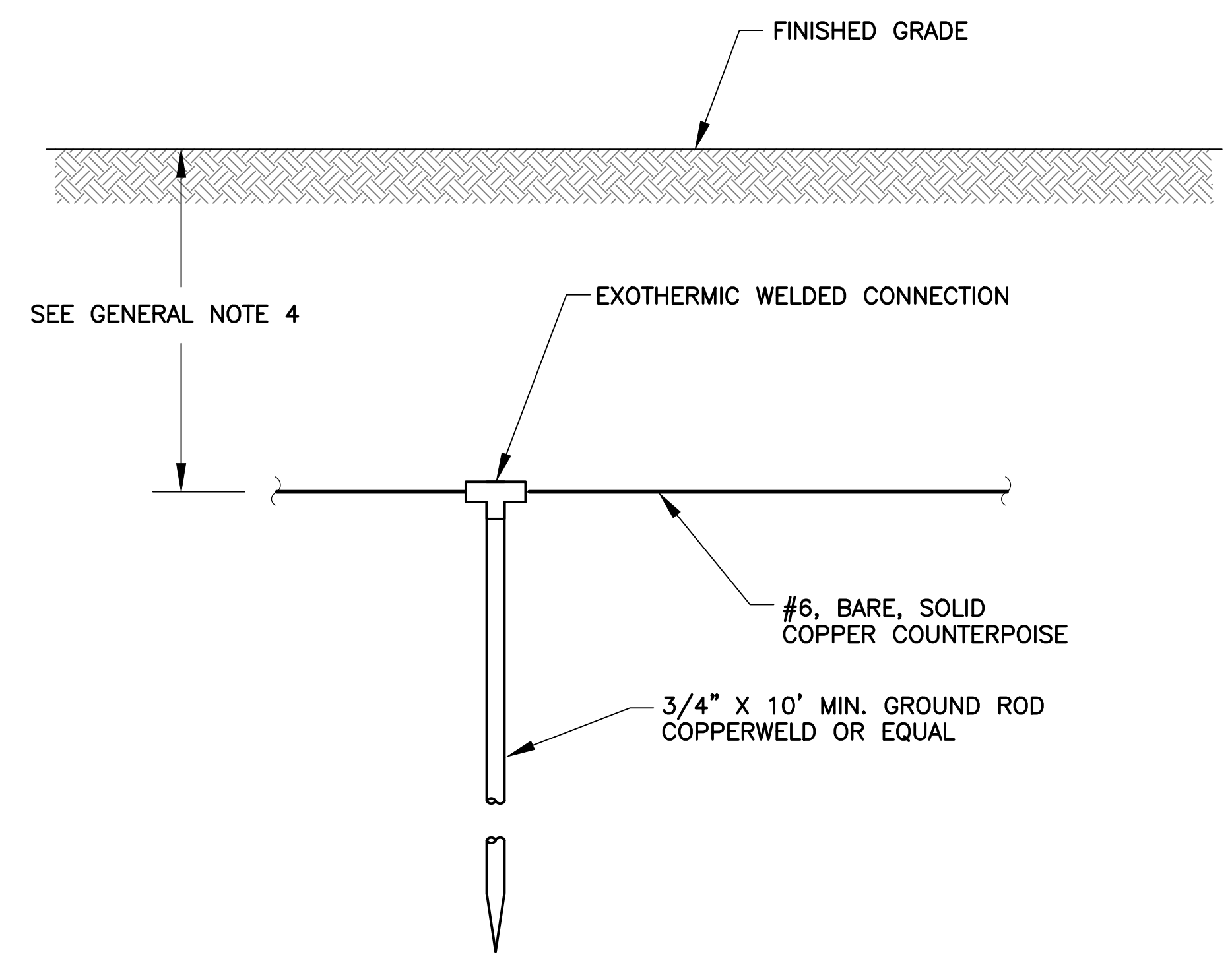
CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

AIRFIELD ELECTRICAL  
 DUCTBANK DETAILS

PROJECT NO. 1021015.11  
 FILE NAME: 23011 E5.0.DWG  
 DWG NO.  
**E5.0**  
 SHEET 28 OF 39



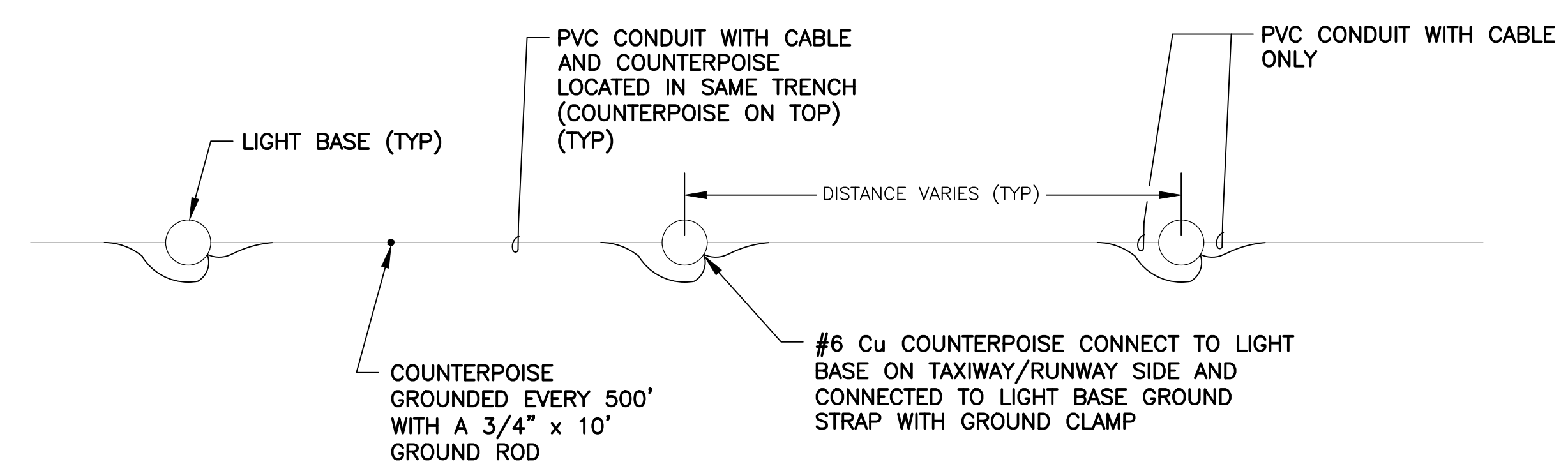
561 CABLE TO GROUND ROD CONNECTION  
TYP NTS



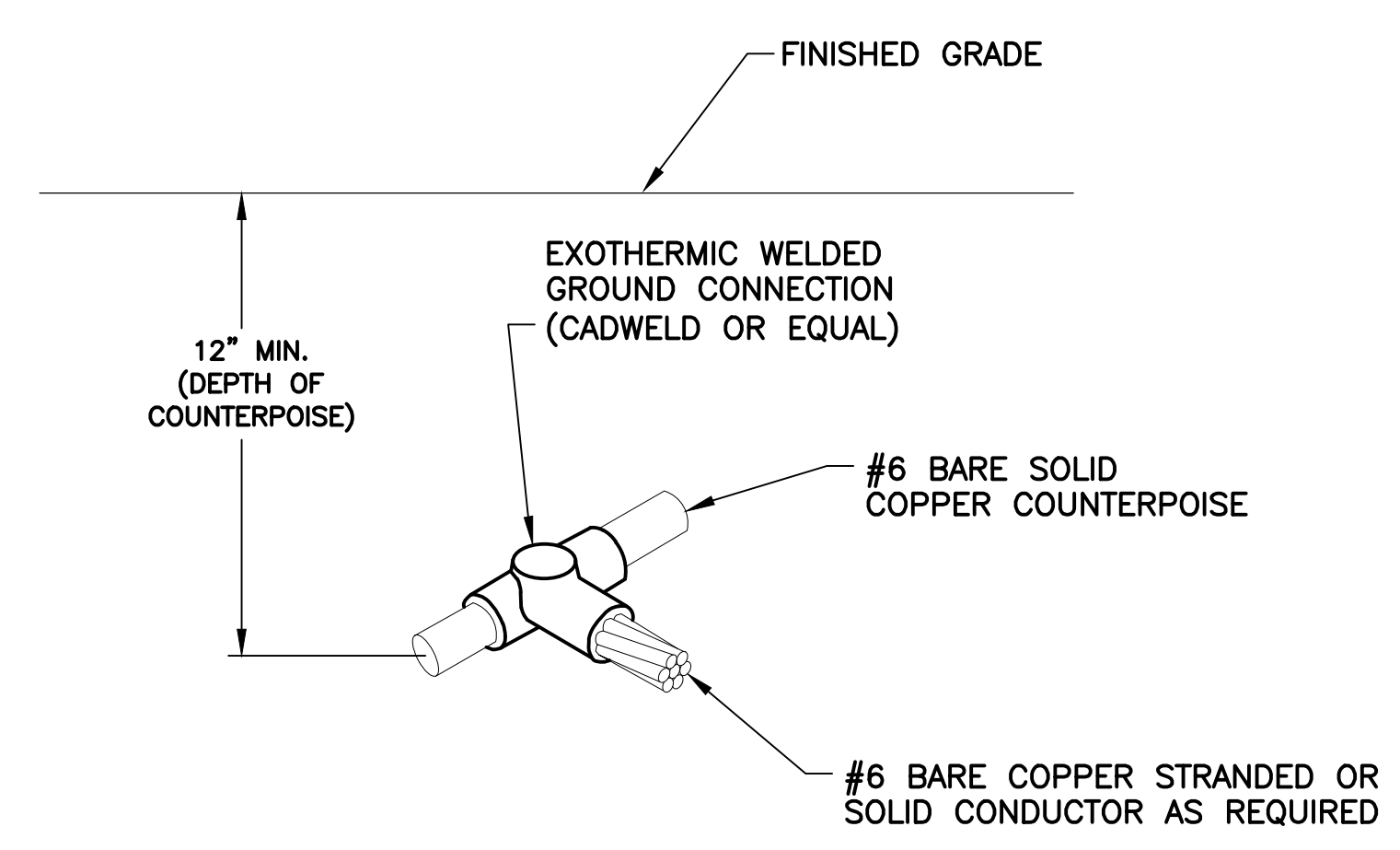
761 COUNTERPOISE INSTALLATION DETAIL  
TYP NTS

**GENERAL NOTES**

1. COST OF GROUND RODS IS INCIDENTAL TO THE ASSOCIATED ITEMS REQUIRING GROUNDING UNLESS OTHERWISE SPECIFIED.
2. WHERE POSSIBLE, NEW COUNTERPOISE SYSTEM SHALL BE CONNECTED TO ANY EXISTING COUNTERPOISE SYSTEM ENCOUNTERED.
3. GROUNDING ELECTRODES INSTALLED AS PART OF THE COUNTERPOISE SYSTEM SHALL BE SPACED AT DISTANCES NO GREATER THAN 500 FT. (MAX).
4. REFER TO CONDUIT/DUCT BANK DETAILS FOR DEPTHS.



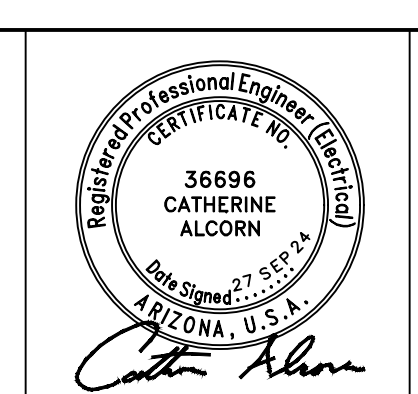
762 COUNTERPOISE AND GROUND ROD CONNECTIONS DETAIL  
TYP NTS



766 COUNTERPOISE TO CABLE CONNECTION DETAIL  
TYP NTS

REV. NO.	DATE	DRWN	CHKD	REMARKS

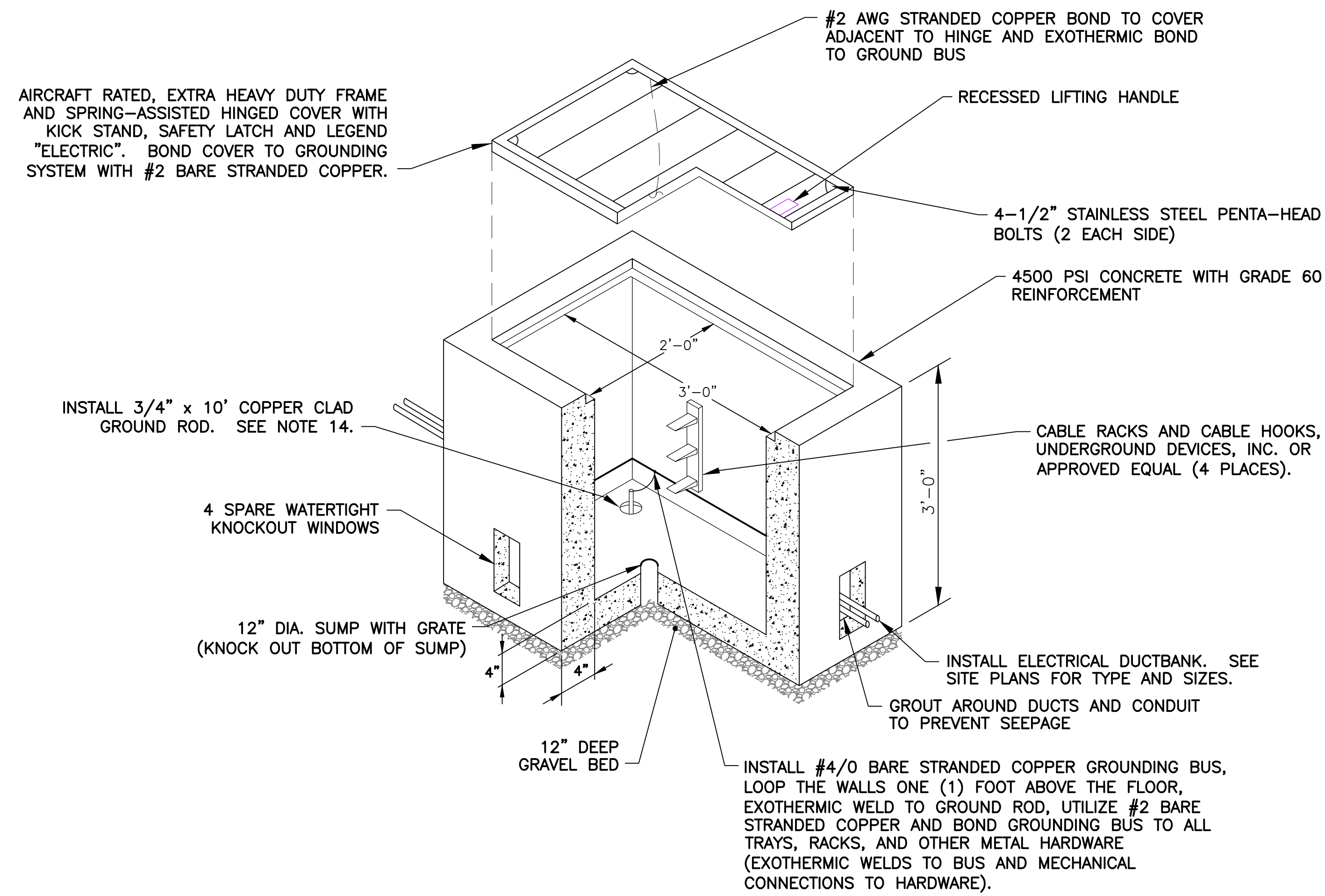
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 SHEET CHK'D BY: CA  
 CROSS CHK'D BY: SW  
 APPROVED BY: CA  
 DATE: 09/27/2024



CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

AIRFIELD ELECTRICAL  
 GROUNDING DETAILS

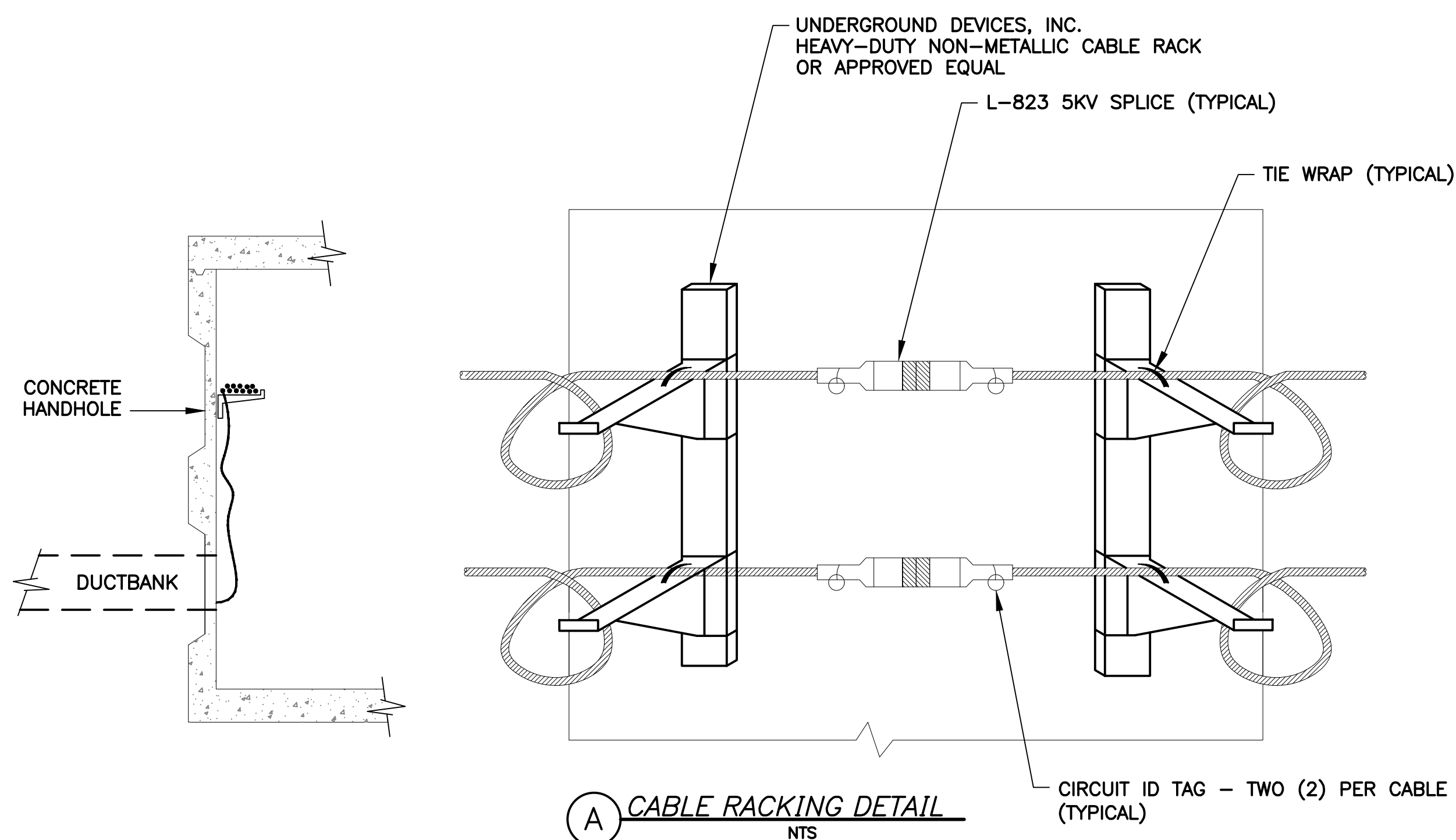
PROJECT NO. 1021015.11  
 FILE NAME: 23011 E5.0.DWG  
 DWG NO.  
**E5.1**  
 SHEET 29 OF 39



572 2' x 3' CONCRETE HANDHOLE DETAIL  
TYP NTS

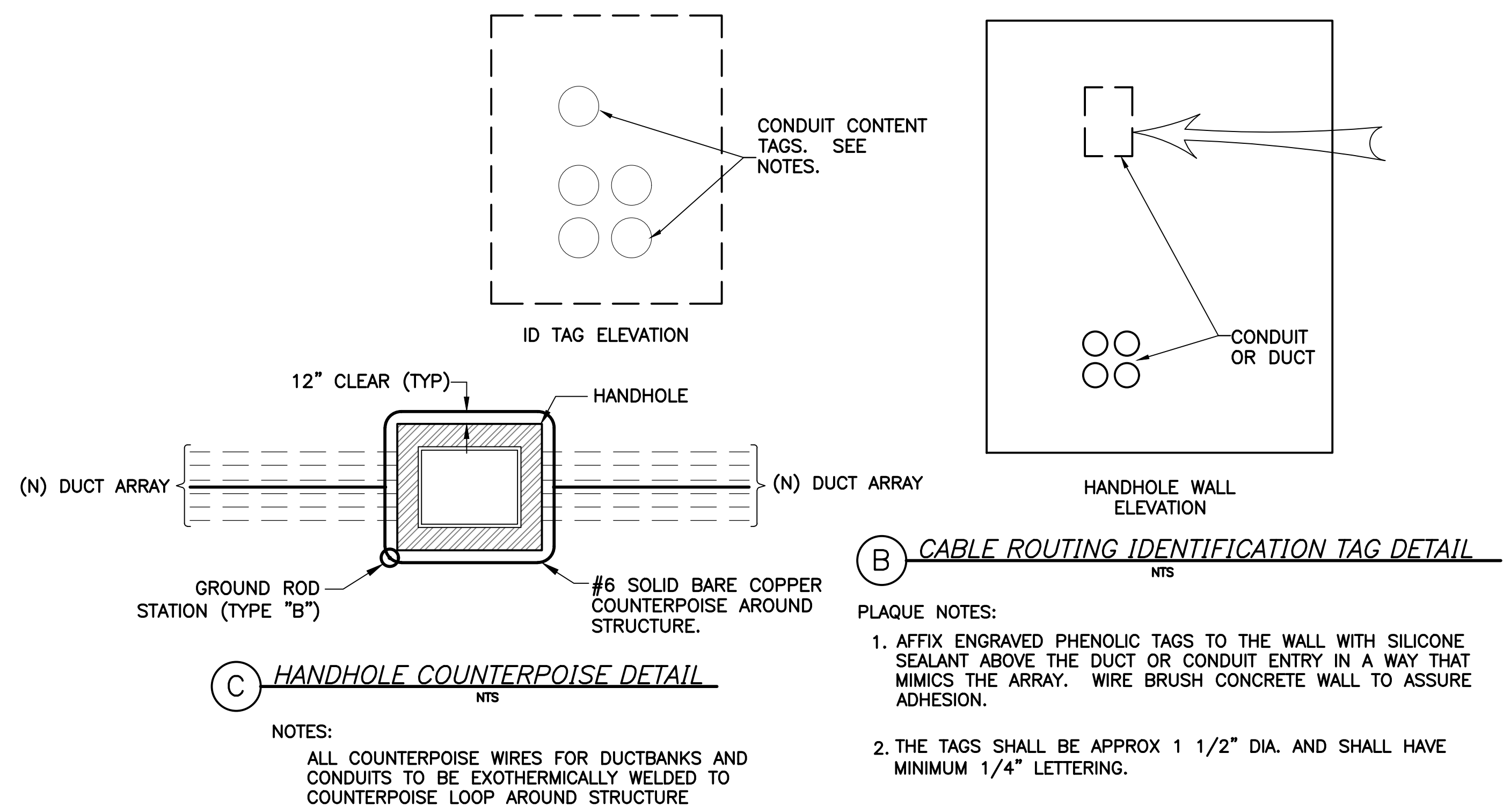
**GENERAL HANDHOLE INSTALLATION NOTES**

- PRE-CAST AIRCRAFT-RATED CONCRETE HANDHOLE AND DUCTILE IRON COVER SHALL BE DESIGNED FOR A 60 KIPS CONCENTRATED WHEEL LOAD OVER A 12" X 24" AREA, OLD CASTLE PRE-CAST CONCRETE PRODUCTS OR APPROVED EQUAL.
- ALL CABLE THROUGH HANDHOLE SHALL HAVE SUFFICIENT SLACK SO CABLE AND CONNECTOR CAN BE BROUGHT TO SURFACE FOR SPLICING, PROVIDE MINIMUM 15 FEET OF SLACK FOR EACH CABLE ENTERING HANDHOLE.
- AT GROUND ROD OPENING, DRIVE GROUND ROD INTO EARTH LEAVING SIX (6) INCHES EXTENDING ABOVE FLOOR. SEAL HOLE WATERTIGHT WITH PORTLAND CEMENT GROUT AROUND ROD. DO NOT INSTALL THROUGH DRAIN SUMP.
- SUBMIT LAYOUT OF EACH HANDHOLE FOR APPROVAL BY ENGINEER, INDICATING ELECTRICAL DUCT, CONDUIT AND SPARE STUBOUTS.
- PROVIDE HANDHOLES WITH WATERTIGHT SPARE DUCT SLOTS IN EACH EMPTY FACE.
- EXCAVATION AND BACKFILL NECESSARY FOR THE CONSTRUCTION OF THE HANDHOLE SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIFICATION REQUIREMENTS.
- PROVIDE END BELLS FINISHED FLUSH WITH INTERIOR WALLS AND NON-SHRINK GROUT AROUND EACH CONDUIT ENTERING THE HANDHOLE VIA A CONDUIT STUBOUT.
- COVER SHALL BE 1" MAXIMUM ABOVE FINISHED GRADE. AREA SURROUNDING HANDHOLE SHALL BE GRADED 10' IN ALL DIRECTIONS, SUCH TO PREVENT WATER AND DIRT ACCUMULATION ACROSS THE TOP OF THE COVER AND ALLOW WATER TO DRAIN AWAY FROM THE HANDHOLE - INCIDENTAL (NPI).
- BOND AND GROUND COVER AND FRAME TO GROUND BUS USING #2 AWG STRANDED COPPER WIRE.
- INSTALL PULLING IRONS IN EACH FACE.
- PROVIDE MANUFACTURER DRAWINGS AND STRUCTURAL CALCULATIONS FOR ALL CONCRETE ELEMENTS OF HANDHOLES AND FABRICATED AIRCRAFT-RATED COVERS.
- CONNECT COUNTERPOISE TO GROUND ROD OUTSIDE HANDHOLE. ROUTE AROUND HANDHOLES.
- A GROUND BUS OF 4/0 BARE COPPER SHALL BE EXOTHERMICALLY BONDED TO THE GROUND ROD AND LOOP THE CONCRETE STRUCTURE WALLS. THE GROUND BUS SHALL BE A MINIMUM OF ONE (1) FOOT (30 CM) ABOVE THE FLOOR OF THE STRUCTURE AND SEPARATE FROM OTHER CABLES. NO. 2 AMERICAN WIRE GAUGE (AWG) BARE COPPER PIG TAILS SHALL BOND THE GROUNDING BUS TO ALL CABLE TRAYS AND OTHER METAL HARDWARE WITHIN THE CONCRETE STRUCTURE. CONNECTIONS TO THE GROUNDING BUS SHALL BE EXOTHERMIC. IF AN EXOTHERMIC WELD IS NOT POSSIBLE, CONNECTION TO THE GROUNDING BUS SHALL BE MADE BY USING CONNECTOR APPROVED FOR DIRECT BURIAL IN SOIL OR CONCRETE PER UL 467. HARD CONNECTIONS MAY BE MECHANICAL USING A LUG DESIGNED FOR THAT PURPOSE.
- A GROUND ROD SHALL BE INSTALLED IN THE FLOOR OF ALL CONCRETE STRUCTURE SO THAT THE TOP OF ROD EXTENDS SIX (6) INCHES (150MM) ABOVE THE FLOOR. THE GROUND ROD SHALL BE INSTALLED WITHIN ONE (1) FOOT (30 CM) OF A CORNER OF THE CONCRETE STRUCTURE. GROUND RODS SHALL BE INSTALLED PRIOR TO CASTING THE BOTTOM SLAB. WHERE THE SOIL CONDITION DOES NOT PERMIT DRIVING THE GROUND ROD INTO THE EARTH WITHOUT DAMAGE TO THE GROUND ROD, THE CONTRACTOR SHALL DRILL A FOUR (4) INCH (100 MM) DIAMETER HOLE INTO THE EARTH TO RECEIVE THE GROUND ROD. THE HOLE AROUND THE GROUND ROD SHALL BE FILLED THROUGHOUT ITS LENGTH, BELOW SLAB, WITH PORTLAND CEMENT GROUT. GROUND RODS SHALL BE INSTALLED IN PRECAST BOTTOM SLAB OF STRUCTURES BY DRILLING A HOLE THROUGH BOTTOM SLAB AND INSTALLING THE GROUND ROD. BOTTOM SLAB PENETRATION SHALL BE SEALED WATERTIGHT WITH PORTLAND CEMENT GROUT AROUND THE GROUND ROD.



**CABLE RACKING NOTES**

- PROVIDE NON-METALLIC, NON-CONDUCTIVE, RACK ARMS, BOLTED TO CHANNELS INSET IN HANDHOLE WALLS. PROVIDE MIN. TWO (2) ARMS FOR EVERY WALL.
- CABLE RACKING SHALL BE LIMITED TO THE UPPER HALF OF THE HANDHOLE.
- PROVIDE TWO (2) CIRCUIT TAGS FOR EVERY L-823 CABLE SPLICE IN HANDHOLE WITHIN 24" OF SPLICE.
- PROVIDE A MIN. OF TWELVE (12) FEET OF CABLE BETWEEN ENTRY POINT AND L-823 SPLICE.
- RACK AIRFIELD AND FIBER OPTICS CABLES, WHERE POSSIBLE, ON SIDE(S) OPPOSITE OF ENTRY/EXIT CONDUITS.
  - DUCT/CONDUIT ARRAY SHALL ENTER HANDHOLE IN LOWER HALF OF THE HANDHOLE.
  - PROVIDE BELL END FOR EVERY CONDUIT IN THE ARRAY.
  - GROUT AROUND/BETWEEN CONDUITS AND CONCRETE WALL.

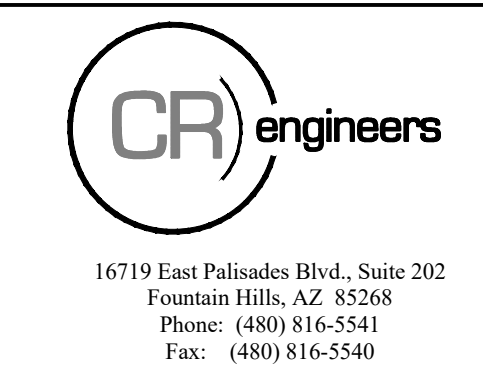
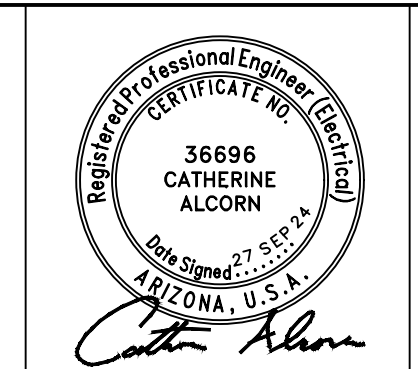


B CABLE ROUTING IDENTIFICATION TAG DETAIL  
TYP NTS

- PLAQUE NOTES:
- AFFIX ENGRAVED PHENOLIC TAGS TO THE WALL WITH SILICONE SEALANT ABOVE THE DUCT OR CONDUIT ENTRY IN A WAY THAT MIMICS THE ARRAY. WIRE BRUSH CONCRETE WALL TO ASSURE ADHESION.
  - THE TAGS SHALL BE APPROX 1 1/2" DIA. AND SHALL HAVE MINIMUM 1/4" LETTERING.

REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: RD  
 DRAWN BY: JBW  
 SHEET CHK'D BY: CA  
 CROSS CHK'D BY: SW  
 APPROVED BY: CA  
 DATE: 09/27/2024



CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

AIRFIELD ELECTRICAL  
 HANDHOLE DETAILS

PROJECT NO. 1021015.11  
 FILE NAME: 23011 E5.0.DWG  
 DWG NO.  
**E5.2**  
 SHEET 30 OF 39

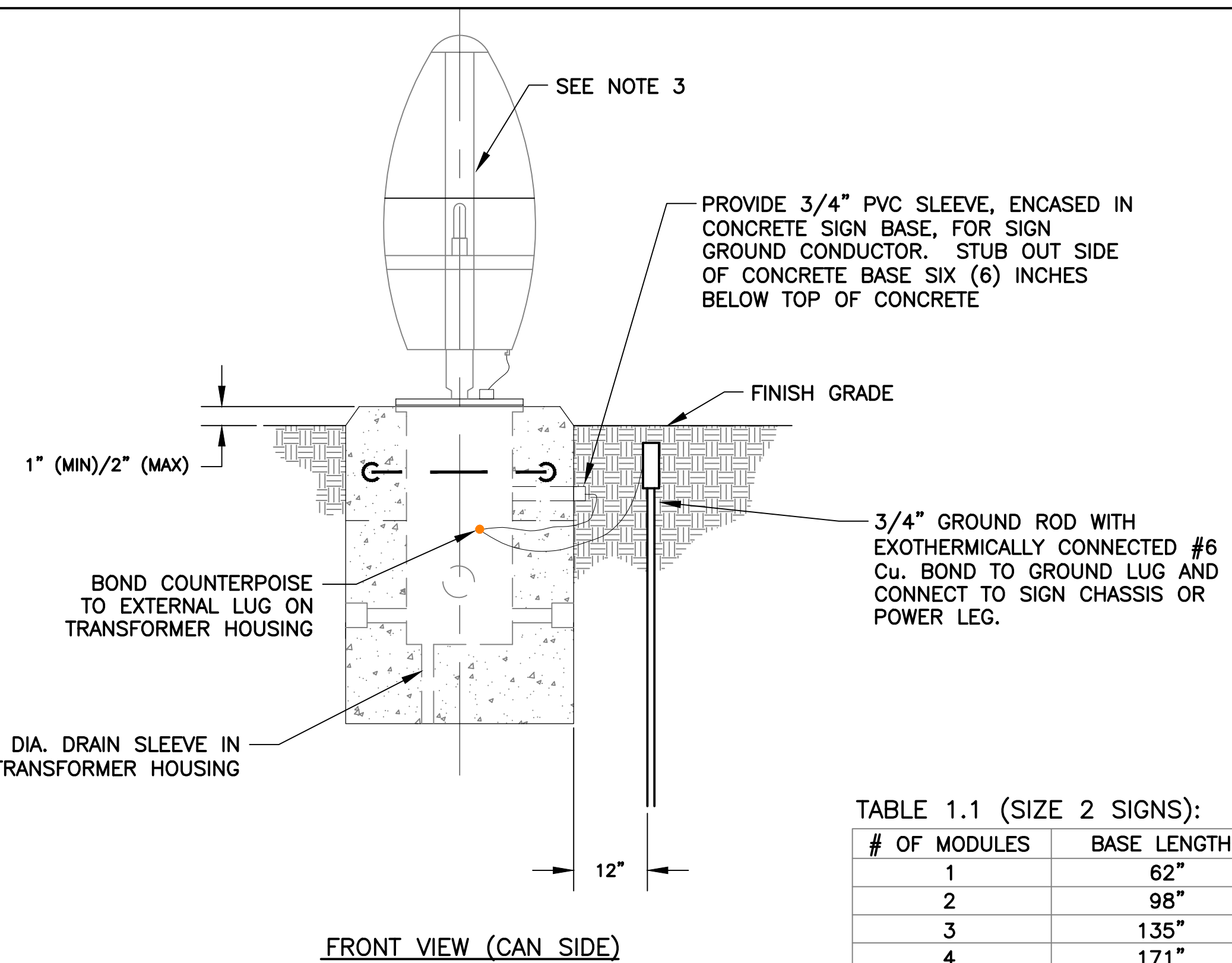
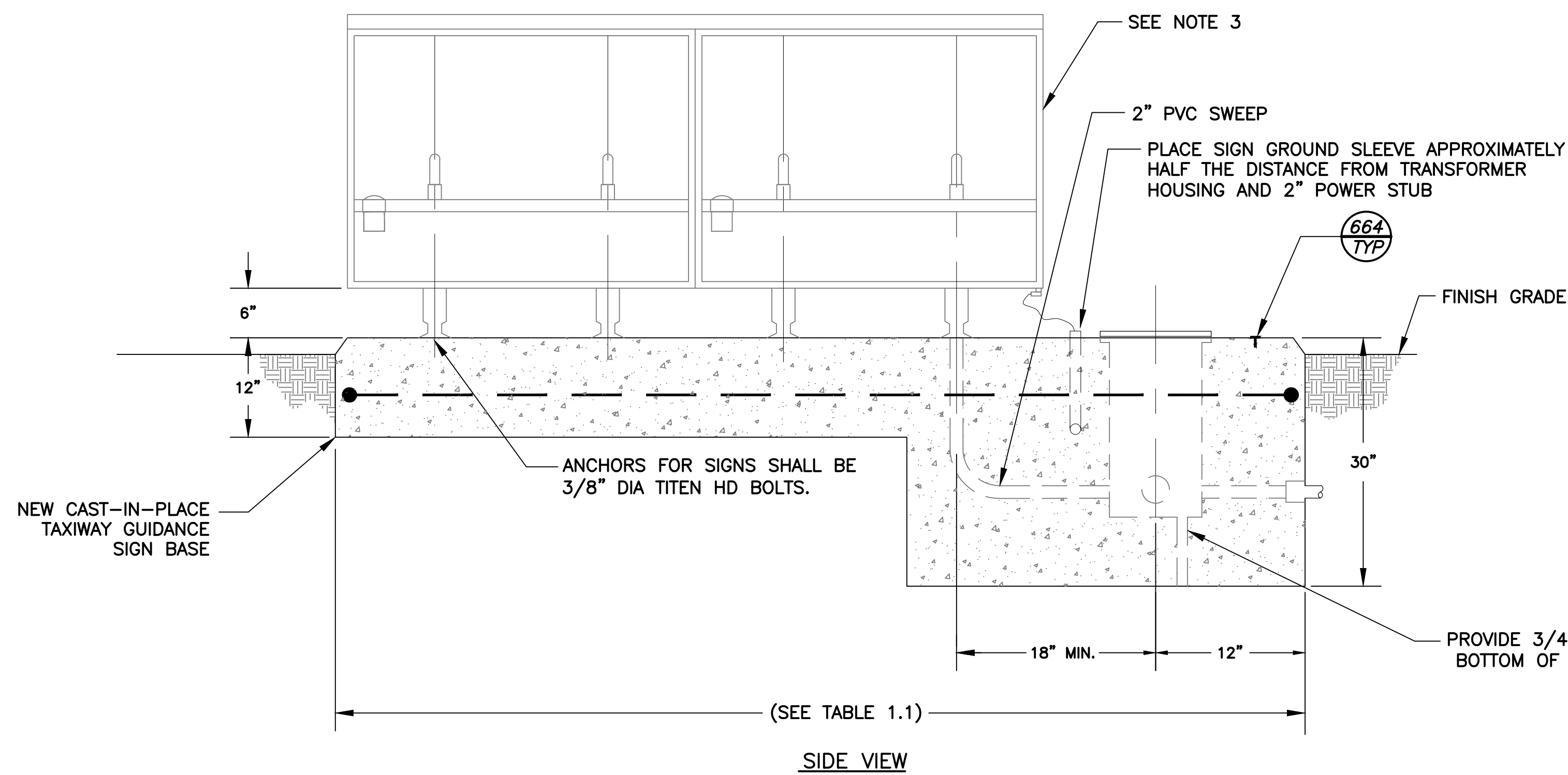
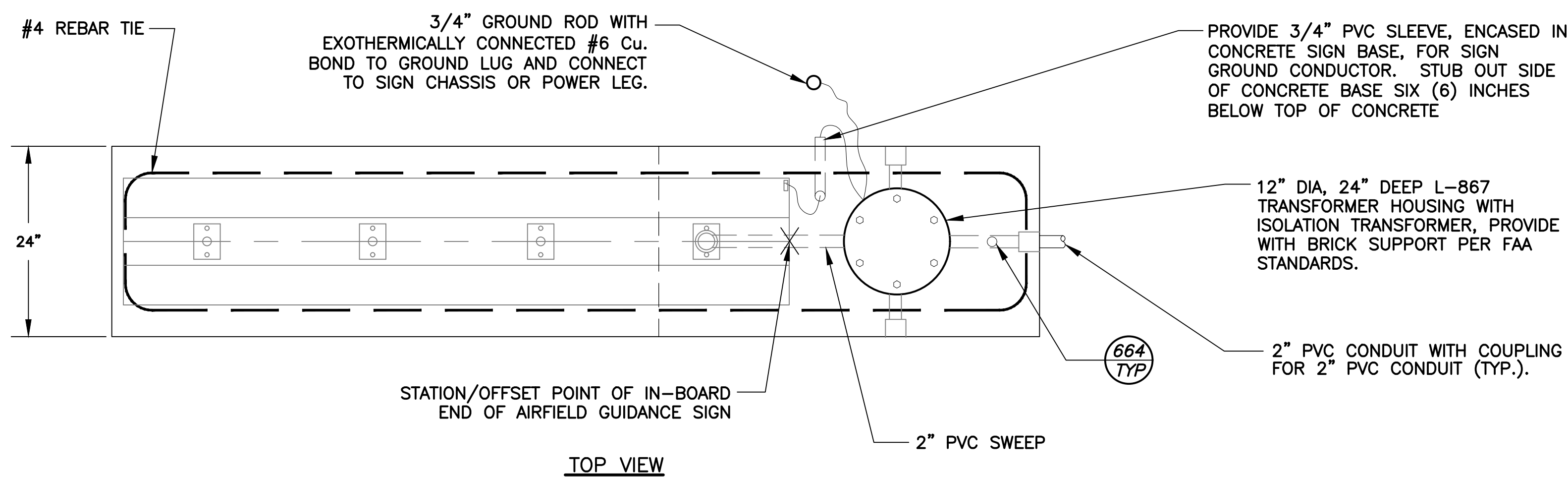


TABLE 1.1 (SIZE 2 SIGNS):

# OF MODULES	BASE LENGTH (A)
1	62"
2	98"
3	135"
4	171"



SIGN HEIGHTS AND LOCATION DISTANCES FOR TAXIWAY GUIDANCE SIGNS

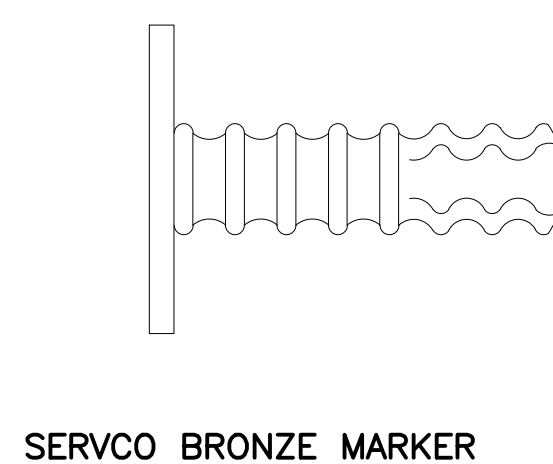
SIGN SIZE	LEGEND HEIGHT	LEGEND PANEL HEIGHT	INSTALLED* (MAX.)	PERPENDICULAR DISTANCE FROM DEFINED PAVMENT EDGE TO NEAR SIDE OF SIGN
1	12"	18"	30"	10' - 20'
2	15"	24"	36"	20' - 35'
3	18"	30"	42"	35' - 60'

\* THE HEIGHT REFERRED TO IN THIS COLUMN IS THE DISTANCE FROM TOP OF THE SIGN TO GRADE MEASURED AT THE SIDE OF THE SIGN THAT IS NEAREST TO THE APPLICABLE RUNWAY, TAXIWAY, OR APRON. IN ACCORDANCE WITH PARAGRAPH 1.14, THIS HEIGHT SHOULD BE REDUCED, IF NECESSARY, TO PROVIDE THE REQUIRED 12-INCH CLEARANCE BETWEEN THE TOP OF THE SIGN AND THE CRITICAL AIRCRAFT (AS NOTED IN PARAGRAPH 1.14.3).  
(SEE FAA ADVISORY CIRCULAR 150/5340-18 FOR FURTHER INFORMATION AND CLARIFICATION)

919 TYP NEW CAST-IN-PLACE AIRFIELD GUIDANCE SIGN BASE DETAIL NTS

NOTES

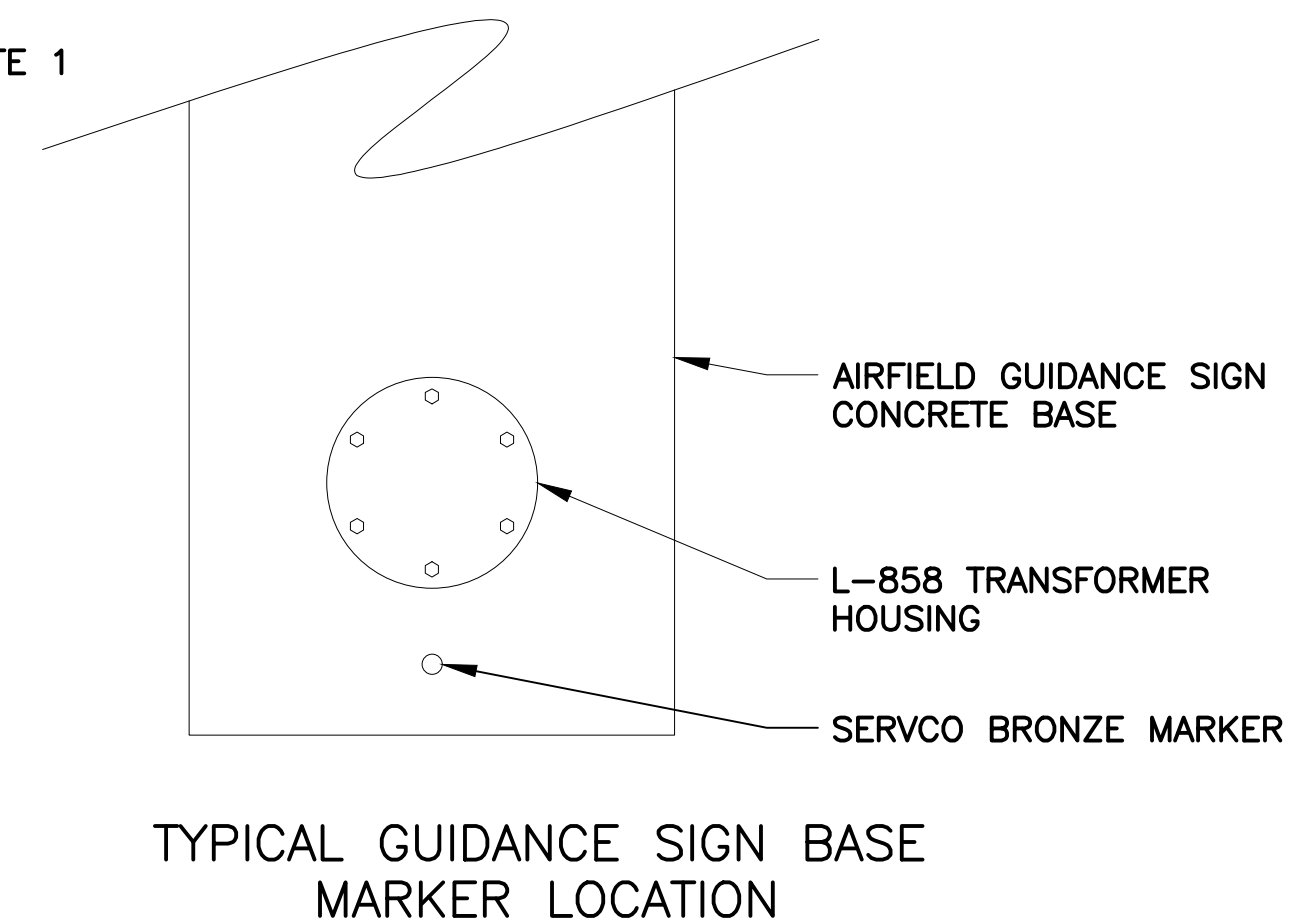
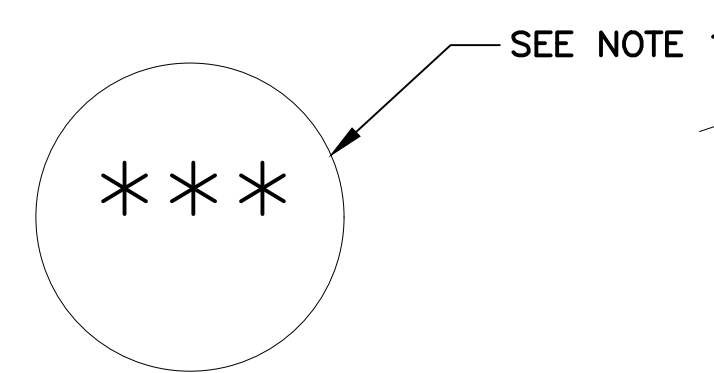
1. NEW CAST-IN-PLACE TAXIWAY GUIDANCE SIGN BASE DETAIL SHOWN WITH A TWO MODULE SIGN FOR ILLUSTRATIVE PURPOSES ONLY. SEE TABLE 1.1 FOR LENGTH OF BASE NEEDED FOR MULTIPLE MODULE SIGNS.
2. TRANSFORMER HOUSING SHALL BE ORIENTED AT TAXIWAY/RUNWAY END OF SIGN BASE.
3. SIGNS SHALL BE EQUIPPED WITH DISCONNECTING SWITCH AND WEATHERPROOF COVER ON INBOARD OF SIGN.
4. BASE SHALL BE LEVEL CONSTRUCTED 1" ABOVE HIGHEST POINT OF SURROUNDING GRADE OR EDGE OF TAXIWAY SHOULDER PAVEMENT AS APPLICABLE.



664 TYP TAXIWAY CIRCUIT IDENTIFICATION MARKER NTS

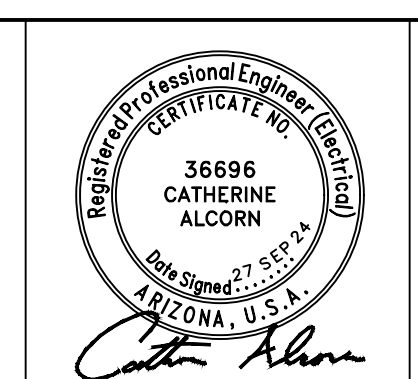
NOTE:

1. STAMP CIRCUIT IDENTIFICATION INTO BASE MARKER WITH 1/2" CHARACTERS.
2. PROVIDE CIRCUIT ID MARKERS AT NEW SIGN BASES AND JUNCTION CANS.



REV. NO.	DATE	DRWN	CHKD	REMARKS

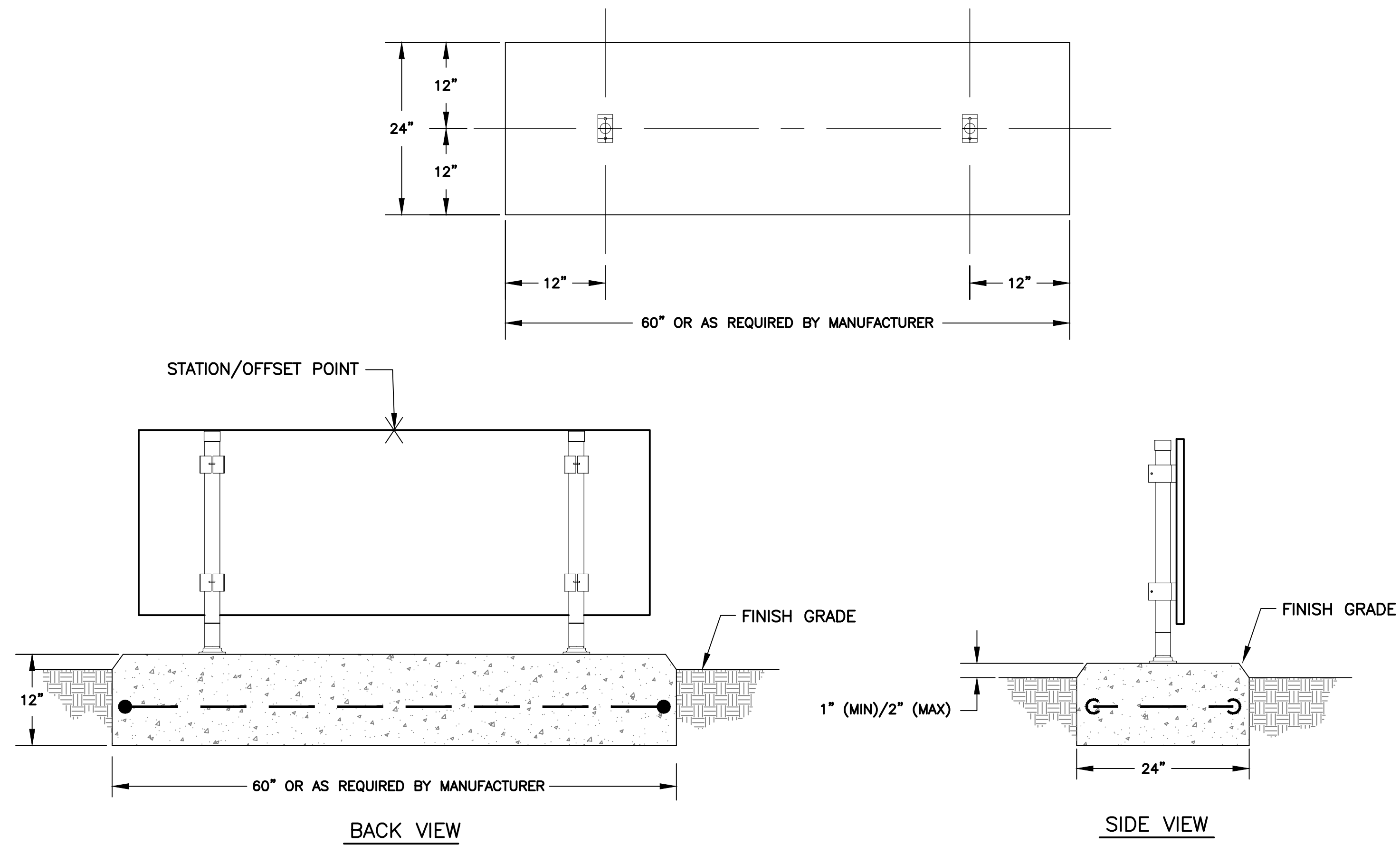
DESIGNED BY: RD  
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 SHEET CHK'D BY: CA  
 CROSS CHK'D BY: SW  
 APPROVED BY: CA  
 DATE: 09/27/2024



CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

CAST-IN-PLACE CONCRETE  
 AIRFIELD GUIDANCE SIGN BASE  
 INSTALLATION DETAILS

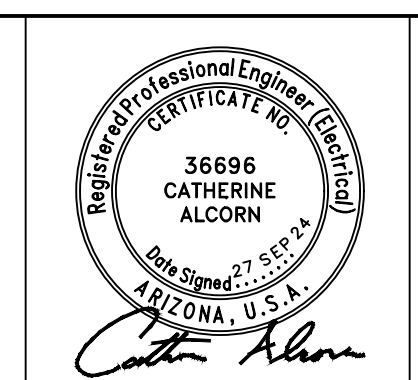
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 FILE NAME: 23011 E5.0.DWG  
 DWG NO.  
 E5.3  
 SHEET 31 OF 39



923 UNLIGHTED AIRSIDE GUIDANCE SIGN BASE DETAIL  
TYP NTS

REV. NO.	DATE	DRWN	CHKD	REMARKS

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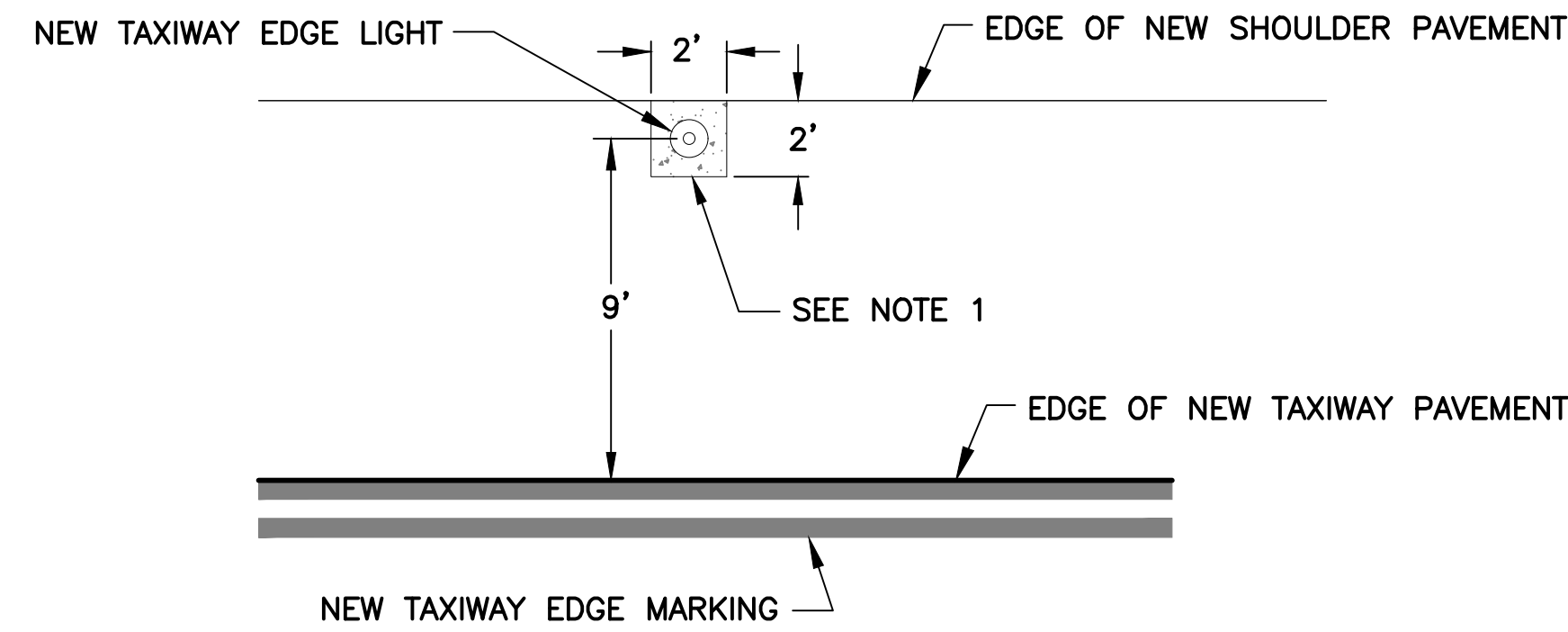


CITY OF CHANDLER, ARIZONA  
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 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

UNLIGHTED AIRFIELD GUIDANCE  
 SIGN INSTALLATION DETAILS

PROJECT NO. 1021015.11  
 FILE NAME: 23011 E5.0.DWG  
 DWG NO.  
**E5.4**  
 SHEET 32 OF 39

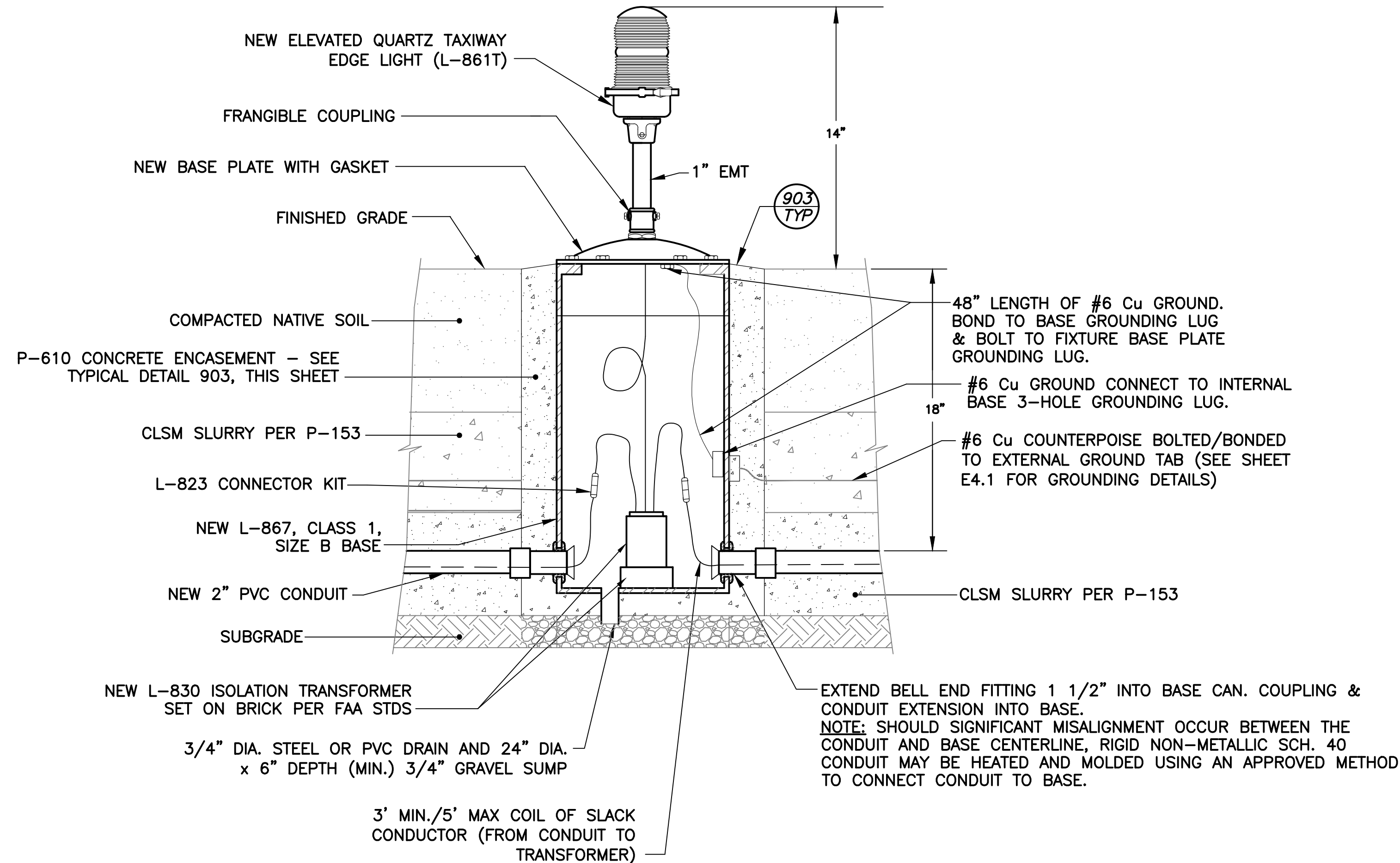




**903 TYP** ELEVATED EDGE LIGHT PLACEMENT DETAIL  
NTS

**NOTE**

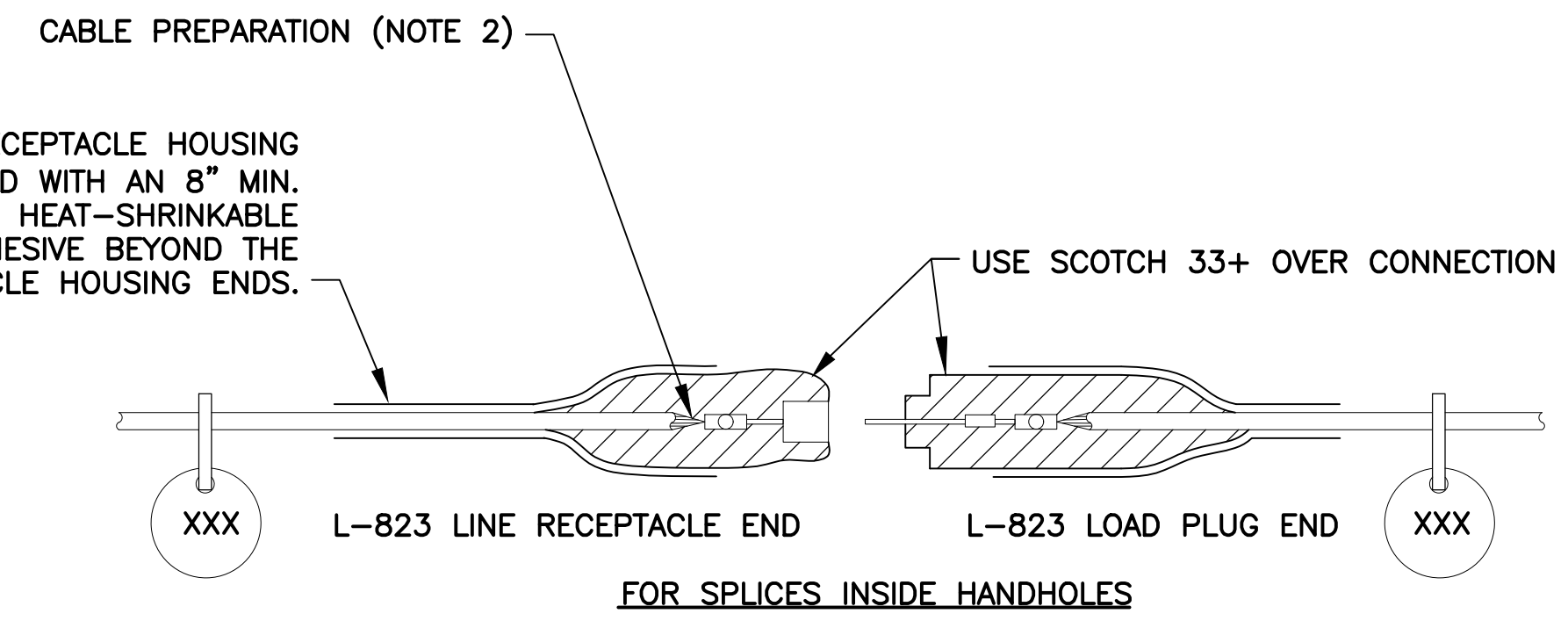
1. CONCRETE AROUND NEW TAXIWAY EDGE LIGHT PLACED IN NEW TAXIWAY SHOULDER PAVEMENT SHALL MEASURE 2 FOOT x 2 FOOT SQUARE.



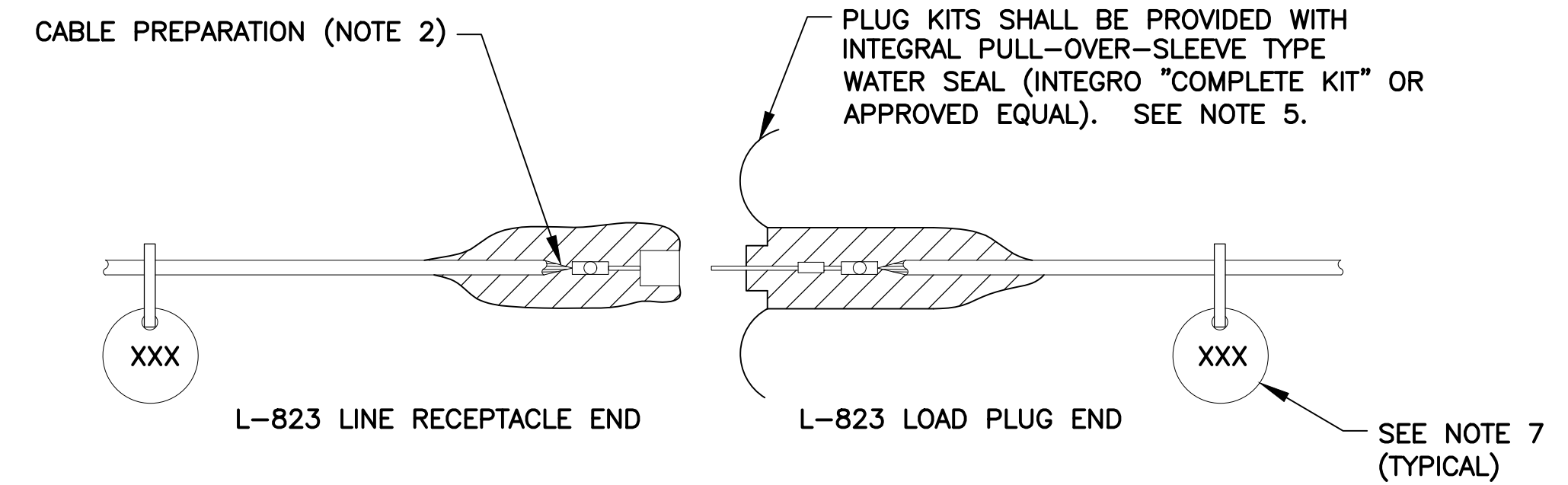
**901 TYP** ELEVATED EDGE LIGHT DETAIL  
NTS

REFER TO CIVIL PLANS FOR PAVEMENT SECTIONS

THE SPLICE KIT PIN AND RECEPTACLE HOUSING ENDS SHALL BE COVERED WITH AN 8" MIN. SECTION OF APPROVED HEAT-SHRINKABLE TUBING WITH INTERNAL ADHESIVE BEYOND THE PIN AND RECEPTACLE HOUSING ENDS.



FOR SPLICES INSIDE HANDHOLES



FOR SPLICES INSIDE CANS

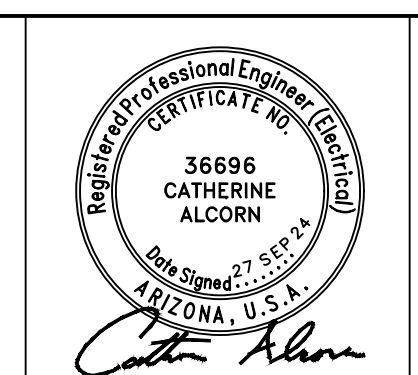
**NOTES**

1. PROVIDE MALE AND FEMALE L-823 CONNECTORS AS REQUIRED ON EACH CONDUCTOR IN EACH BASE, HANDHOLE, OR MANHOLE TO ALLOW ISOLATION OF HOMERUN CIRCUIT, NO STRAIGHT-THROUGH ALLOWED.
2. ALL CABLE ENDS SHALL BE PREPARED WITH THE USE OF A TAPERING TOOL SPECIFICALLY DESIGNED FOR USE WITH L-824 CABLES.
3. PLUG AND RECEPTACLE END FITTINGS SHALL BE CRIMPED ONTO THE CONDUCTOR BY USE OF AN AIRPORT PERSONNEL ACCEPTED RATCHETING TYPE CRIMPING TOOL.
4. AT THE POINT OF CONNECTION WITH THE EXISTING FIELD CIRCUITS, INSTALL NEW L-823 PLUGS ON BOTH THE NEW AND EXISTING CABLES. VERIFY INSULATION TYPES OF BOTH NEW AND EXISTING CABLES AND COORDINATE WITH TERMINATION KITS TO ASSURE PROPER AND WATERPROOF FIT.
5. INSTALL SCOTCH 33+ VINYL ELECTRICAL TAPE ON CONNECTION AFTER PULLING SLEEVE OVER COMPLETE KIT.
6. THERE SHALL BE NO SPLICES BETWEEN LIGHTS, ONLY IN BASES OR HANDHOLES.
7. PROVIDE AND INSTALL NON-CONDUCTIVE CIRCUIT IDENTIFICATION TAGS ATTACHED TO EACH SIDE OF ALL CONNECTOR KITS.
8. ON THE CABLES FOR THE RUNWAY CIRCUIT, TAPE FROM THE BACK END OF THE CONNECTOR KIT ONTO CABLE FOR 3" EACH, RED OR WHITE CIRCUIT TAPE FOR FASTER IDENTIFICATION AND MATCH EXISTING MANHOLE LAYOUTS.
9. ON THE CABLES FOR THE TAXIWAY AND SIGNAGE CIRCUITS, TAPE FROM THE BACK END OF THE CONNECTOR KIT ONTO CABLE FOR 3" EACH, BLUE THEN WHITE CIRCUIT TAPE FOR SIGNS, BLUE ONLY CIRCUIT TAPE FOR TAXIWAY EDGE LIGHTS, FOR FASTER IDENTIFICATION AND MATCH EXISTING MANHOLE LAYOUTS.

**925 TYP** TYPICAL 5KV CABLE SPLICE  
NTS

REV. NO.	DATE	DRWN	CHKD	REMARKS

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 DRAWN BY: JBW  
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 APPROVED BY: CA  
 DATE: 09/27/2024



CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

AIRFIELD ELECTRICAL EDGE LIGHTS AND SPLICE DETAILS

PROJECT NO. 1021015.11  
 FILE NAME: 23011 E5.0.DWG  
 DWG NO. E5.5  
 SHEET 33 OF 39

TAXIWAY EDGE LIGHTS DATA TABLE

NUMBER	STATION	OFFSET
TEL-1	428+85.00	30.68' LT
TEL-2	428+42.50	30.70' LT
TEL-3	428+00.00	30.72' LT
TEL-4	427+57.50	30.74' LT
TEL-5	427+15.00	30.76' LT
TEL-6	426+08.61	29.83' LT
TEL-7	426+08.74	29.98' RT
TEL-8	427+15.00	30.58' RT
TEL-9	428+85.00	30.68' RT
TEL-10	120+38.18	57.00' LT
TEL-11	120+58.68	36.50' LT
TEL-12	12+86.68	29.00' LT
TEL-13	121+36.68	29.00' LT
TEL-14	122+77.30	29.00' LT
TEL-15	124+67.91	29.00' LT
TEL-16	126+58.53	29.00' LT
TEL-17	128+49.15	29.00' LT
TEL-18	130+39.77	29.00' LT
TEL-19	131+80.02	29.00' LT
TEL-20	132+30.02	29.00' LT

NUMBER	STATION	OFFSET
TEL-21	132+58.02	36.50' LT
TEL-22	132+78.51	57.00' LT
TEL-23	528+85.00	30.95' LT
TEL-24	527+15.00	30.66' LT
TEL-25	132+78.80	57.00' RT
TEL-26	132+58.30	36.50' RT
TEL-27	132+30.30	29.00' RT
TEL-28	131.80.30	29.00' RT
TEL-29	130+39.68	29.00' RT
TEL-30	128+49.06	29.00' RT
TEL-31	126+58.44	29.00' RT
TEL-32	124+67.82	29.00' RT
TEL-33	122+77.20	29.00' RT
TEL-34	121+36.57	29.00' RT
TEL-35	120+86.58	29.00' RT
TEL-36	120+58.58	36.50' RT
TEL-37	120+38.09	57.00' RT

HANDHOLE DATA TABLE

NUMBER	STATION	OFFSET
HH-1	132+07.08	50.00' RT
HH-2	132+07.08	45.00' LT

AIRFIELD GUIDANCE SIGNS DATA TABLE

NUMBER	STATION	OFFSET
SGN-1	427+14.91	42.58' LT
SGN-2	428+85.43	43.33' RT
SGN-3	120+86.82	42.00' RT
SGN-4	132+30.02	42.00' LT
SGN-5	528+15.00	43.66' LT
SGN-6	EXISTING BASE	
SGN-7	428+00.00	42.64' LT

GENERAL NOTES

- EQUIPMENT NUMBERS SHOWN ARE FOR CONSTRUCTION REFERENCE ONLY. COORDINATE WITH AIRPORT MAINTENANCE FOR LABELING OF ALL EQUIPMENT.
- CONTRACTOR TO ENSURE NEW TAXIWAY EDGE LIGHT LOCATIONS DO NOT EXCEED 10 FEET FROM EDGE OF FULL STRENGTH PAVEMENT/OUTER EDGE OF TAXIWAY EDGE MARKING AND ARE IN-LINE WITH EXISTING TAXIWAY EDGE LIGHTING (IF APPLICABLE) - PERFORM FIELD ADJUSTMENTS AS NECESSARY. CONTRACTOR SHALL CONFIRM ANY DIFFERENCES OF MEASUREMENTS/DISCREPANCIES, WHEN SURVEY OF LIGHT LOCATIONS IS BEING PERFORMED, WITH ENGINEER BEFORE LIGHT BASE IS INSTALLED.
- OFFSETS GIVEN FOR EACH EQUIPMENT TYPE ARE MEASURED FROM NEAREST RUNWAY/TAXIWAY CENTERLINE (ALIGNMENT).
- CONTRACTOR TO ENSURE AIRFIELD GUIDANCE SIGN LOCATIONS FALL WITHIN FAA SPECIFICATIONS WITH RESPECT TO MEASUREMENT FROM EDGE OF FULL STRENGTH PAVEMENT/OUTER EDGE OF TAXIWAY OR RUNWAY EDGE MARKING. CONTRACTOR SHALL CONFIRM ANY DIFFERENCES OF MEASUREMENTS/DISCREPANCIES, WHEN SURVEY OF AIRFIELD GUIDANCE SIGN AND BASE IS BEING PERFORMED, WITH ENGINEER BEFORE SIGN BASE IS INSTALLED.

EQUIPMENT LOCATION STANDARDS TABLE

(PROVIDED FOR REFERENCE ONLY)

EQUIPMENT TYPE	LOCATION STANDARD	NOTES
SIZE 2 AIRFIELD GUIDANCE SIGNS	20' - 35' (SEE GENERAL NOTE 4)	MEASURE TO SIGN EDGE CLOSEST TO TAXIWAY EDGE/OUTER TAXIWAY EDGE MARKING (SEE FAA AC 1505340-18)
HANDHOLE		STATION/OFFSET GIVEN IS CENTER OF HANDHOLE LID
TAXIWAY EDGE LIGHT	2'-10' (SEE GENERAL NOTE 2)	MEASURE TO CENTER OF LIGHT FIXTURE; STATION/OFFSET GIVEN IS CENTER OF LIGHT FIXTURE (SEE FAA AC 150/5340-30)

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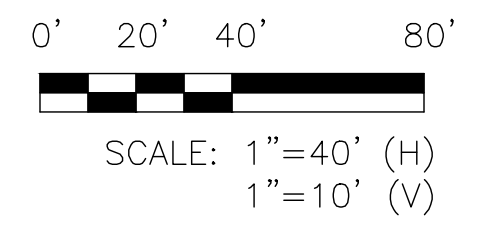
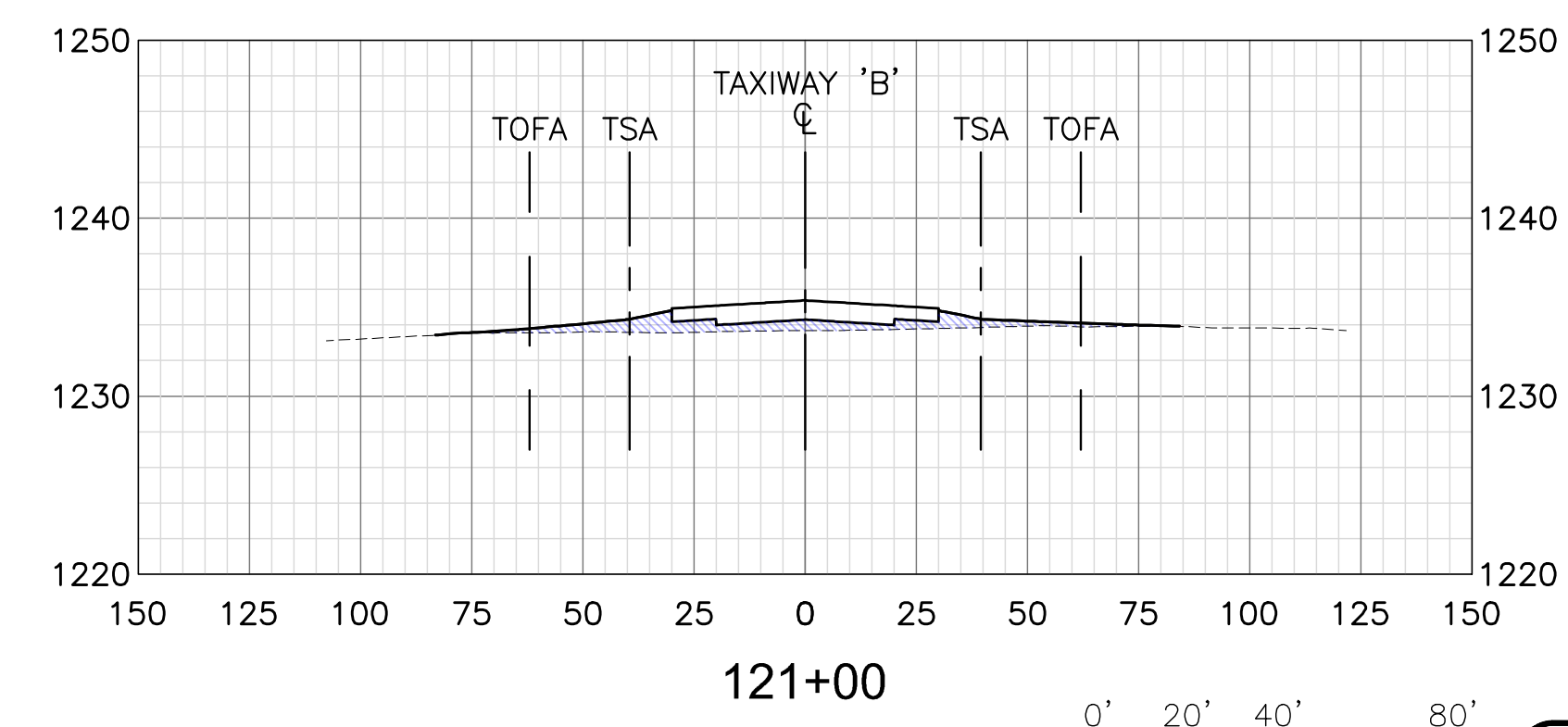
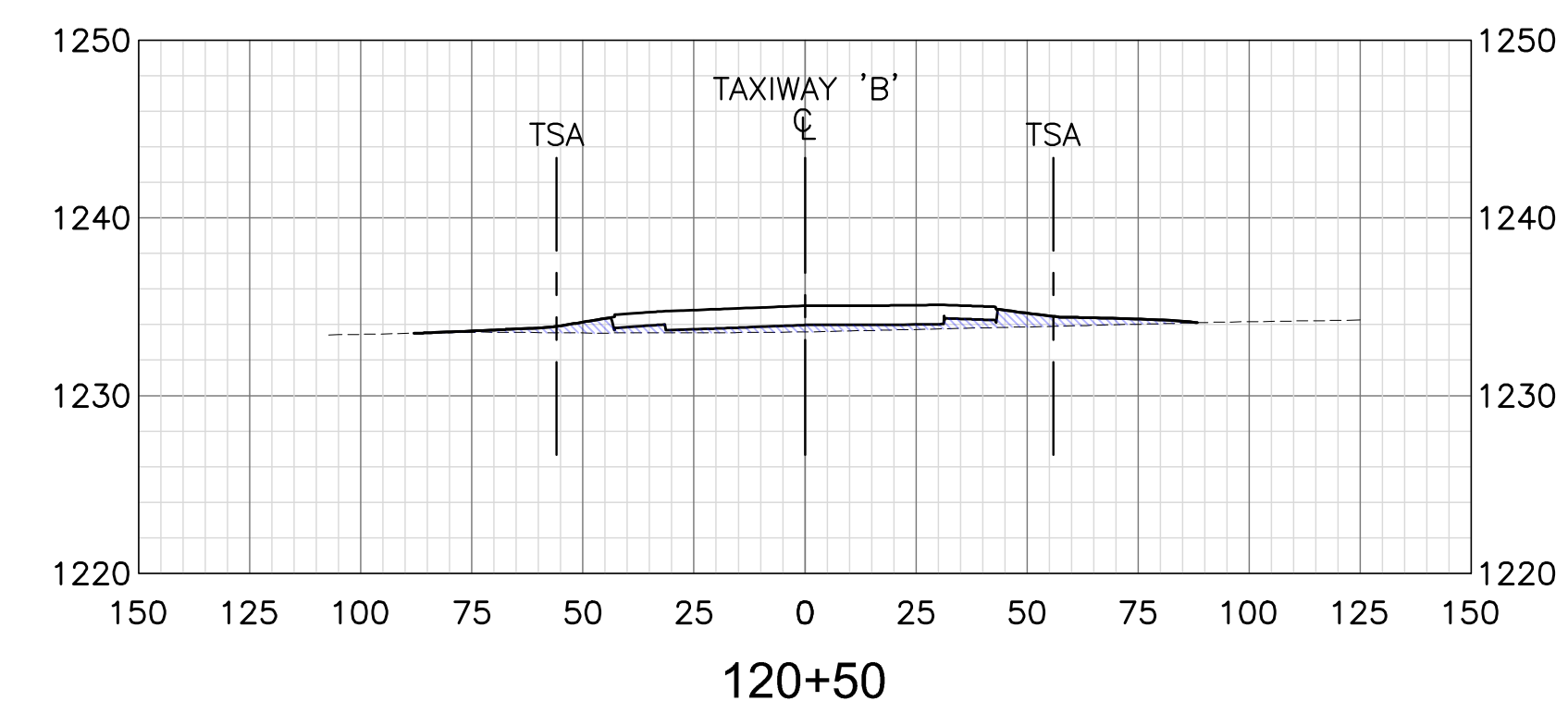
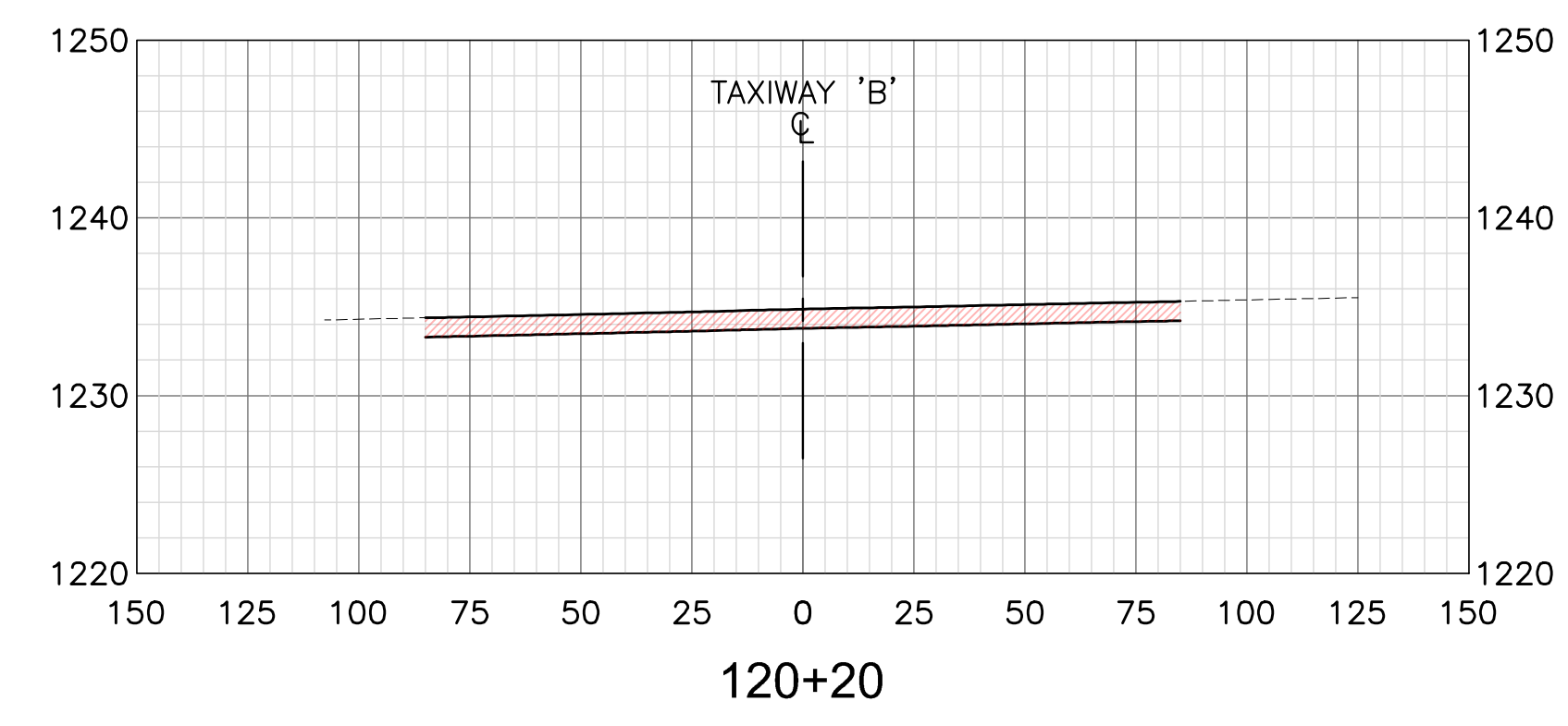
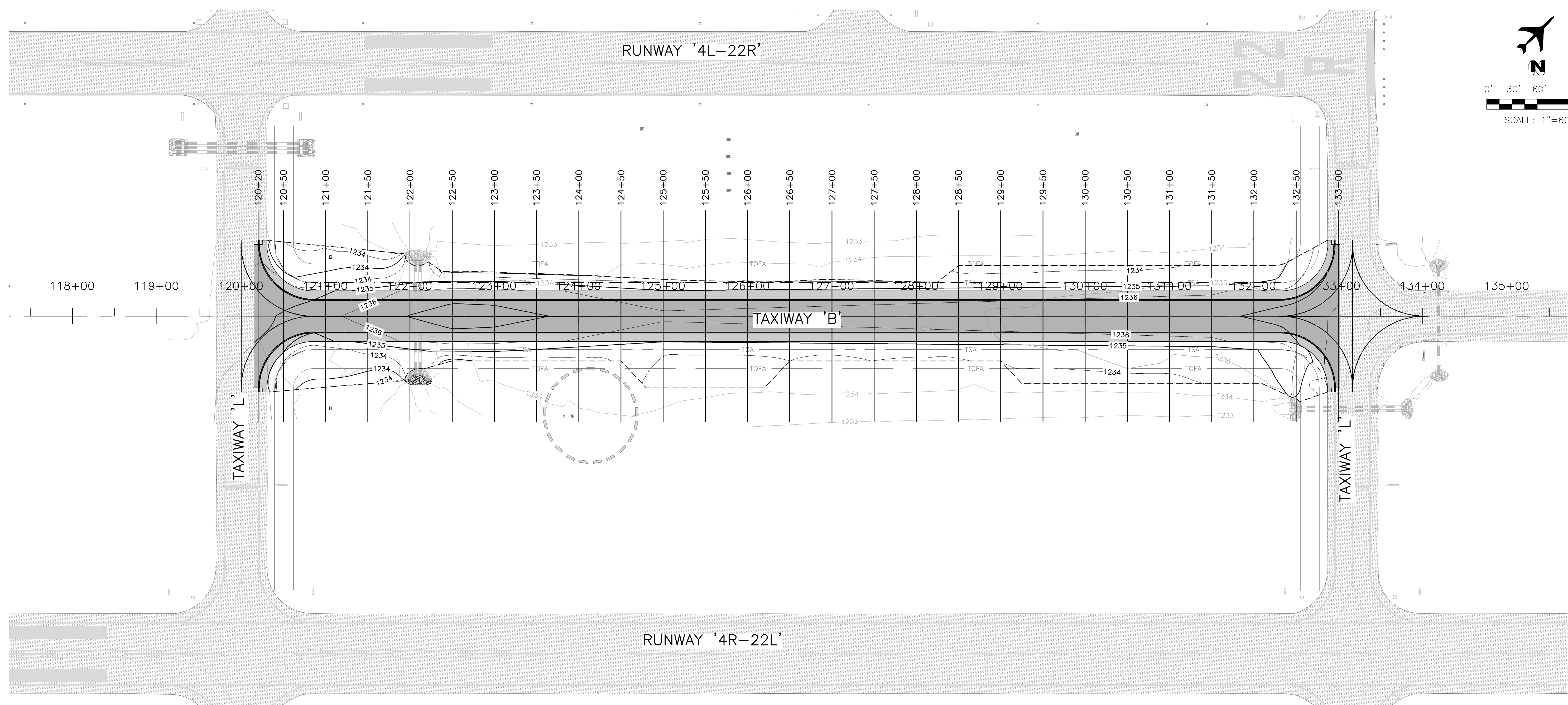
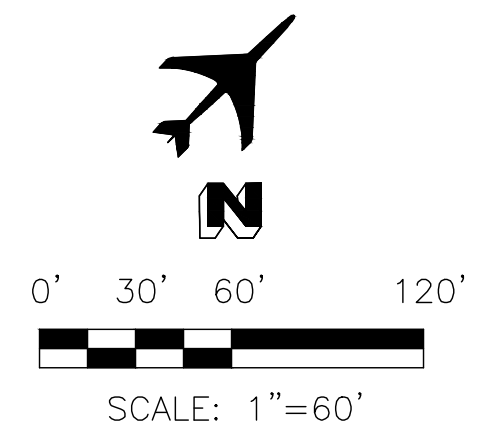
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CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

EQUIPMENT DATA TABLES

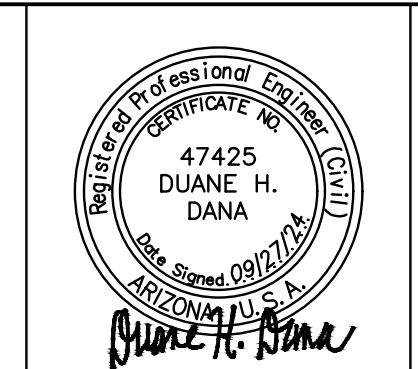
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 DWG NO.  
**E6.0**  
 SHEET 34 OF 39



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REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
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 APPROVED BY: DHD  
 DATE: 09/27/24

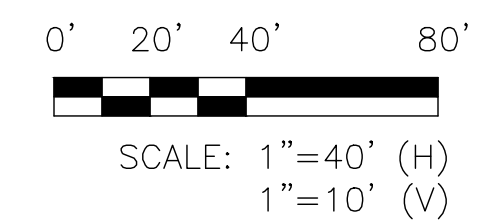
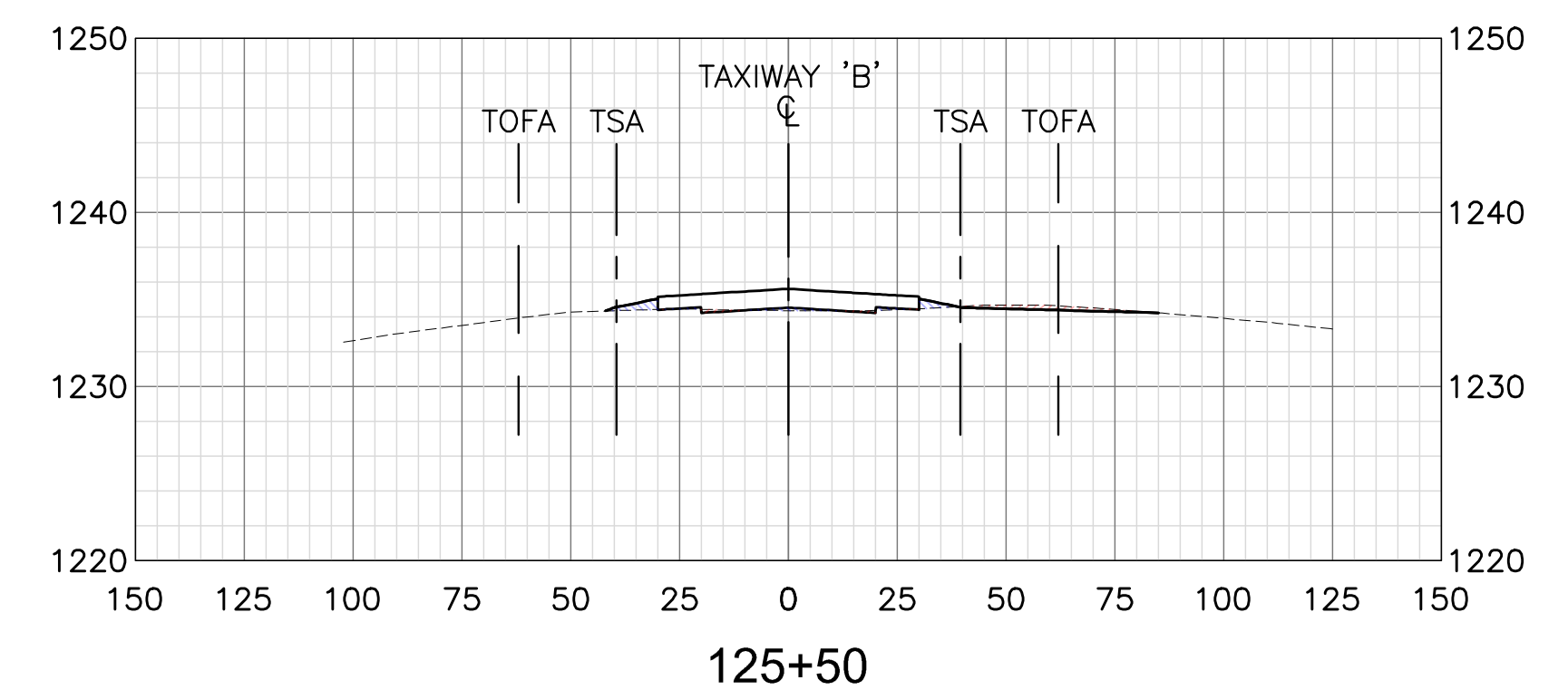
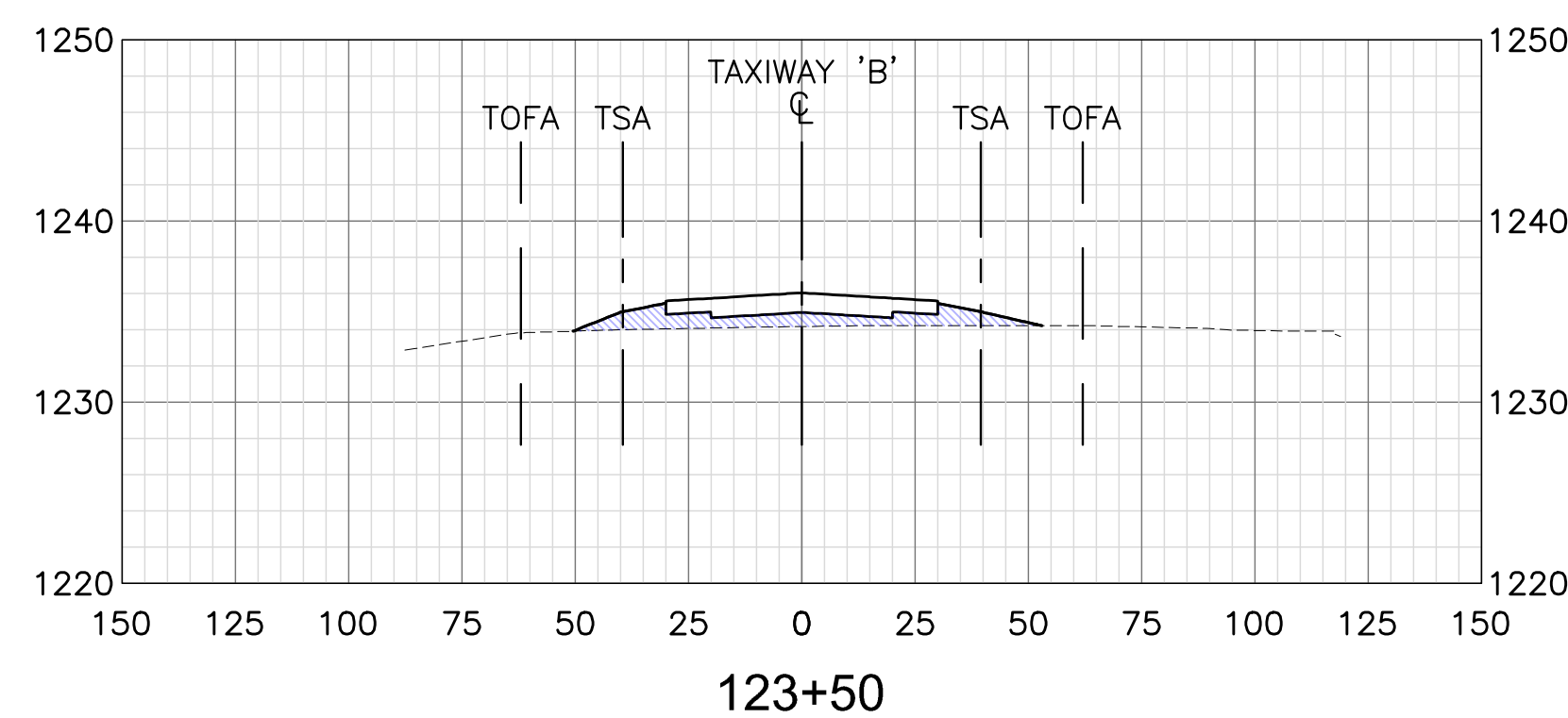
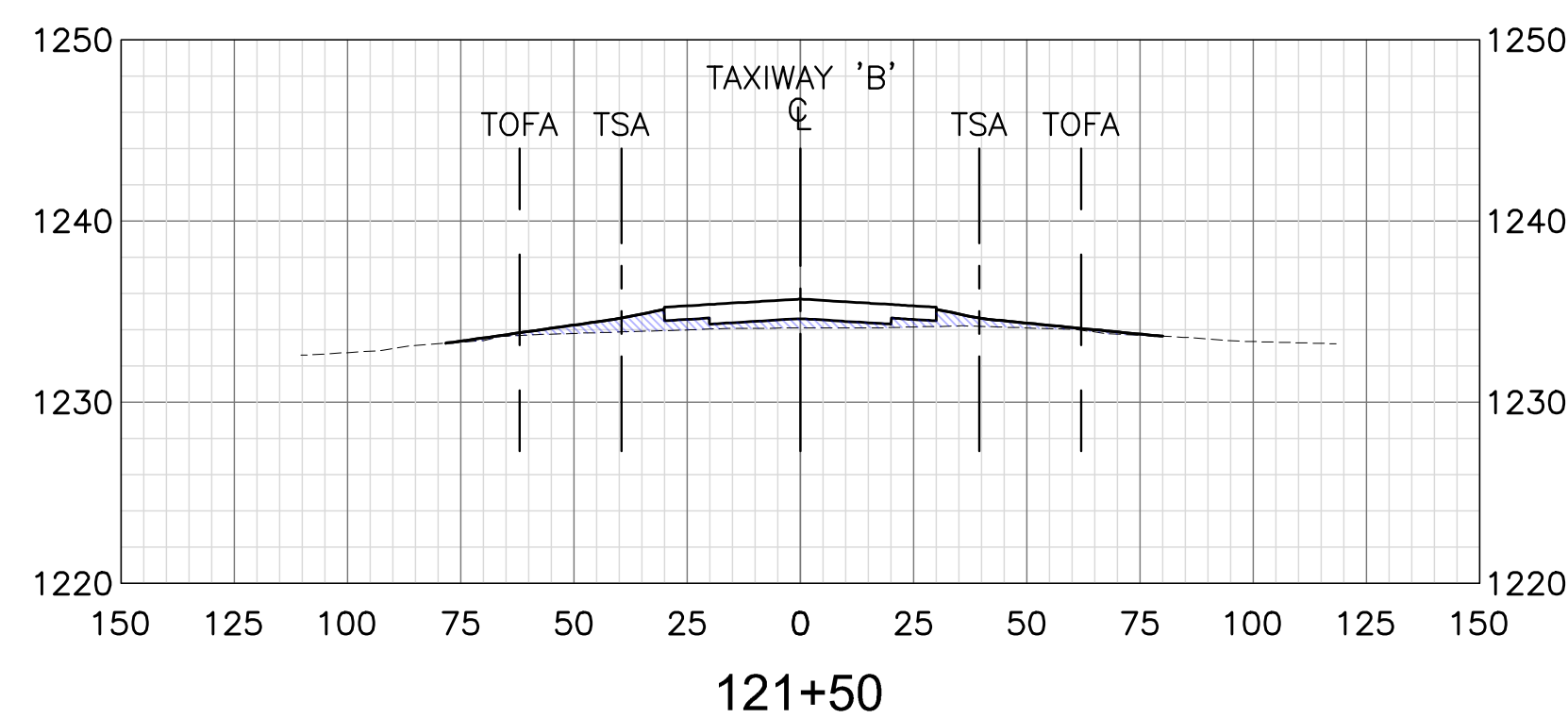
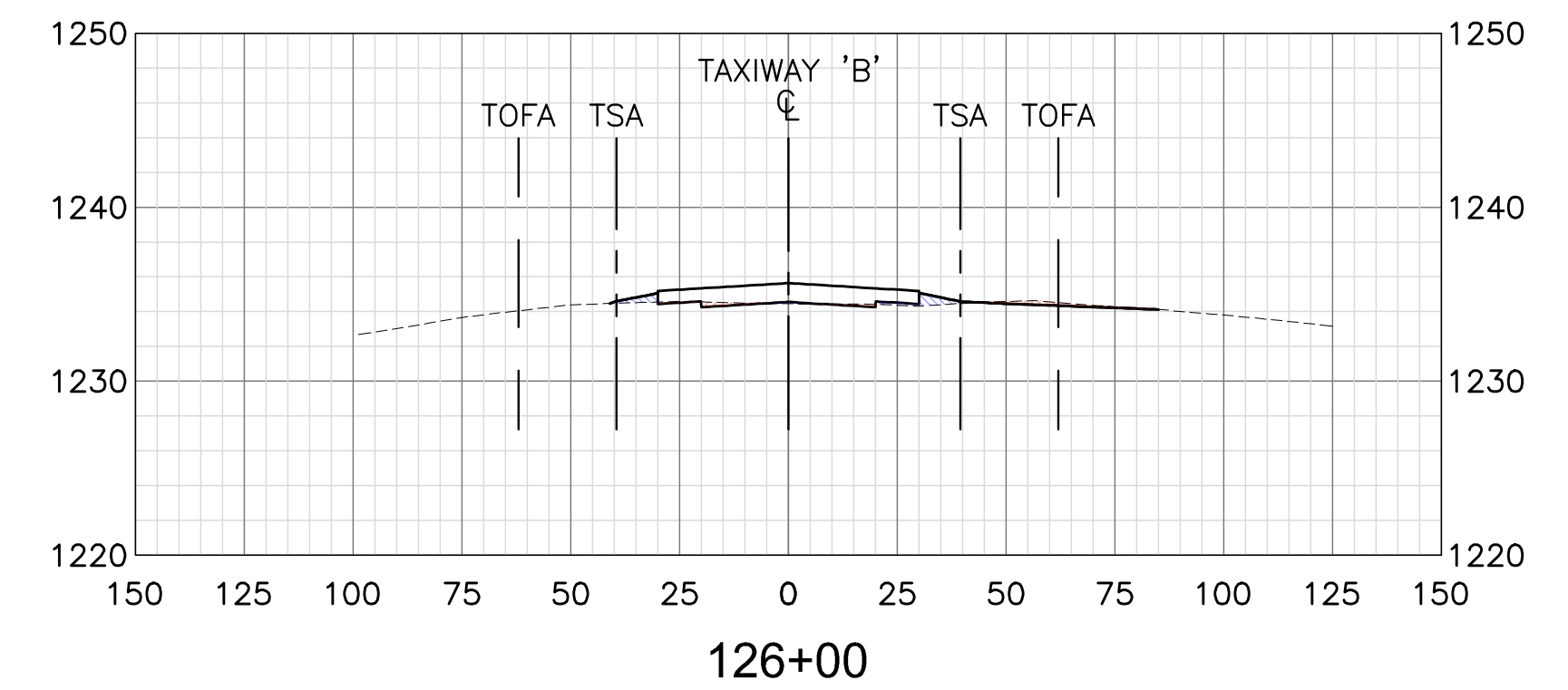
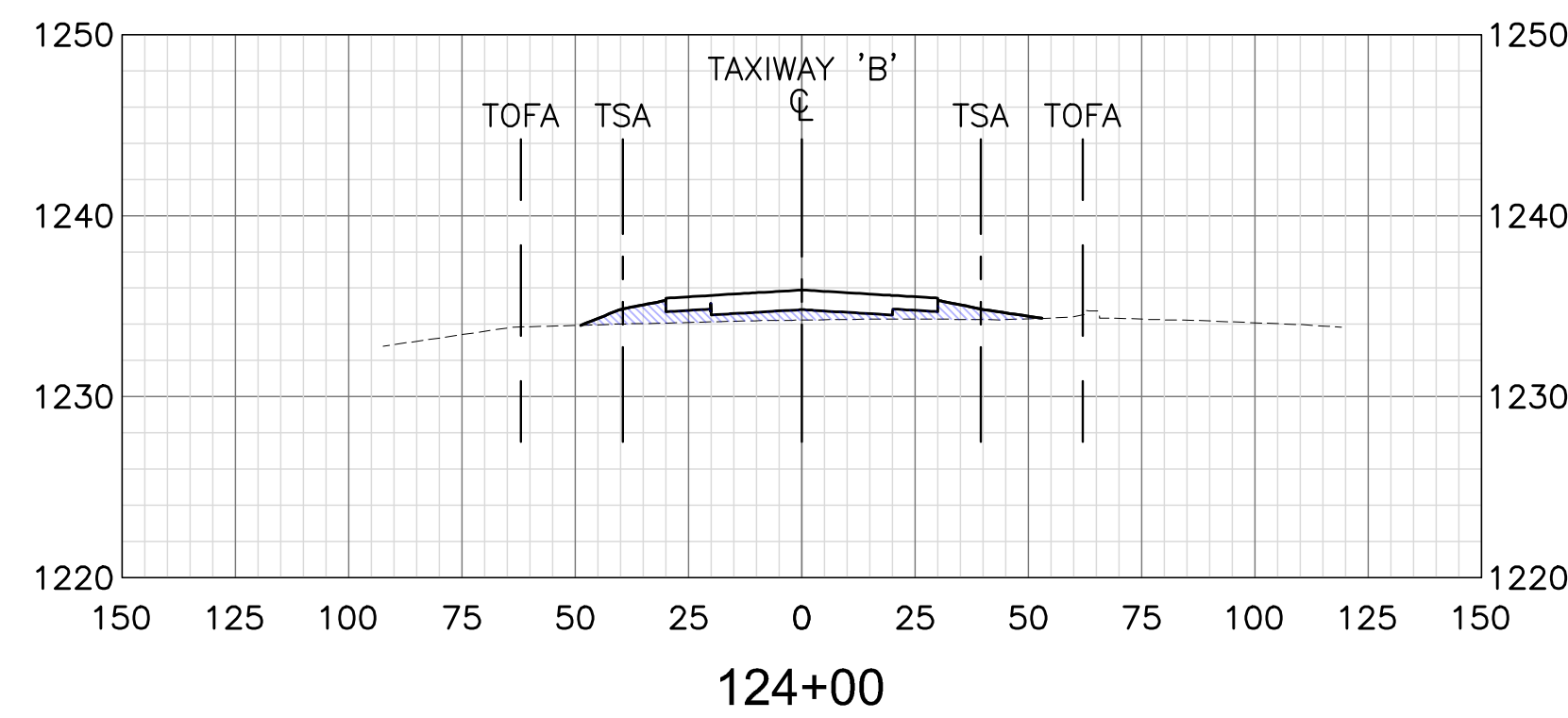
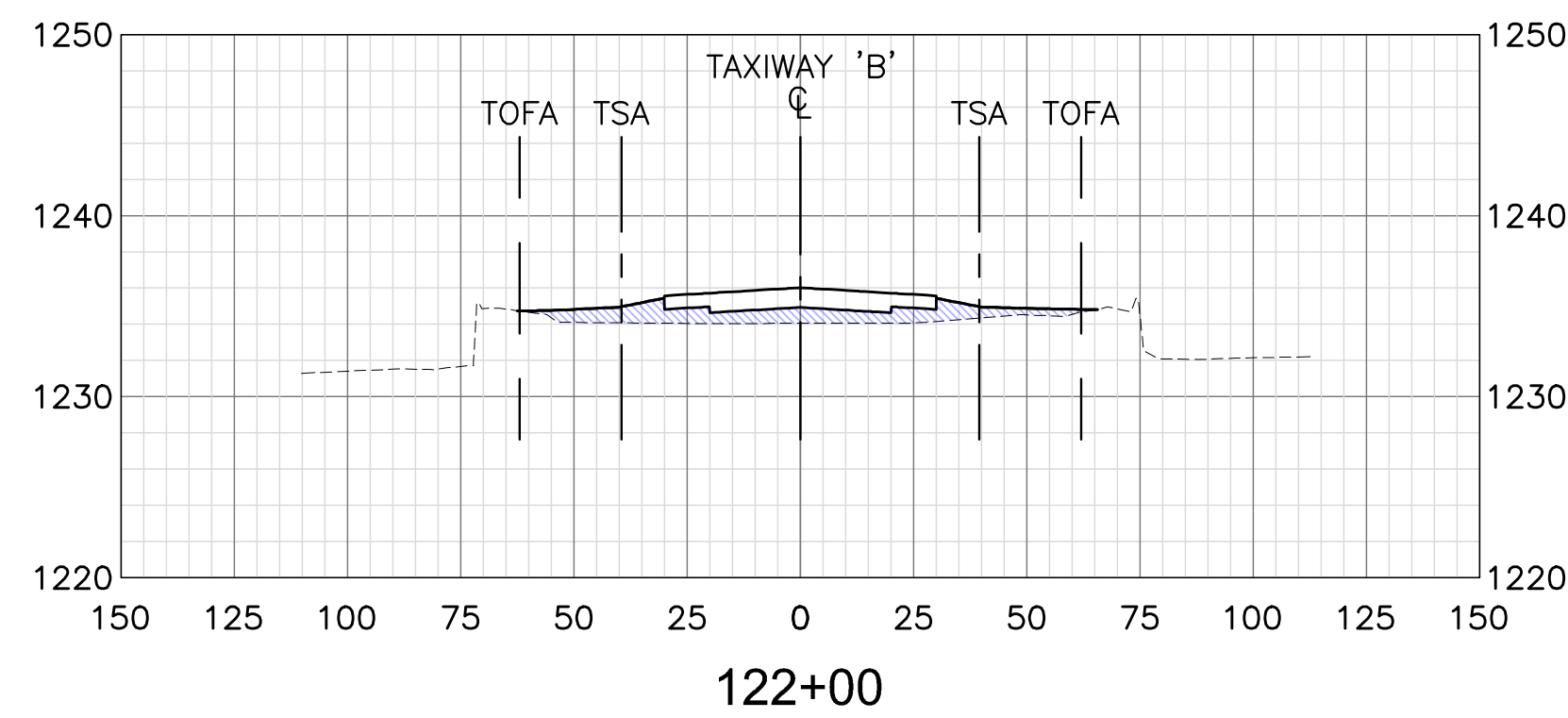
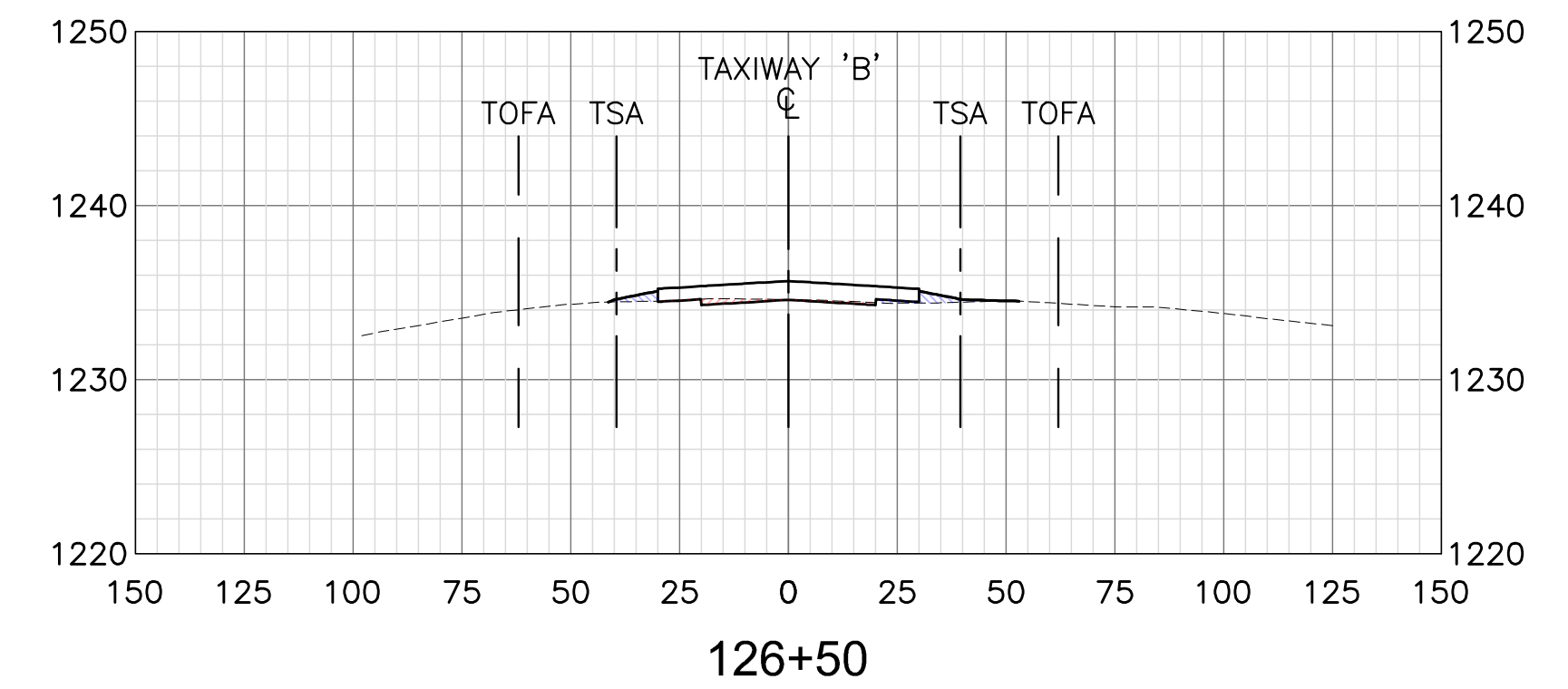
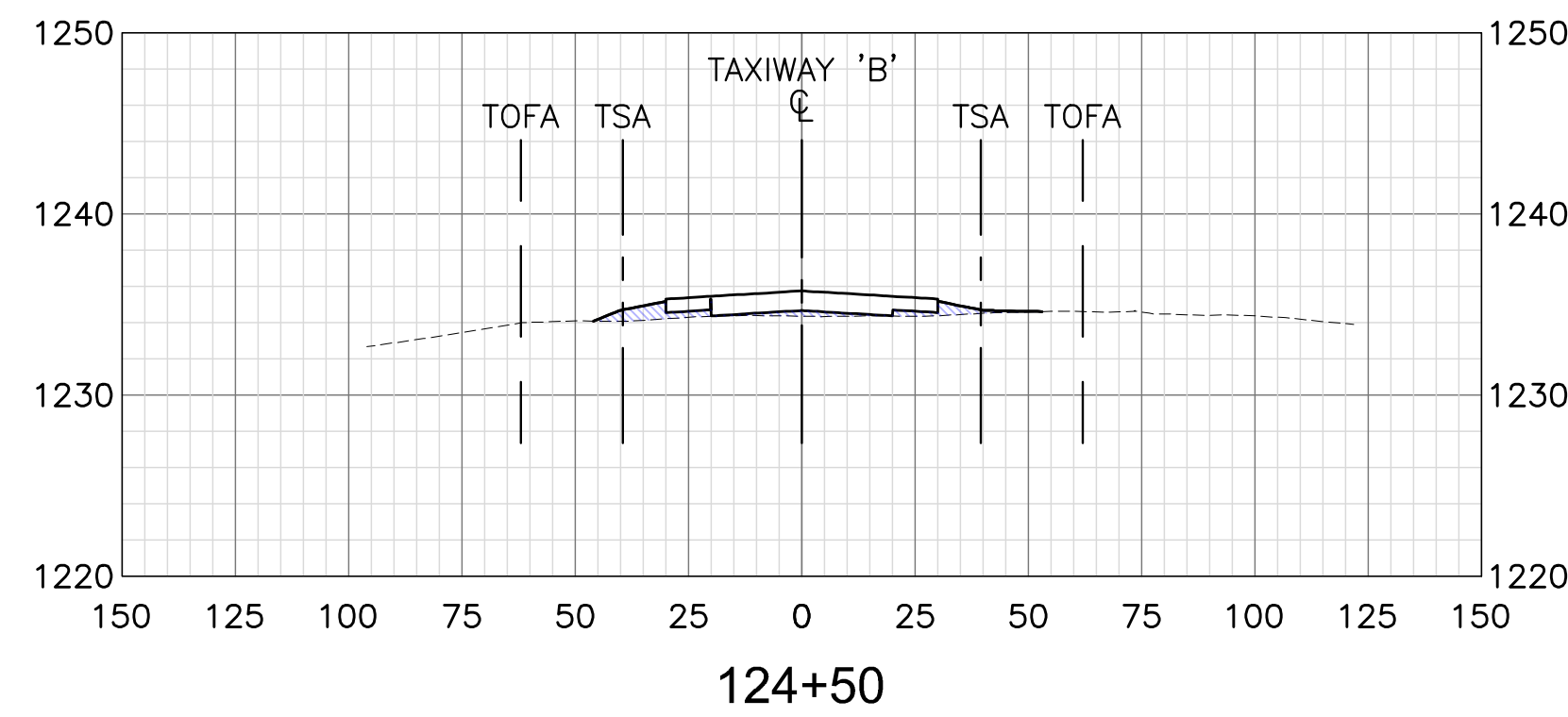
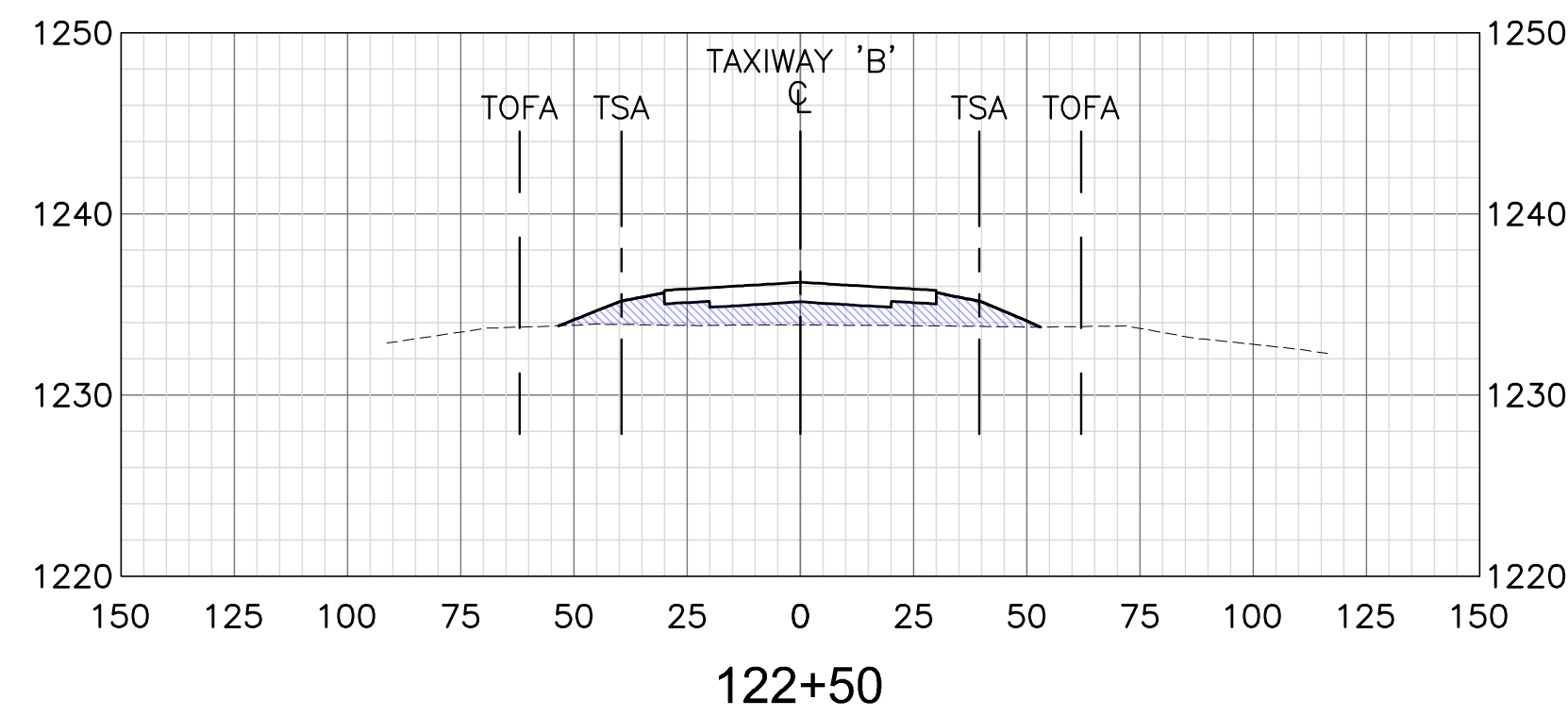
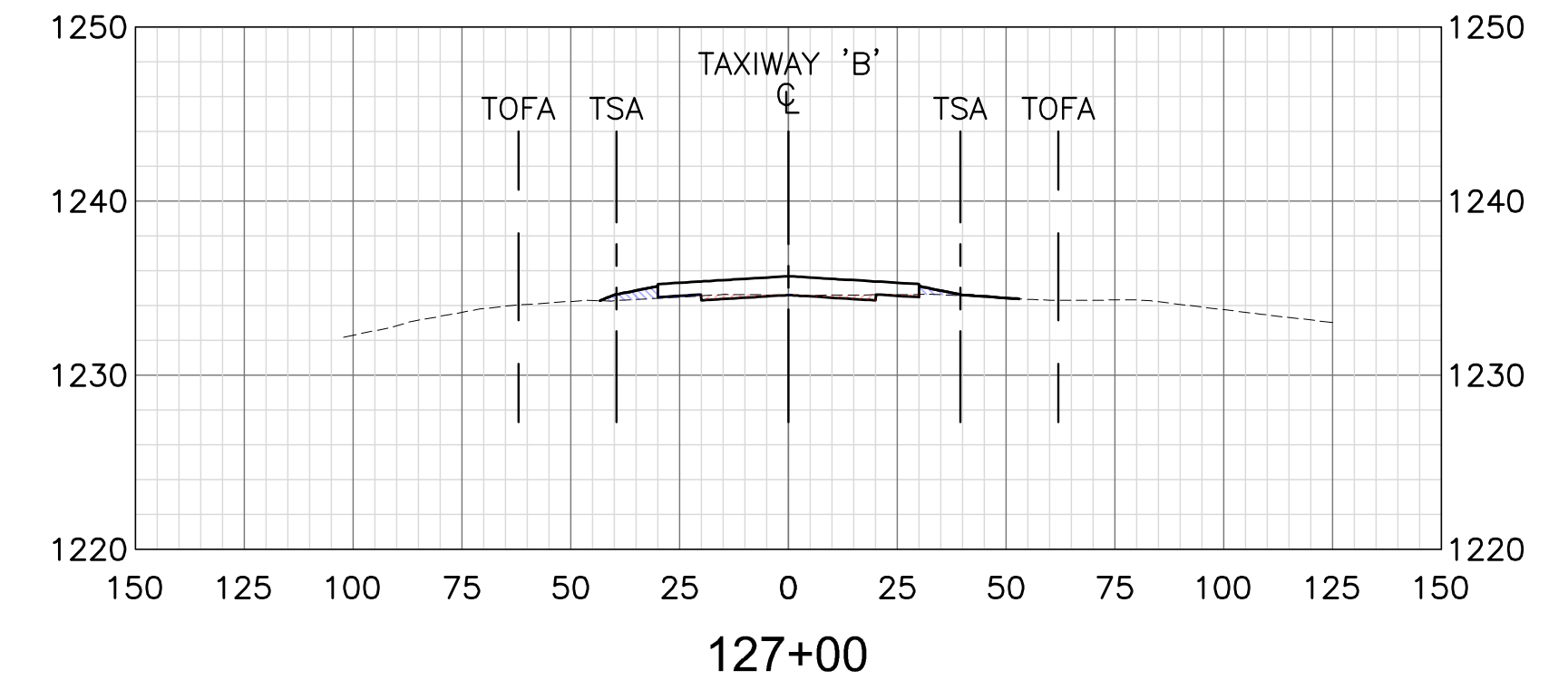
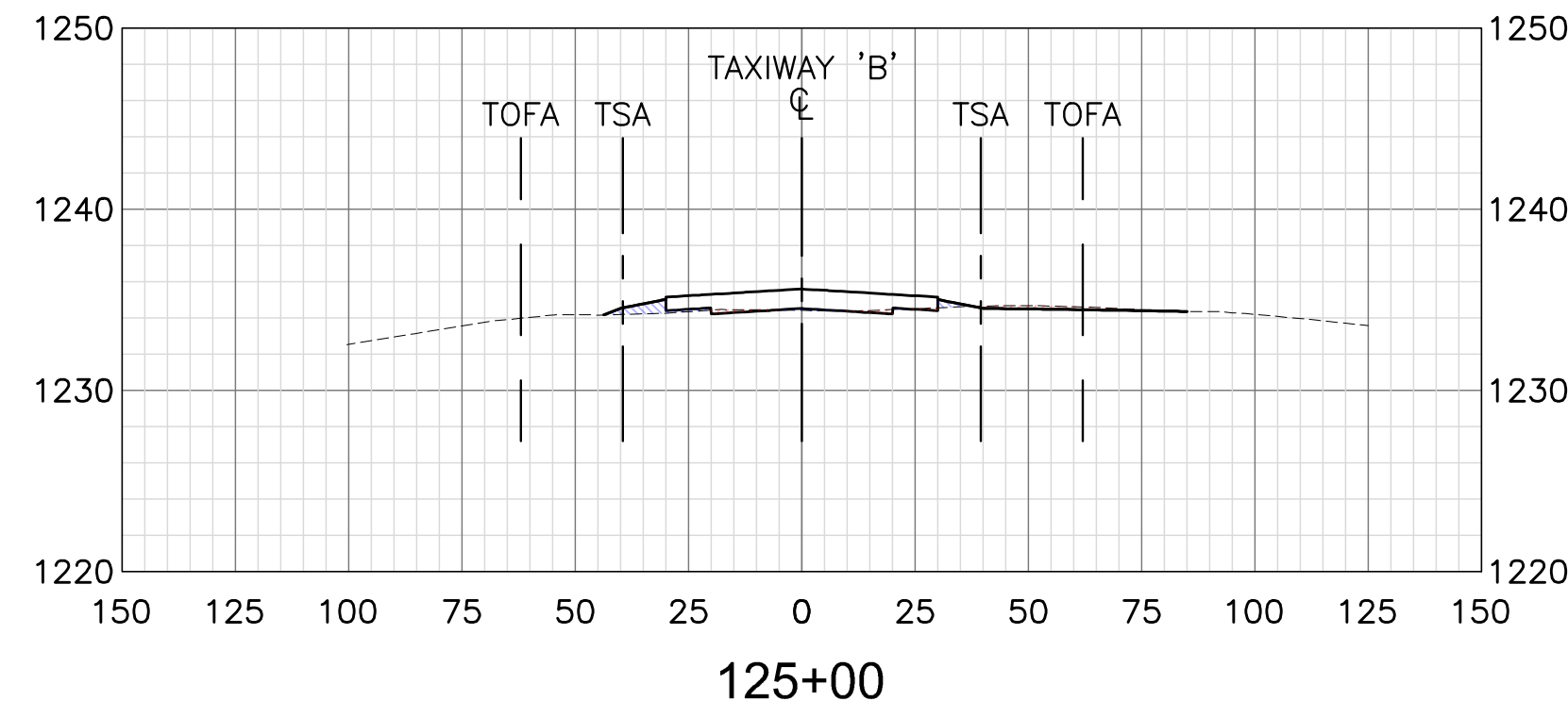
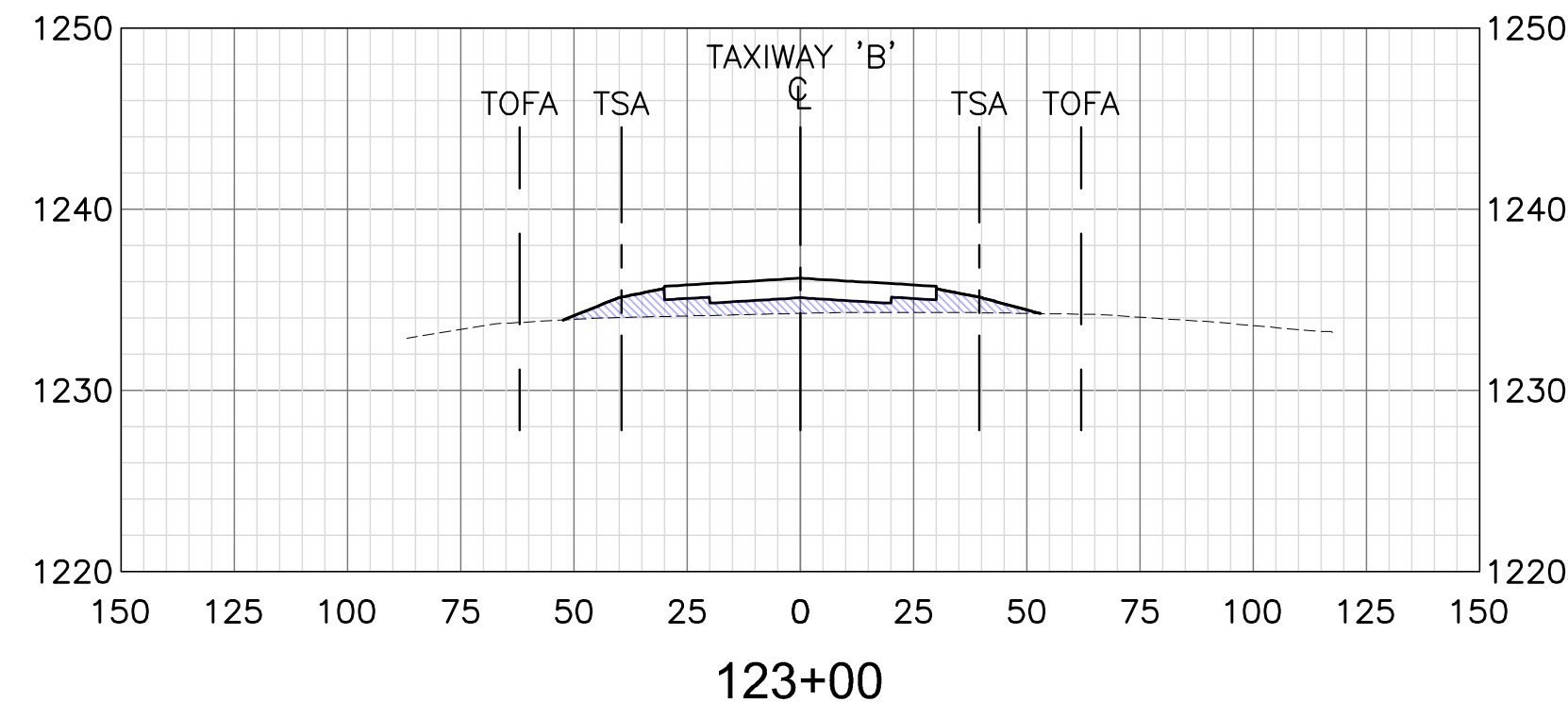


CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

CROSS SECTIONS  
 SHEET 1

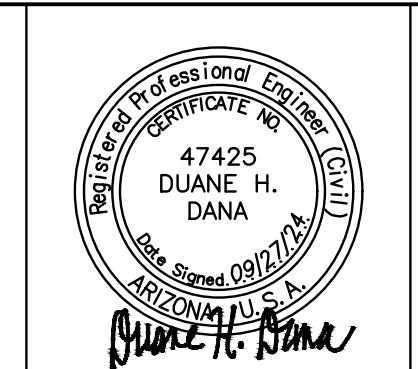
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 DWG NO.  
**X1.1**  
 SHEET 35 OF 39

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REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24

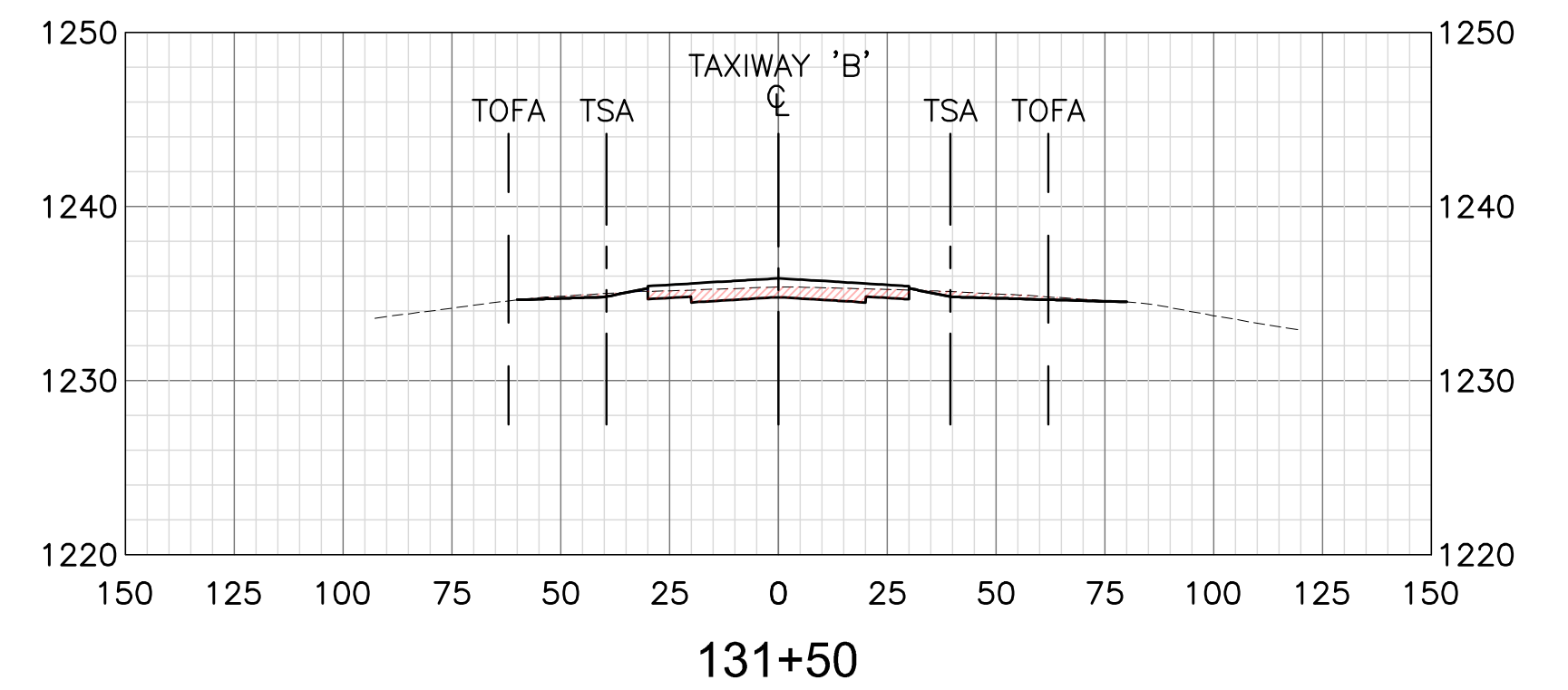
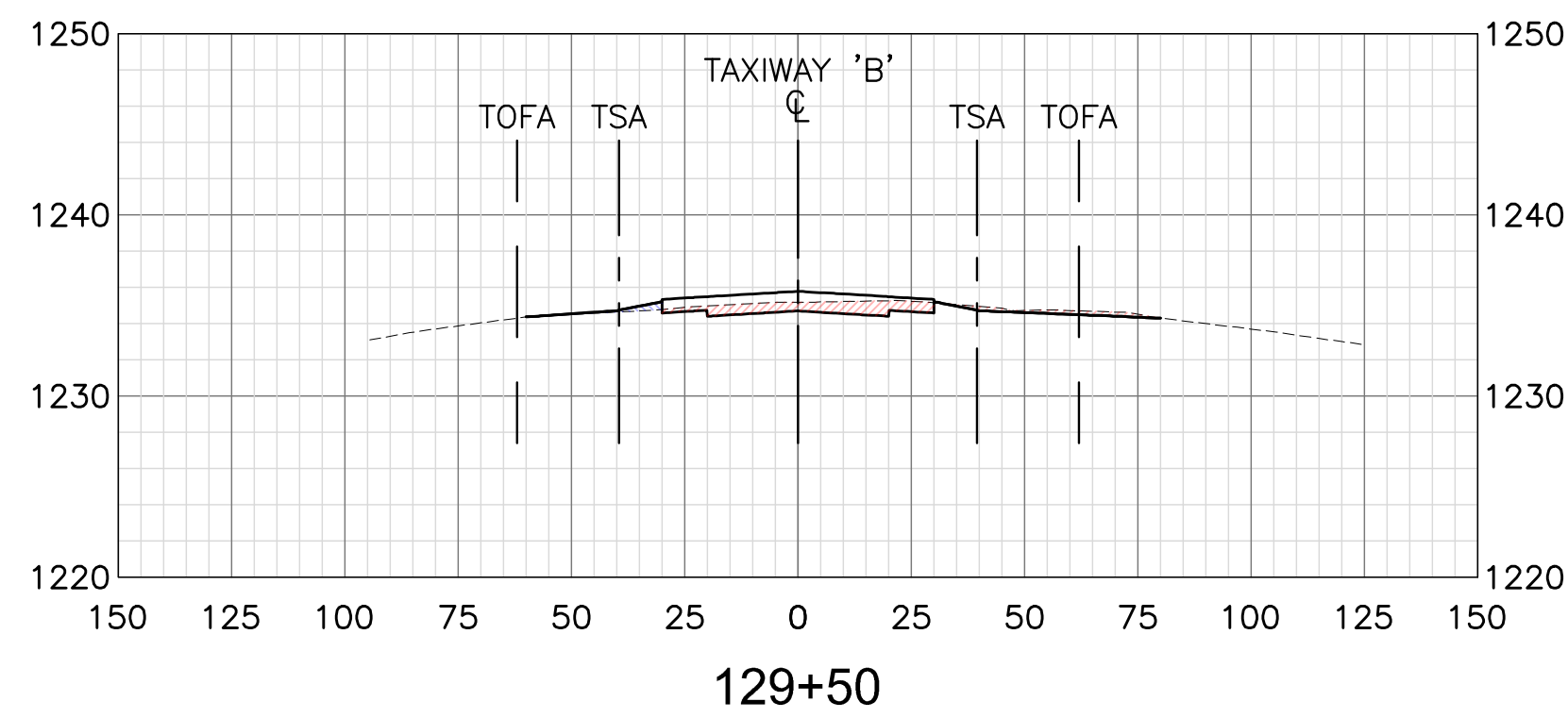
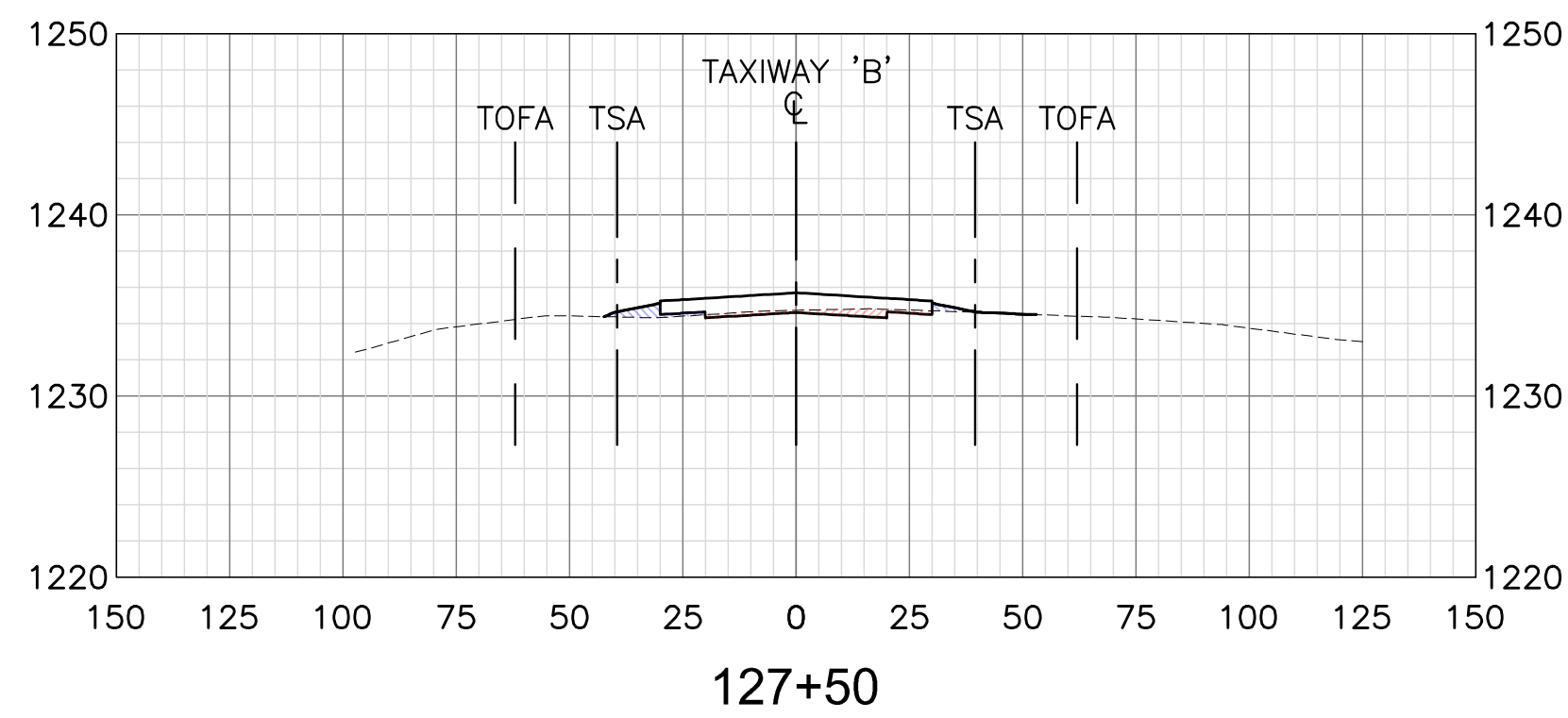
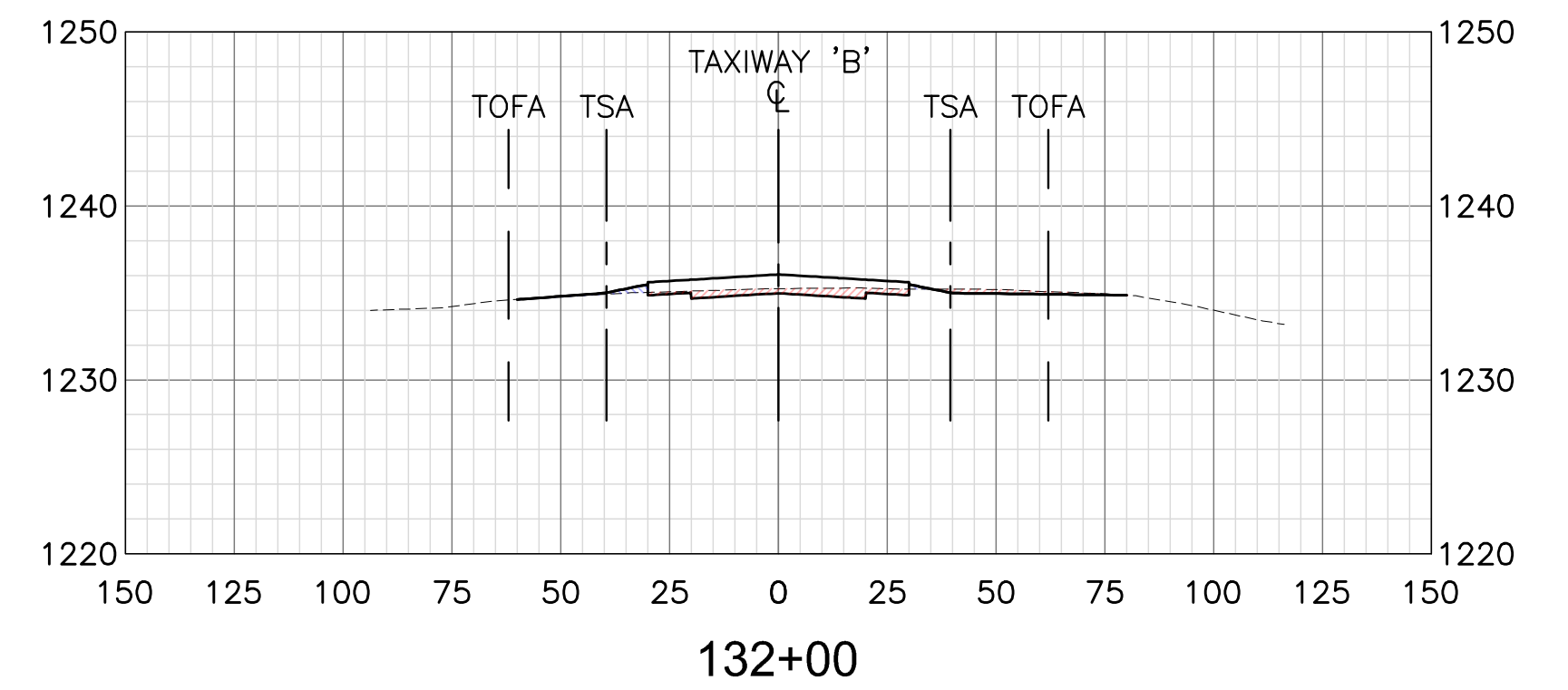
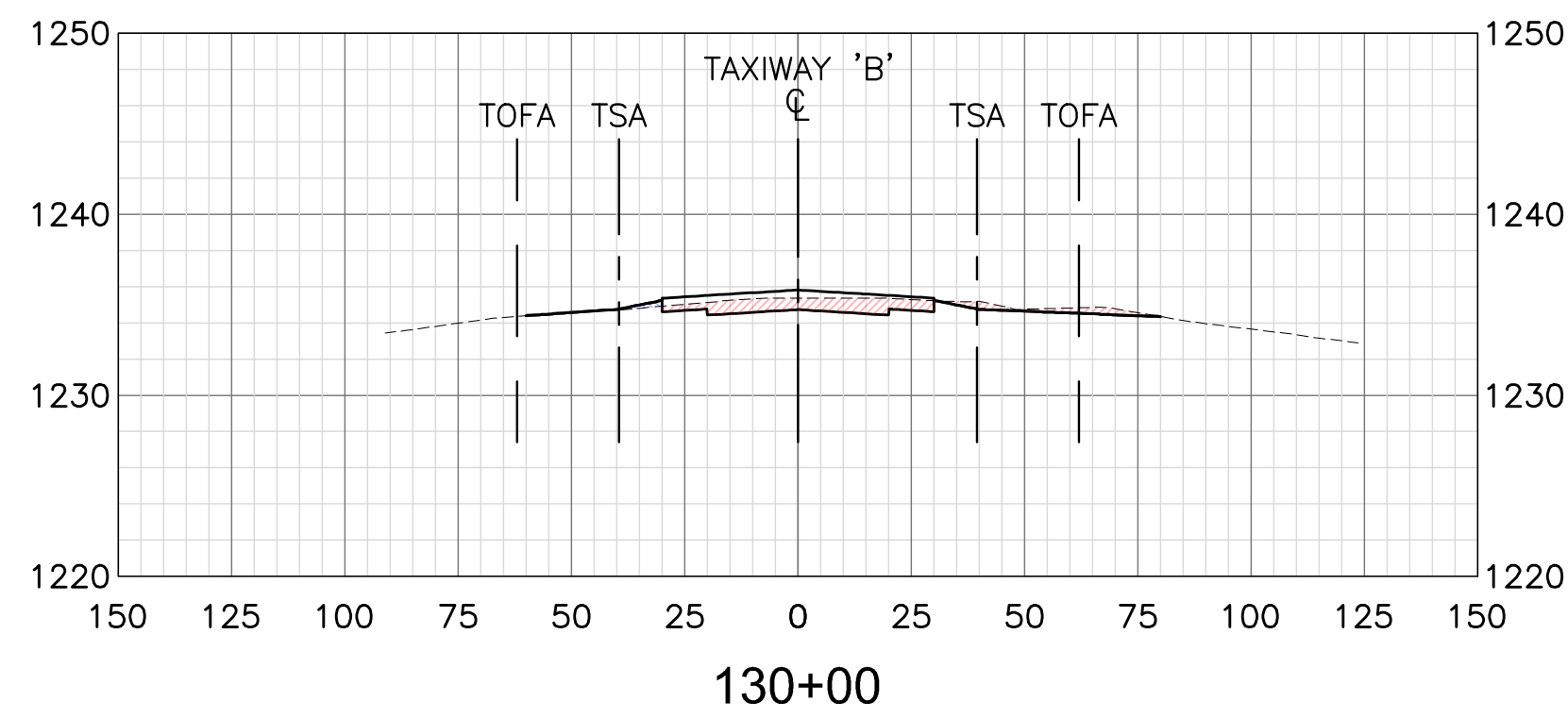
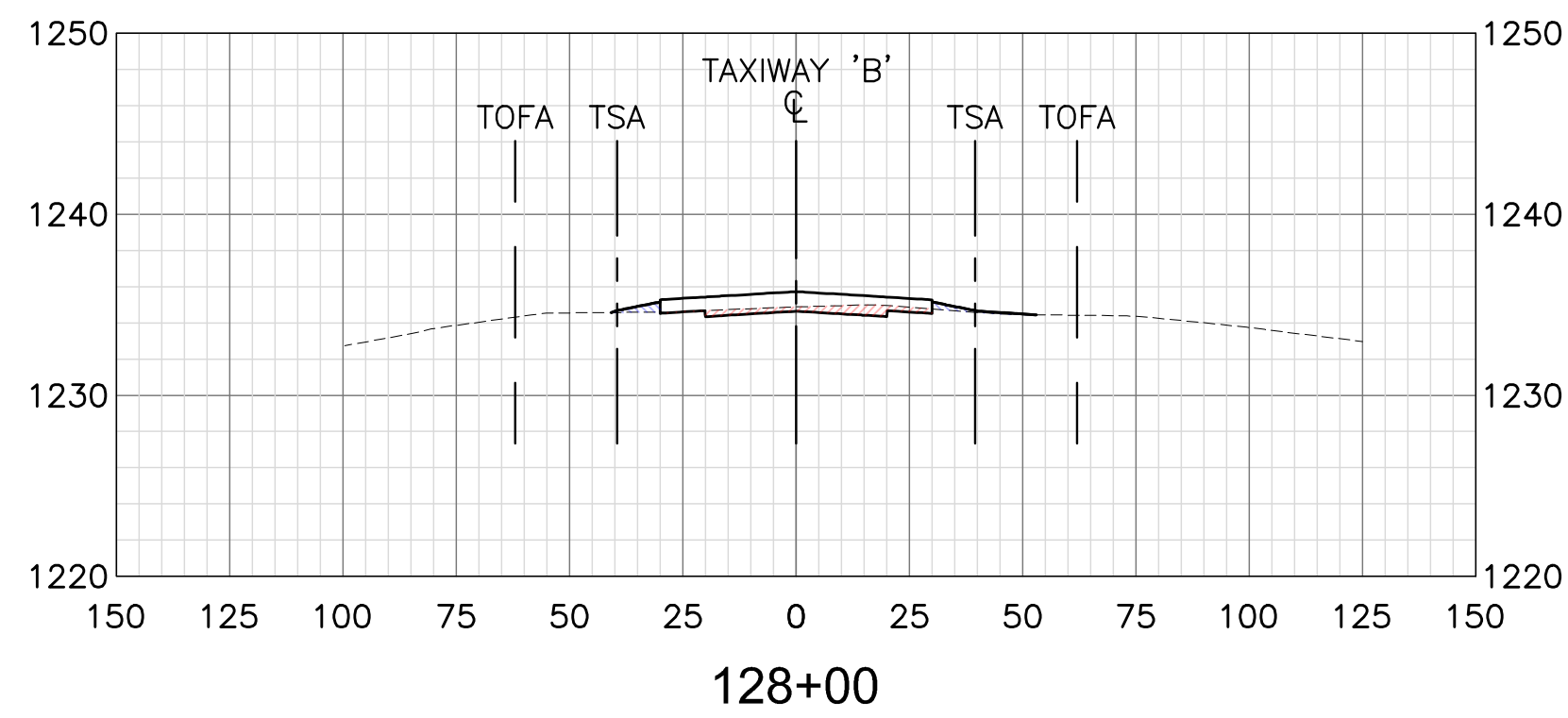
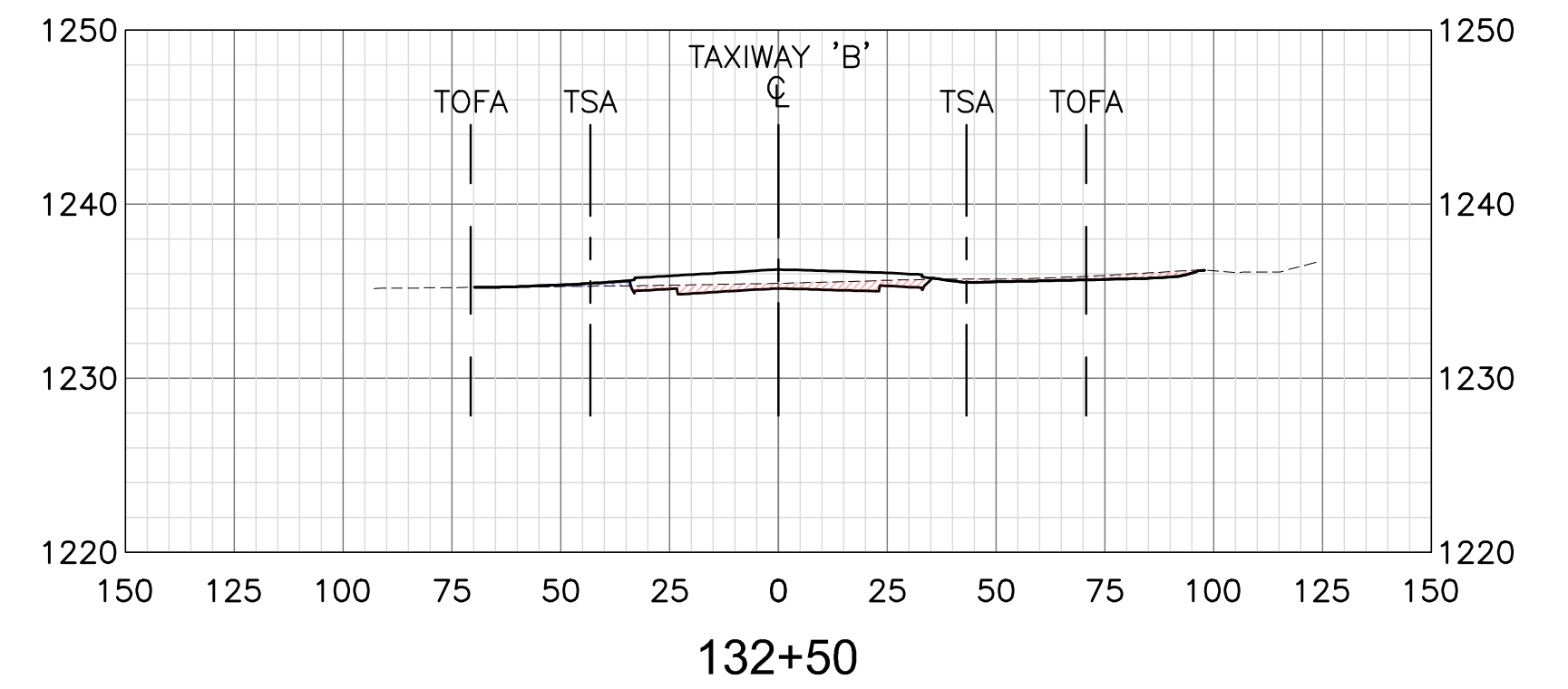
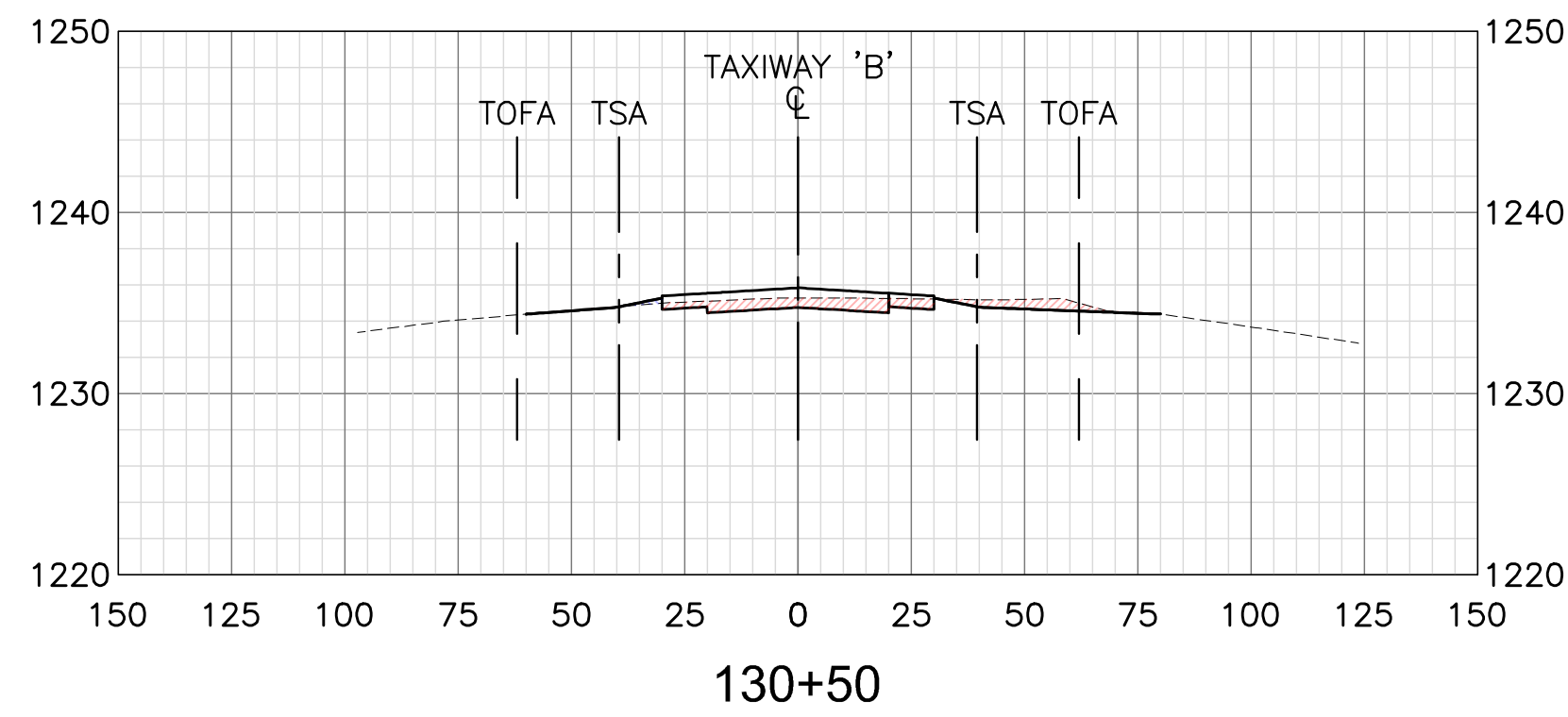
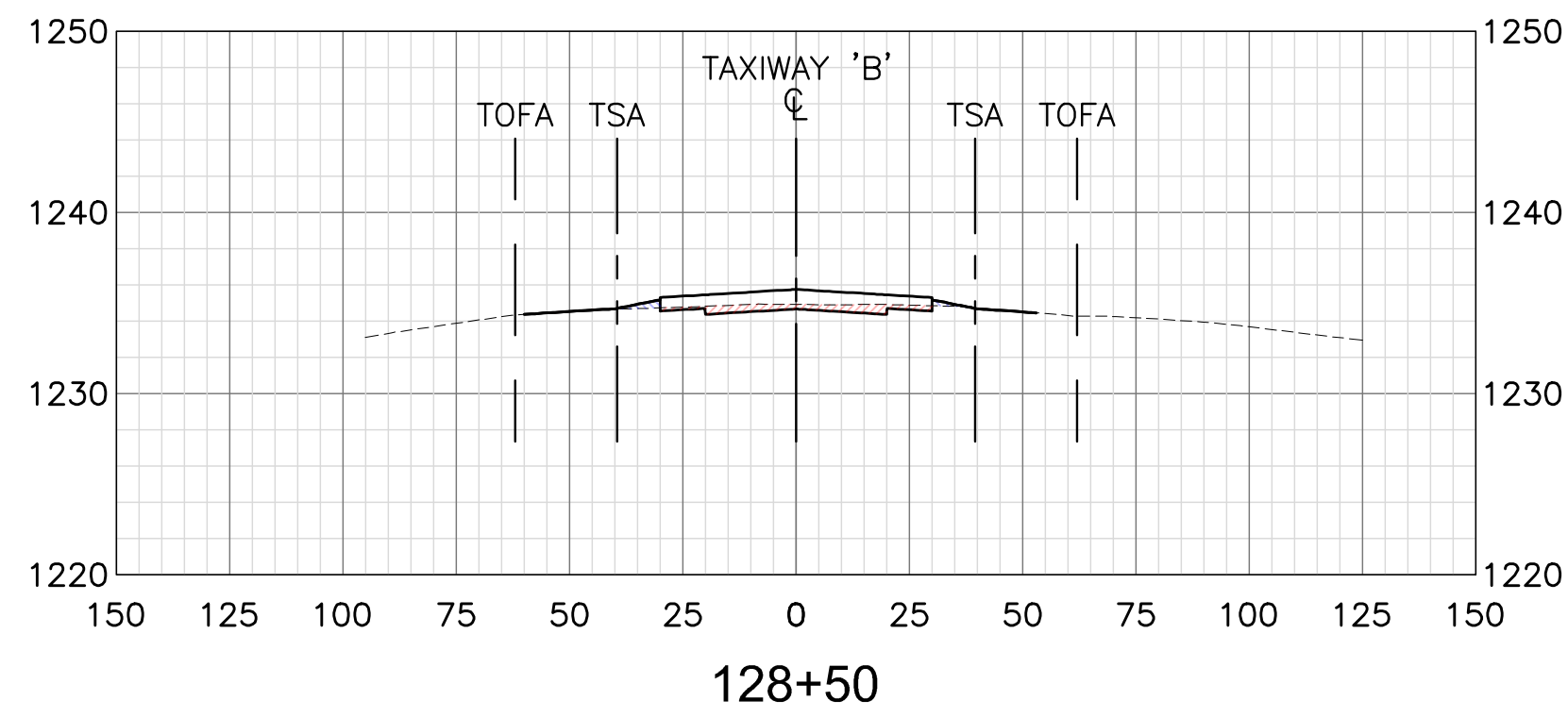
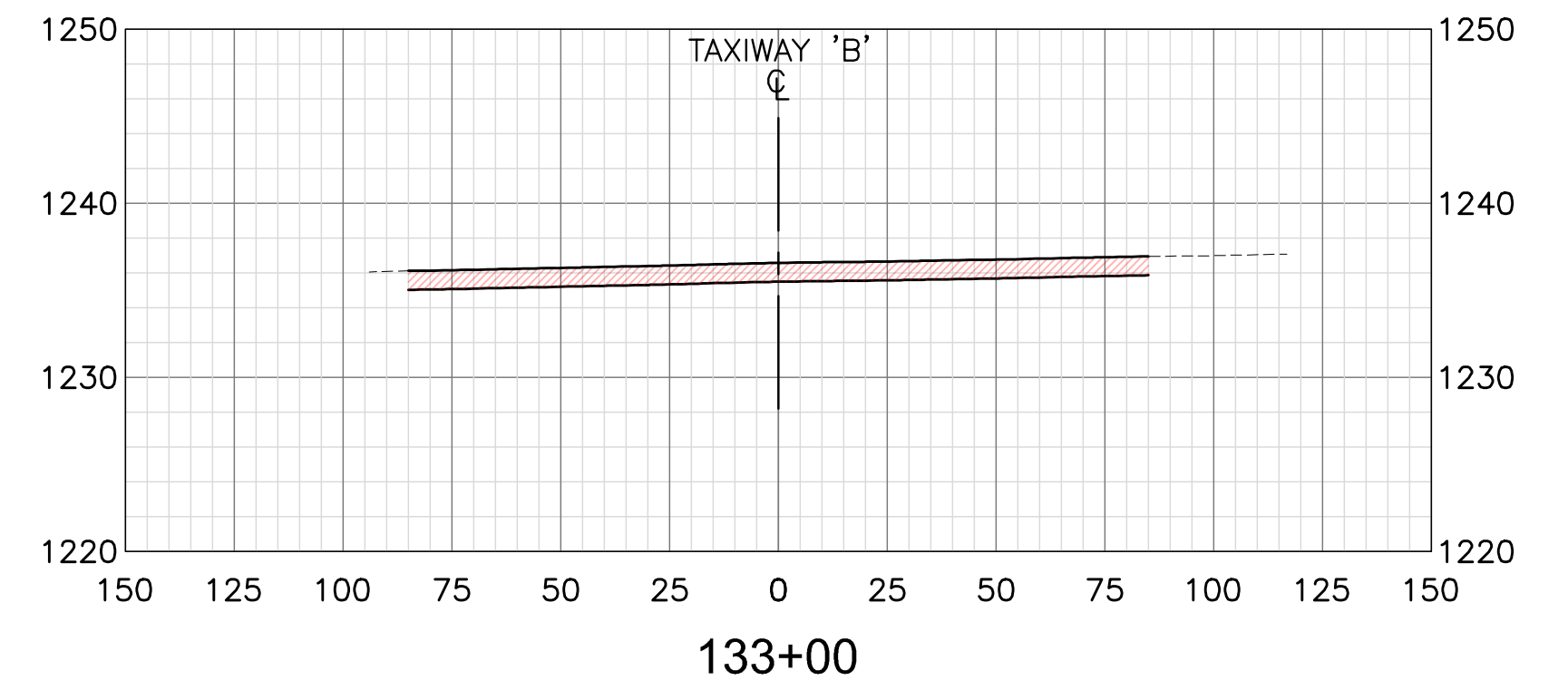
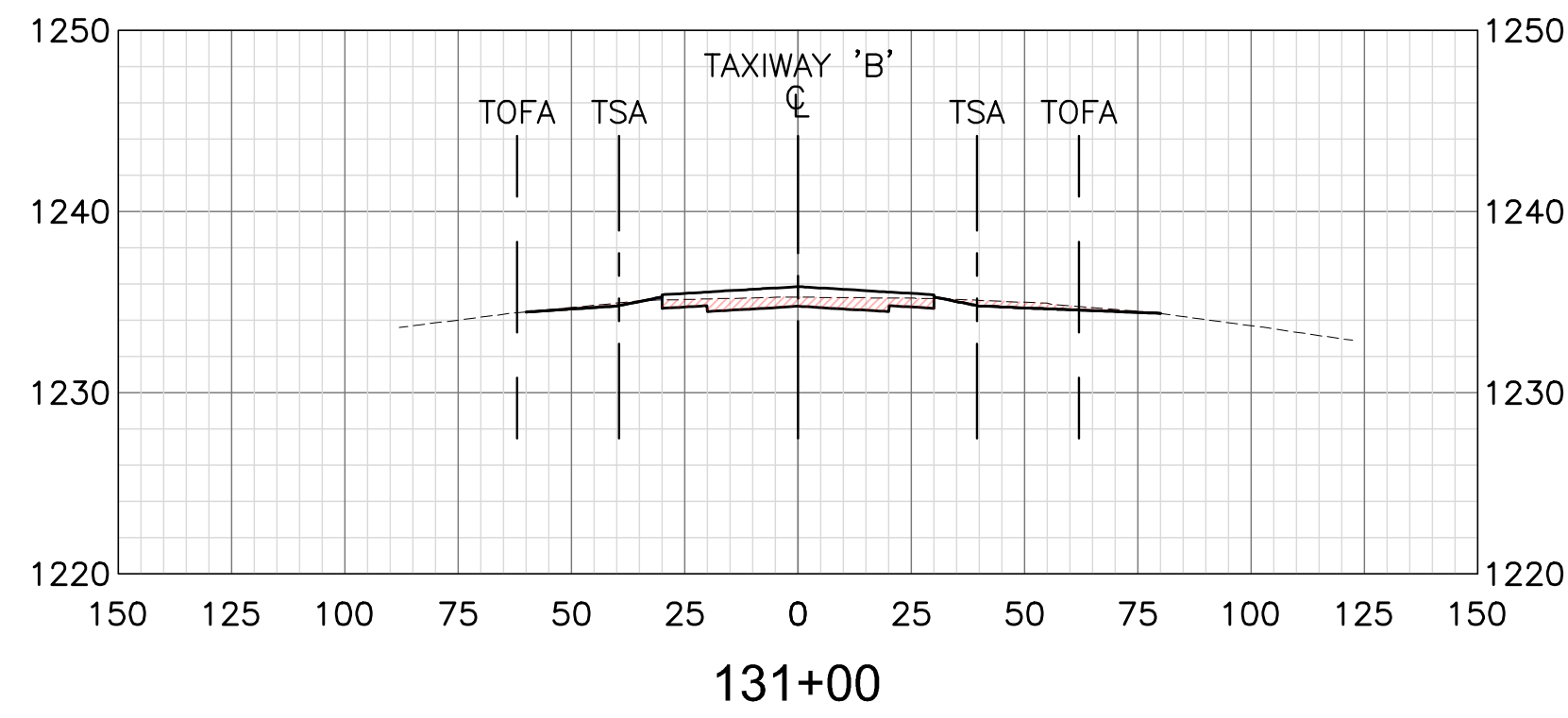
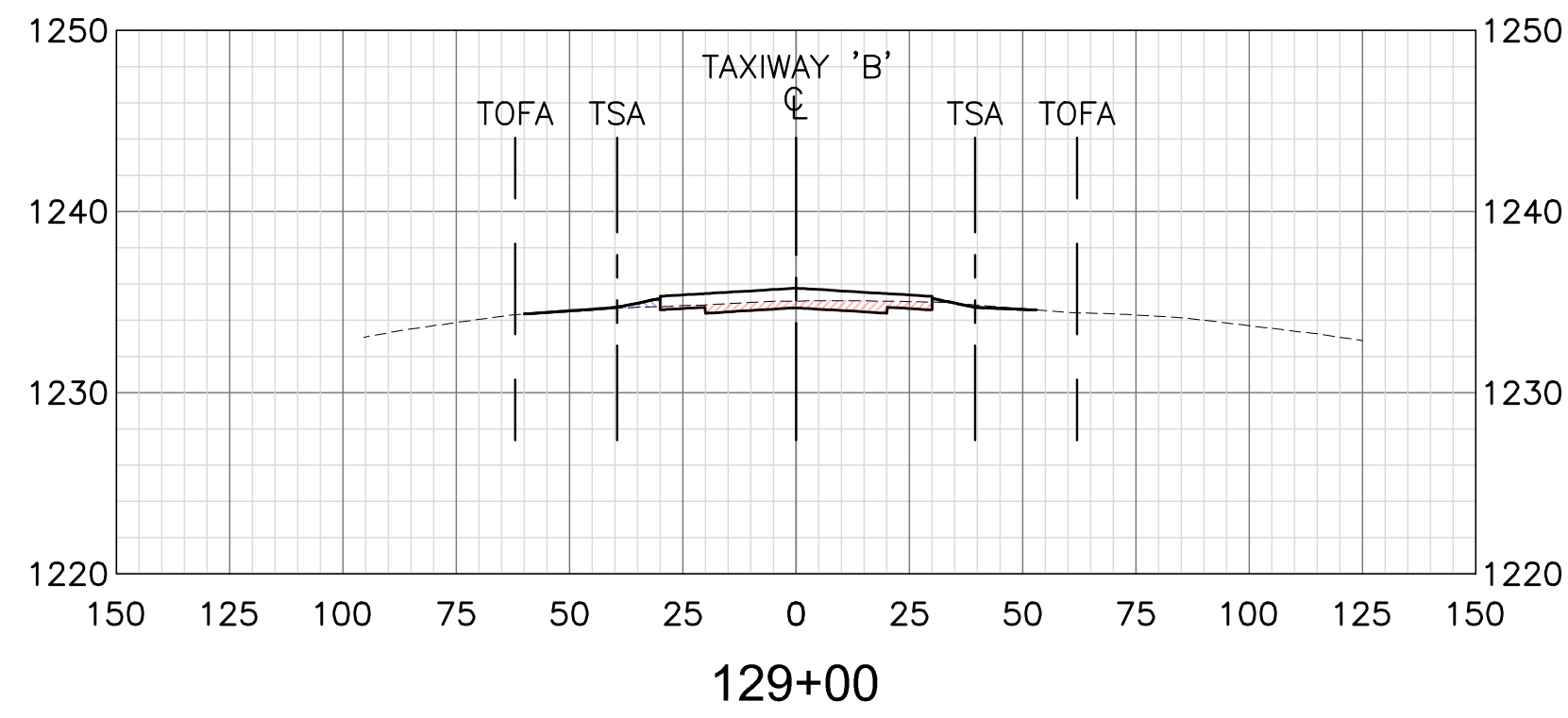


CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

CROSS SECTIONS  
 SHEET 2

PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-XX.DWG  
 DWG NO.  
**X1.2**  
 SHEET 36 OF 39

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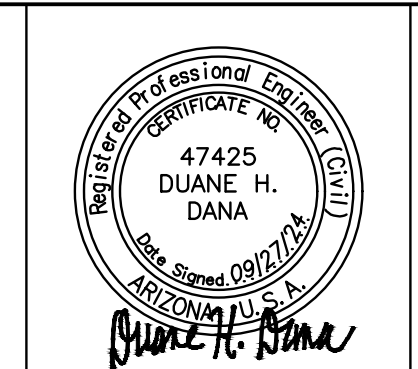


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1"=10' (V)



REV. NO.	DATE	DRWN	CHKD	REMARKS

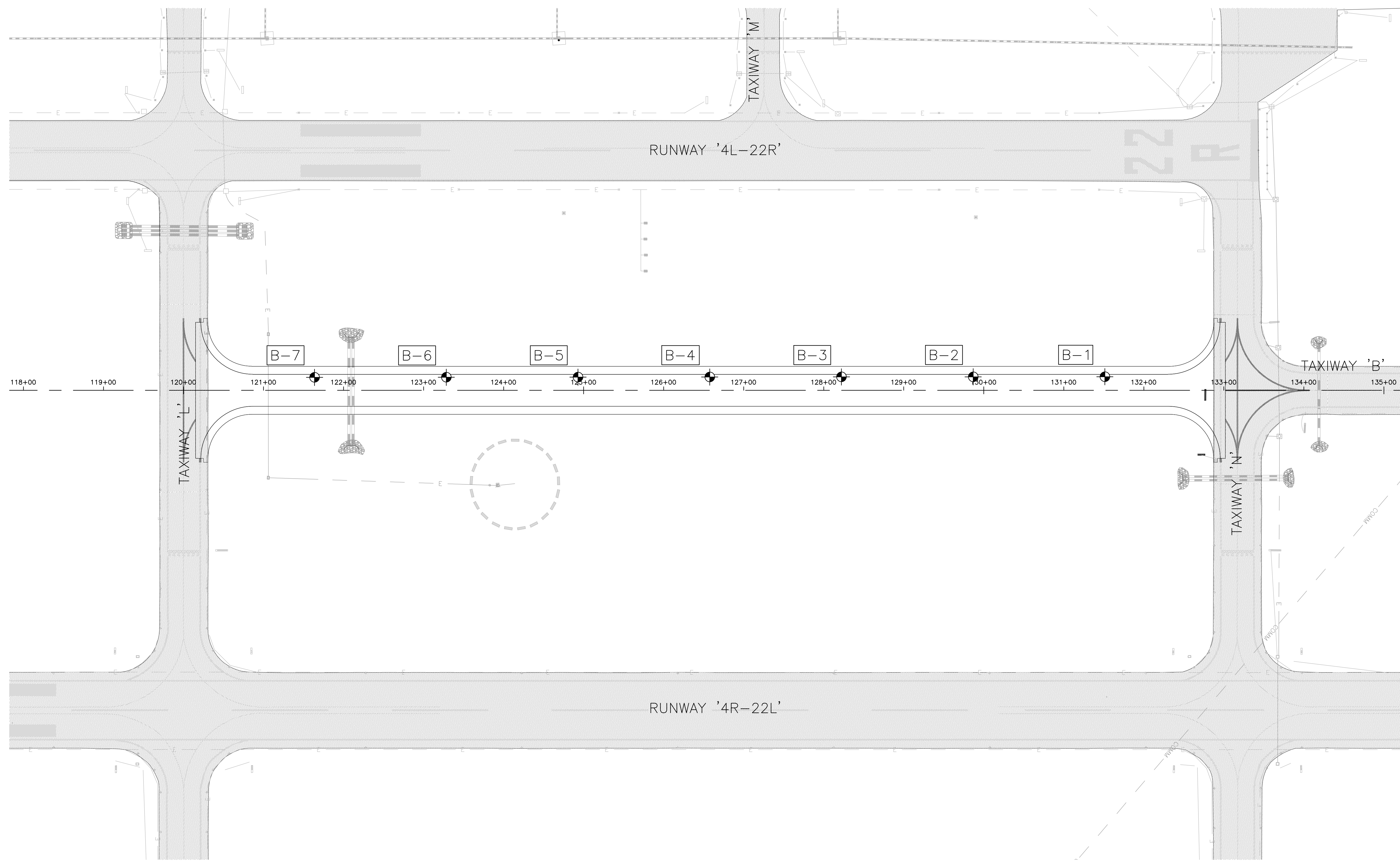
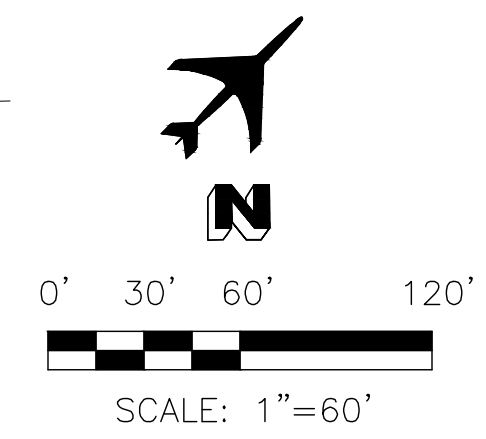
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 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24



CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

CROSS SECTIONS  
 SHEET 3

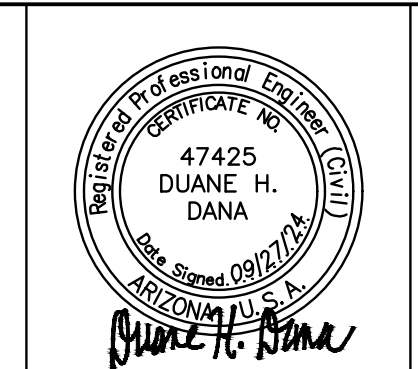
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**X1.3**  
 SHEET 37 OF 39



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REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
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 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24

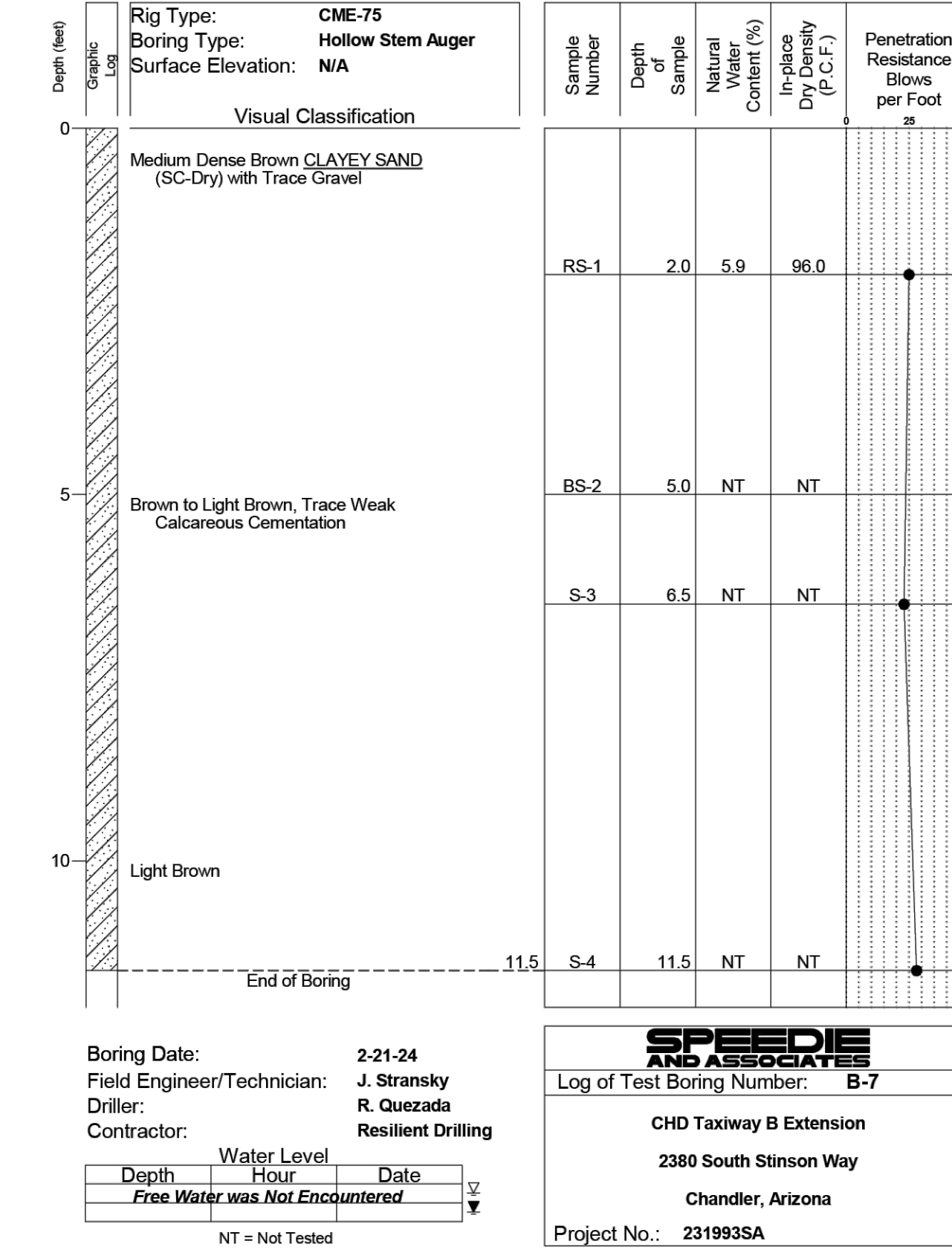
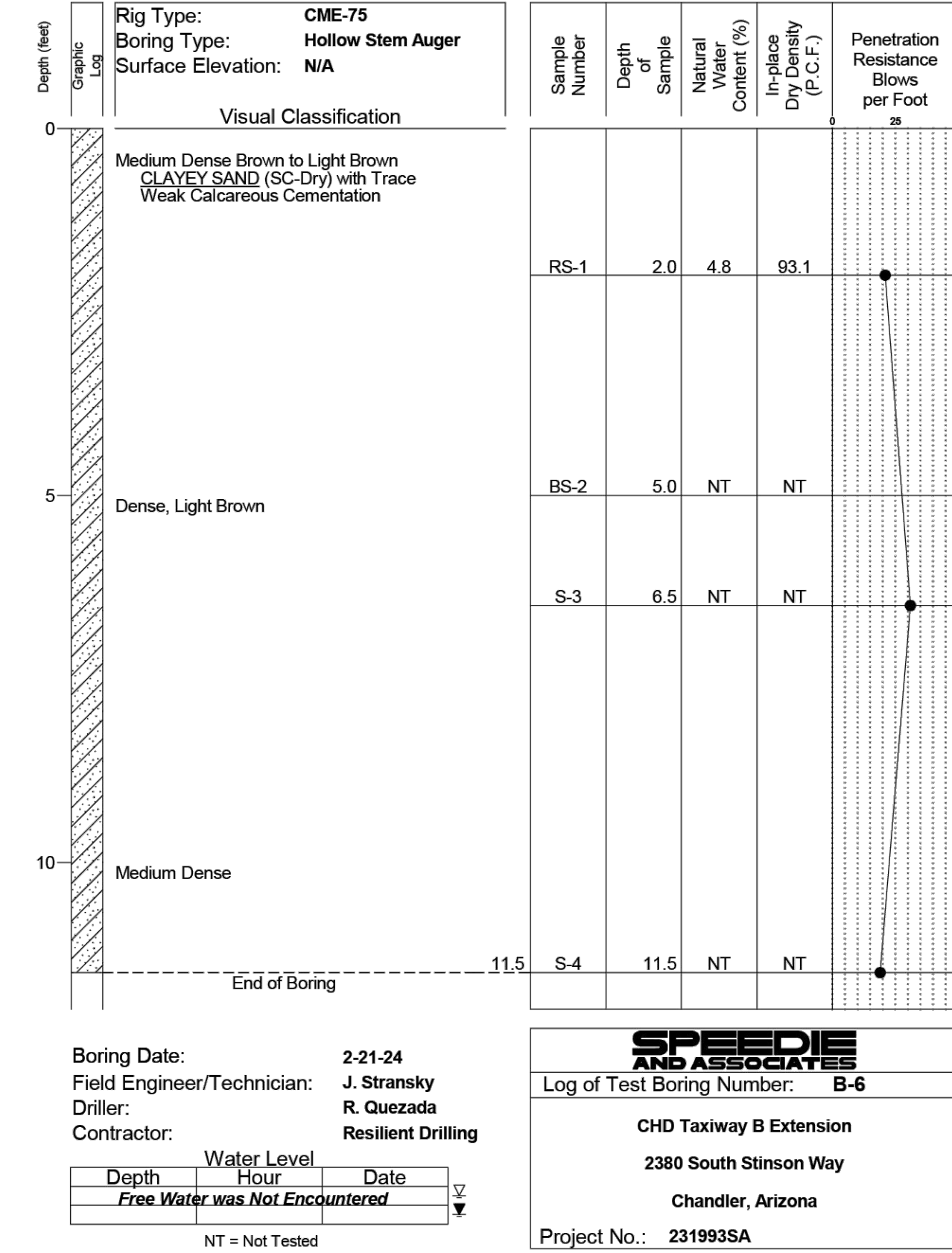
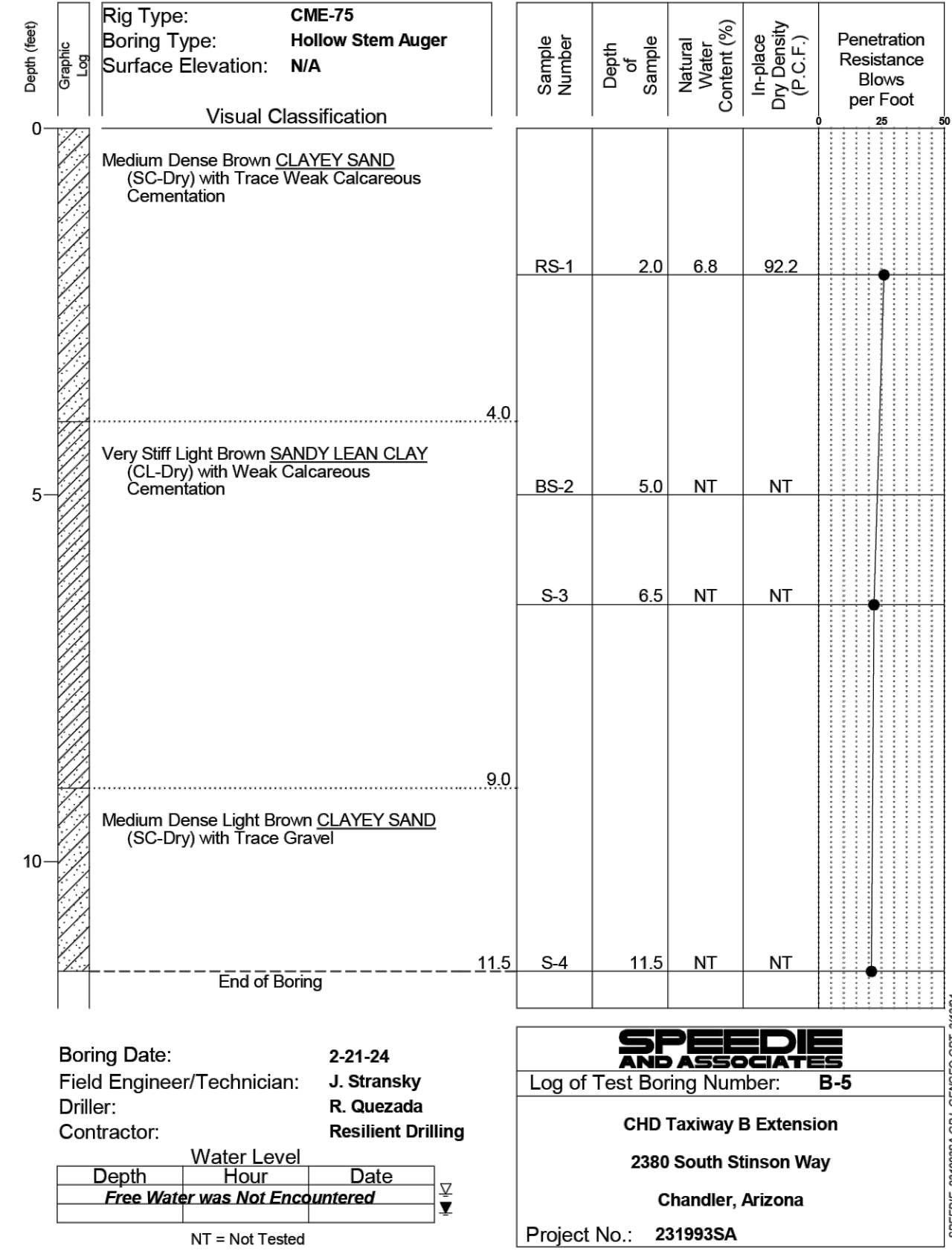
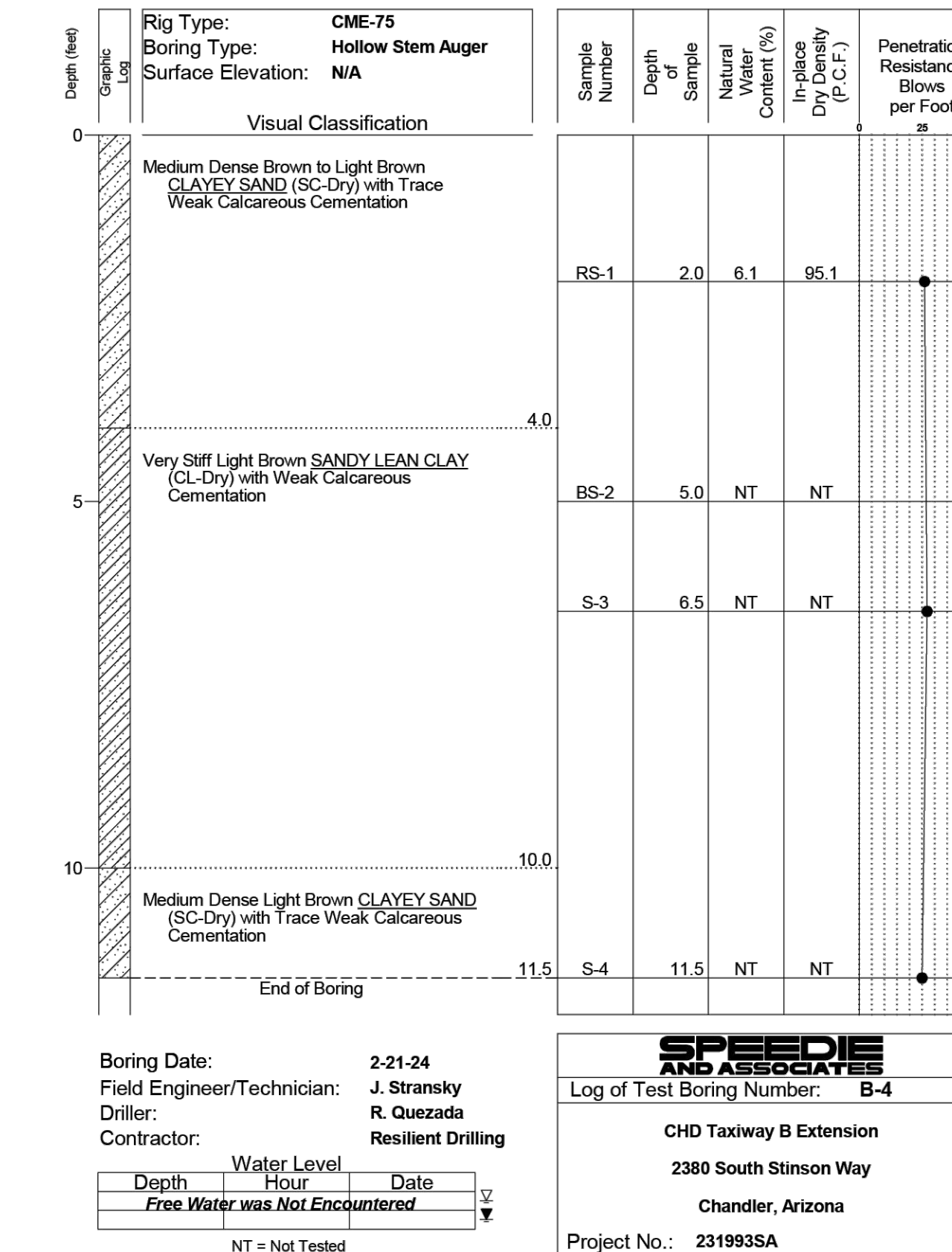
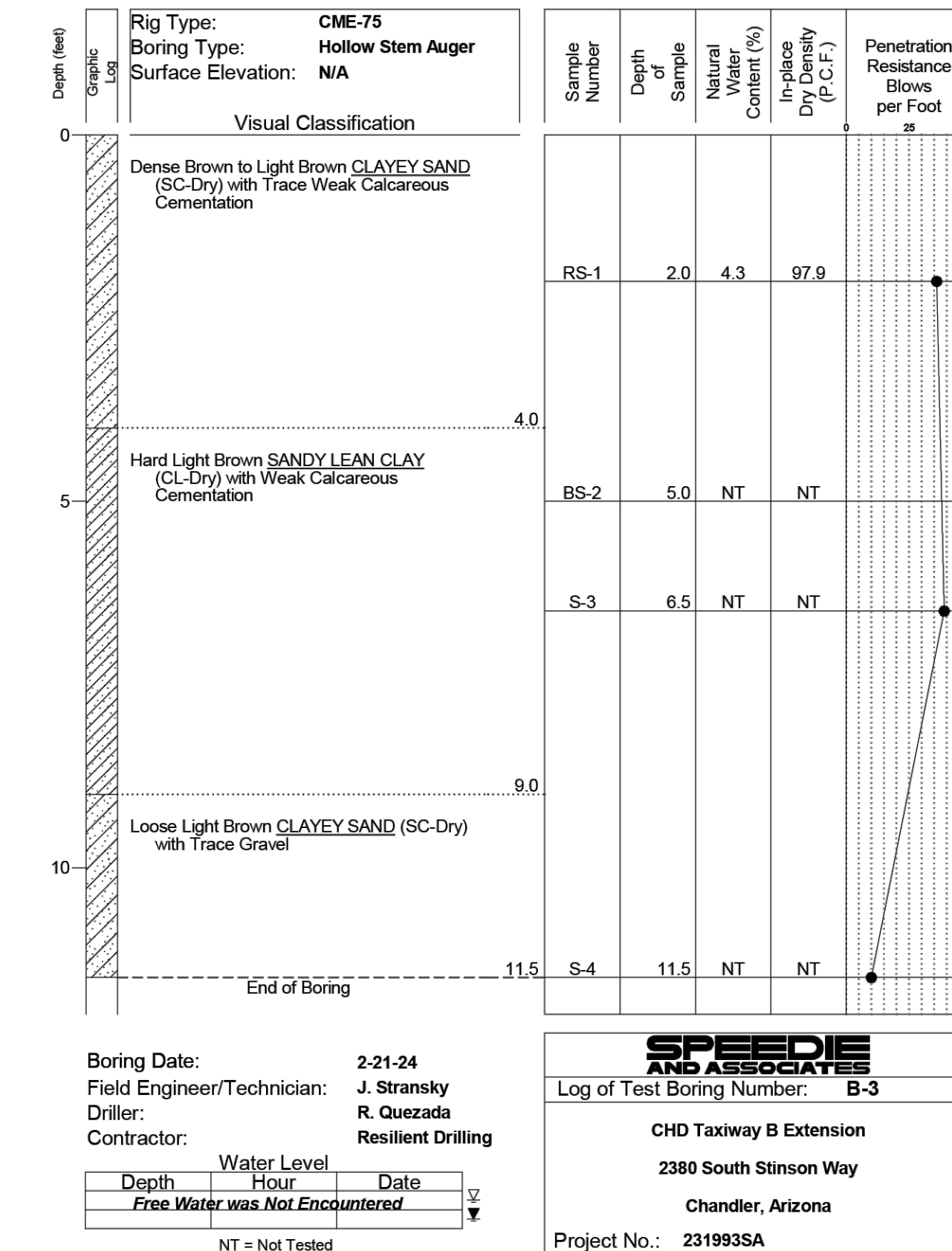
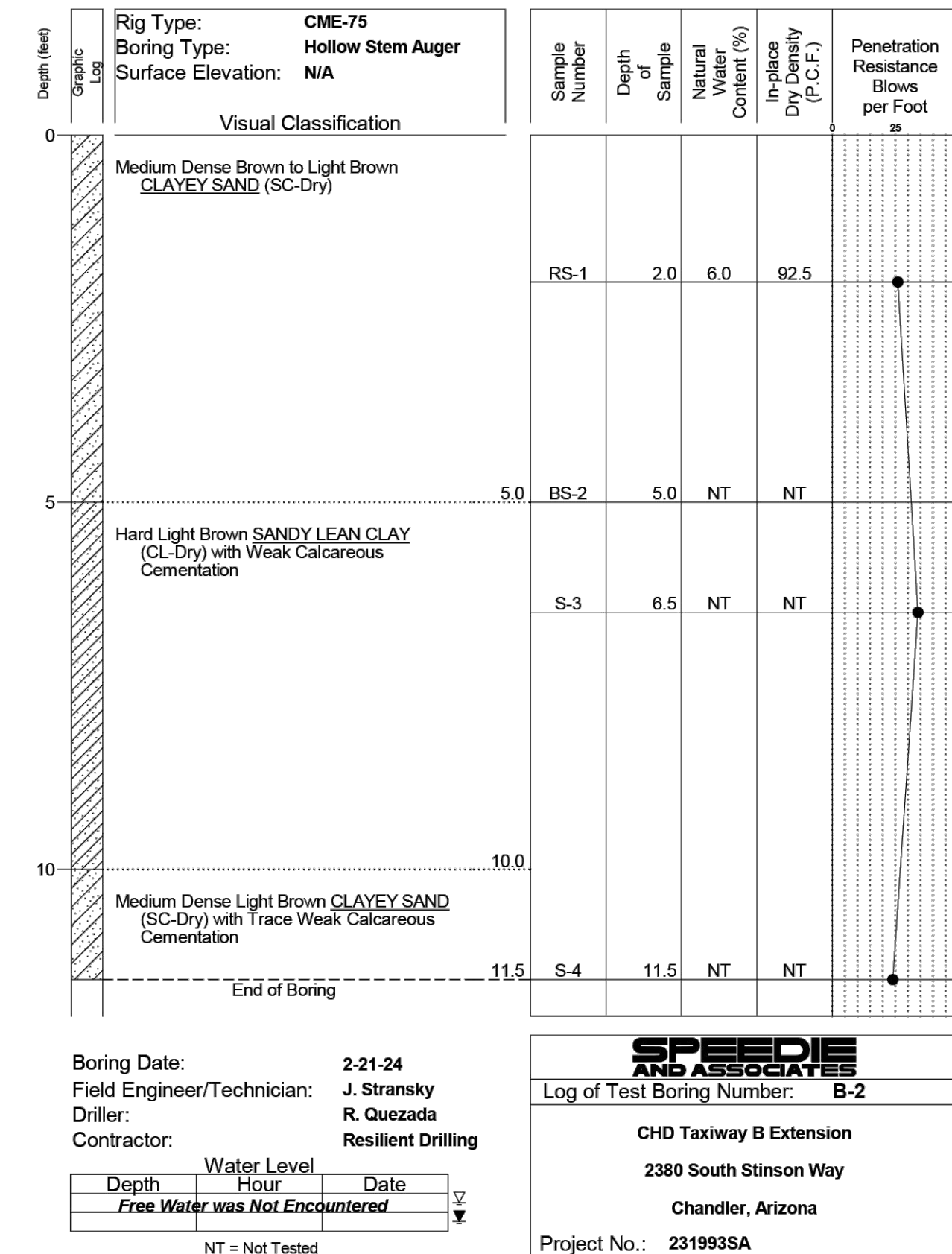
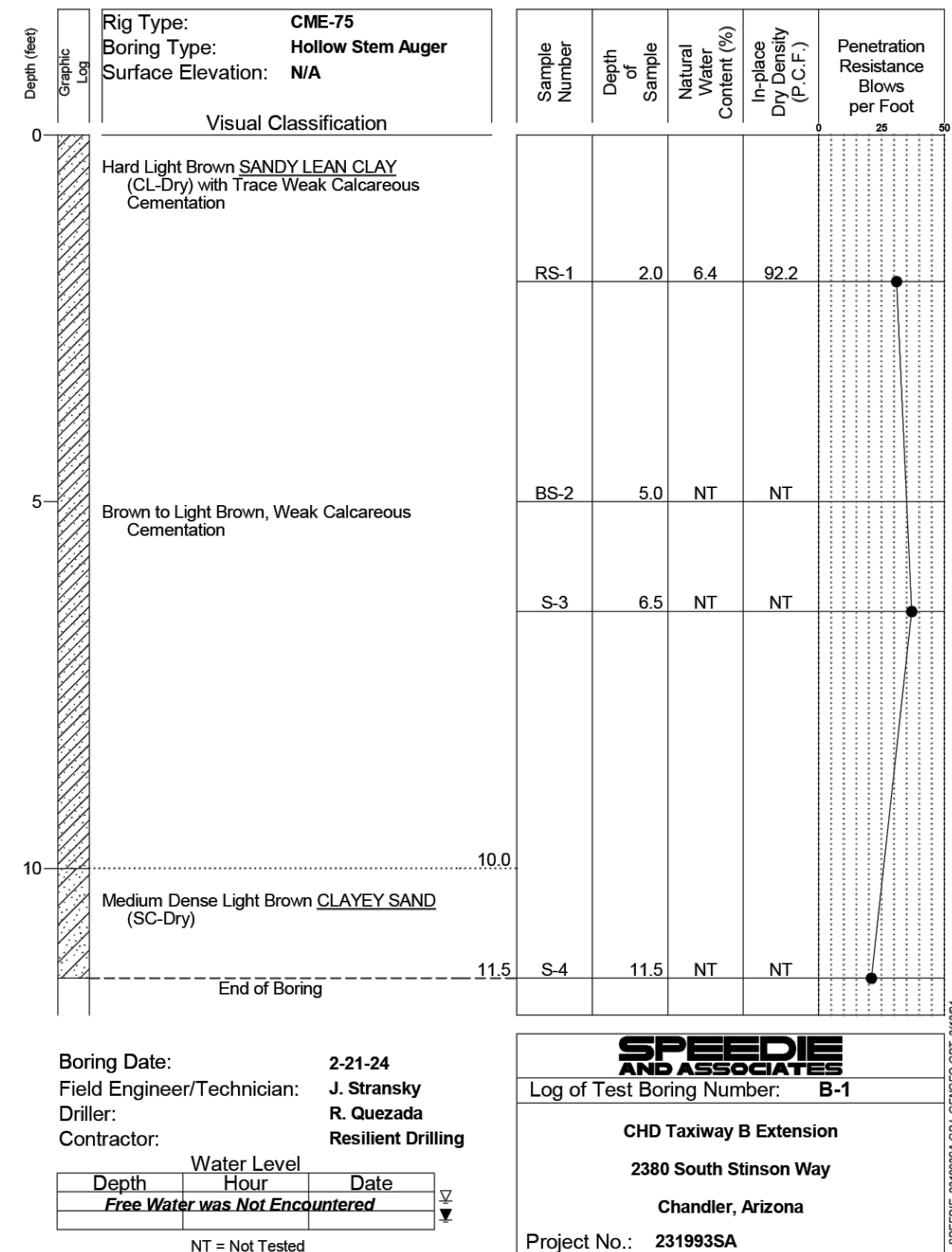


CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

BORING LOCATION MAP  
 PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-GT1X.DWG  
 DWG NO.  
**GT1.0**  
 SHEET 38 OF 39



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REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
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CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

BORING LOGS



PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-GT1X.DWG  
 DWG NO.  
**GT1.1**  
 SHEET 39 OF 39



# CITY OF CHANDLER, ARIZONA CHANDLER MUNICIPAL AIRPORT TAXIWAY 'B' IMPROVEMENTS PHASE 1: TAXIWAY 'L' TO TAXIWAY 'N'

**CITY PROJECT NO. AI2302.401**

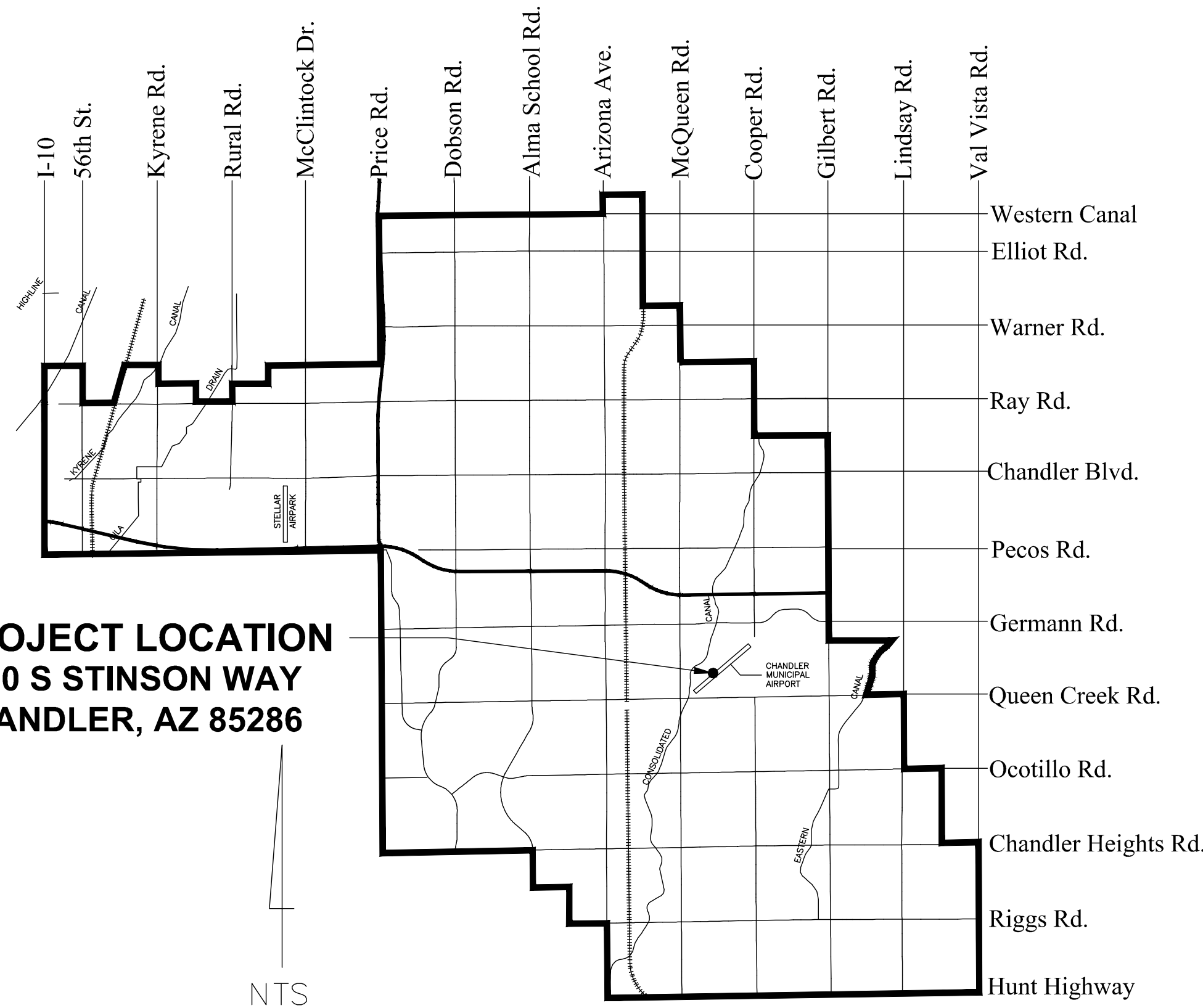
**ADOT GRANT NO. E3S4C**

**DATE: SEPTEMBER 27, 2024**

**MAYOR**  
KEVIN HARTKE

**VICE MAYOR**  
OD HARRIS

**COUNCIL**  
CHRISTINE ELLIS  
ANGEL ENCINAS  
MATT ORLANDO  
JANE POSTON  
MARK STEWART



### VICINITY MAP

#### CITY OF CHANDLER CHANDLER MUNICIPAL AIRPORT

2380 S STINSON WAY  
CHANDLER, AZ 85286  
PHONE NO. (480) 782 - 3545

**DIBBLE**  
DUANE DANA, PE  
1640 S. STAPLEY DRIVE, SUITE 120  
MESA, AZ 85204  
PHONE NO. (480) 757 - 7876  
DUANE.DANA@DIBBLECORP.COM

**EROSION AND SEDIMENT CONTROL PLAN NOTES:**

1. THE OWNER/OPERATOR OR CONTRACTOR SHALL NOTIFY THE CITY OF CHANDLER PUBLIC WORKS INSPECTOR ASSIGNED TO THE SITE 24 HOURS PRIOR TO COMMENCING ANY LAND DEVELOPMENT OR GRADING ACTIVITIES. NOTE: A NOTICE OF INTENT (NOI) SHALL BE SUBMITTED TO AN APPROVED BY THE ARIZONA DEPARTMENT OF ENVIRONMENTAL QUALITY (ADEQ) PRIOR TO COMMENCING LAND DEVELOPMENT OR GRADING ACTIVITIES.
2. A COPY OF THE APPROVED GRADING AND DRAINAGE PLAN FOR THIS PROJECT, ALONG WITH A COPY OF AN APPROVED NOI, THE EROSION AND SEDIMENT CONTROL PLAN (ESCP) AND THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) SHALL BE MAINTAINED ON THE SITE AND AVAILABLE FOR REVIEW BY REPRESENTATIVES OF THE CITY OF CHANDLER AND THE ARIZONA DEPARTMENT OF ENVIRONMENTAL QUALITY. THOSE ELEMENTS OF THE GRADING AND DRAINAGE PLAN PERTINENT TO, OR REFERENCED ON THE ESCP SHALL BE CONSIDERED PART OF THE ESCP.
3. THE OWNER/OPERATOR OR CONTRACTOR SHALL POST A SIGN OR OTHER NOTICE NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE WITH THE FOLLOWING INFORMATION: AZPDES AUTHORIZATION NUMBER FOR THE PROJECT OR COPY OF THE NOTICE OF INTENT, THE NAME AND TELEPHONE NUMBER OF A LOCAL OFFICE OR ON-SITE CONTACT PERSON; A DESCRIPTION OF THE TYPE OF CONSTRUCTION PROJECT; AND THE LOCATION OF THE SWPPP.
4. DETAILS, STANDARDS, AND SPECIFICATIONS FOR APPLICABLE BEST MANAGEMENT PRACTICES (BMP'S) TO BE IMPLEMENTED ON-SITE SHALL BE ILLUSTRATED ON THE ESCP SHEETS. ADDITIONAL DETAILS, STANDARDS, AND SPECIFICATIONS FOR BMP'S IMPLEMENTED ON-SITE FOLLOWING FINAL SUBMITTAL OF THE ESCP SHALL BE ADDED AND MAINTAINED WITHIN THE SWPPP.
5. THE OWNER/OPERATOR SHALL PERFORM, AT A MINIMUM, A VISUAL INSPECTION OF THE CONSTRUCTION SITE ONCE EVERY FOURTEEN DAYS AND ALSO WITHIN 24 HOURS OF A RAINFALL EVENT GREATER THAN OR EQUAL TO A HALF OF AN INCH OR MORE. THE OWNER/OPERATOR OR CONTRACTOR SHALL PREPARE AN INSPECTION REPORT DOCUMENTING HIS/HER FINDINGS ON THE CONDITIONS OF THE SWPPP CONTROLS AND NOTE ANY LOCATION(S) OF DISCHARGES OF SEDIMENT OR OTHER POLLUTANTS FROM THE SITE, BMP'S THAT REQUIRE MAINTENANCE, ADDITIONAL BMP'S NEEDED, CORRECTIVE ACTIONS REQUIRED AND IDENTIFICATION OF ALL SOURCES OF NON-STORM WATER DISCHARGES AND THE ASSOCIATED POLLUTION PREVENTION CONTROL MEASURES AND IDENTIFICATION OF EVIDENCE OF OR POTENTIAL FOR, POLLUTANT DISCHARGE FROM THE SITE.
6. THE OWNER/OPERATOR OR CONTRACTOR SHALL AMEND THE SWPPP AS NECESSARY DURING THE COURSE OF CONSTRUCTION IF THERE ARE CHANGES AT THE SITE THAT HAVE A SIGNIFICANT EFFECT ON THE DISCHARGE OF POLLUTANTS OR IF INSPECTIONS OR INVESTIGATIONS HAVE DETERMINED THAT THE SWPPP IS NO LONGER EFFECTIVE AT MINIMIZING POLLUTANTS IN STORMWATER DISCHARGES FROM THE SITE.
7. THE OWNER/OPERATOR OR CONTRACTOR IS REQUIRED TO MAINTAIN COMPLIANCE WITH ALL REQUIREMENTS OUTLINED IN THE ARIZONA POLLUTANT DISCHARGE ELIMINATION SYSTEM (AZPDES) CONSTRUCTION GENERAL PERMIT ADMINISTERED BY ADEQ.
8. THE OWNER/OPERATOR OR CONTRACTOR SHALL SUBMIT A NOTICE OF TERMINATION (NOT) FOLLOWING COMPLETION OF CONSTRUCTION AND PERMANENT STABILIZATION AS OUTLINED IN THE ARIZONA POLLUTANT DISCHARGE ELIMINATION SYSTEM (AZPDES) CONSTRUCTION GENERAL PERMIT ADMINISTERED BY ADEQ.
9. THE OWNER/OPERATOR OR CONTRACTOR SHALL OBTAIN A DUST CONTROL PERMIT FROM THE MARICOPA COUNTY AIR QUALITY DEPARTMENT FOR ALL DUST GENERATING ACTIVITIES THAT DISTURB 0.10 ACRE OR GREATER AND IMPLEMENT ALL APPLICABLE CONTROL MEASURES AS OUTLINED IN THE MARICOPA COUNTY AIR POLLUTION CONTROL RULE 310.

**STANDARD DETAILS, SPECIFICATIONS, BEST MANAGEMENT PRACTICES AND APPLICABLE LAW:**

- WORK SHALL BE DONE IN ACCORDANCE WITH THIS PLAN, THE FOLLOWING SPECIFICATIONS & DETAILS AND APPLICABLE LAW.
1. CITY OF CHANDLER CODE CHAPTER 45 AND CITY OF CHANDLER MANUAL ON STORMWATER QUALITY PROTECTION.
  2. CURRENT EDITION OF THE CITY OF CHANDLER STANDARD DETAILS AND SPECIFICATIONS.
  3. CITY OF CHANDLER TECHNICAL DESIGN MANUAL #3 - STORM DRAINAGE SYSTEM DESIGN.
  4. DRAINAGE DESIGN MANUAL FOR MARICOPA COUNTY, ARIZONA - VOLUME III - EROSION CONTROL, SEPTEMBER 2004 DRAFT - BEST MANAGEMENT PRACTICES (BMP'S), BMP NUMBERS ARE SPECIFIED IN THIS DOCUMENT.
  5. BEST MANAGEMENT PRACTICES WILL BE FOLLOWED AS OUTLINED IN THIS ESCP AND THE SWPPP.
  6. THE FOLLOWING ON-SITE MANAGEMENT PRACTICES SHOULD BE CONSIDERED WHENEVER POSSIBLE TO LIMIT THE EXPOSURE OF THE SITE TO EROSION, SEDIMENTATION, AND DISCHARGES OF POLLUTANTS AND/OR WASTES.
    - A. SEQUENCE CONSTRUCTION ACTIVITIES SO THAT DISTURBED AREAS ARE NOT EXPOSED FOR EXTENDED PERIODS OF TIME.
    - B. SCHEDULE LANDSCAPING AND OTHER WORK THAT PERMANENTLY STABILIZES DISTURBED AREAS IMMEDIATELY FOLLOWING LAND DEVELOPMENT ACTIVITIES AND FINAL CONTOUR.
    - C. IMPLEMENT TEMPORARY STABILIZATION PRACTICES WHEN DISTURBED AREAS WILL BE EXPOSED FOR PERIODS GREATER THAN 14 DAYS.
    - D. INSTALL AND MAINTAIN BMP'S TO CONTROL EROSION, SEDIMENTATION, AND DISCHARGES OF POLLUTANTS AND/OR WASTES.

SHEET INDEX		
SHT NO.	DWG NO.	SHEET TITLE
EROSION & SEDIMENT CONTROL PLAN		
SW1	SW1.0	EROSION & SEDIMENT CONTROL PLAN COVER SHEET
SW2	SW1.1	EROSION & SEDIMENT CONTROL PLAN NOTES & DETAILS
SW3	SW1.2	EROSION & SEDIMENT CONTROL PLAN

**FEMA FLOOD ZONE INFORMATION:**

THIS PROJECT AREA IS ENTIRELY WITHIN THE LIMITS OF THE CHANDLER MUNICIPAL AIRPORT WITHIN FLOOD HAZARD AREA ZONE X, FIRM PANELS 04013C2739M AND 04013C2743M.

**STATUS: 100%**



SHEET NO.  
**SW1.0**  
SW1 OF SW3

REV. NO.	DATE	DRWN	CHKD	REMARKS



PART 1 – TO BE COMPLETED BY DESIGN ENGINEER

I. PROJECT DESCRIPTION

A. OWNER/OPERATOR NAME & ADDRESS:

CITY OF CHANDLER  
CHANDLER MUNICIPAL AIRPORT  
2380 S STINSON WAY  
CHANDLER, AZ 85286

B. PROJECT LOCATION/ADDRESS:

CHANDLER MUNICIPAL AIRPORT  
2380 S STINSON WAY  
CHANDLER, AZ 85286

C. PROJECT DESCRIPTION:

CONSTRUCT NEW TAXIWAY

B. PERMANENT CONTROL MEASURES IMPLEMENTED

POST CONSTRUCTION ACTIVITIES:

- \_\_\_ SEDIMENT BASIN
- \_\_\_ ROCK/RIPRAP LINED CHANNEL
- \_\_\_ SCUPPERS/SPILLWAYS/CATCH BASINS
- \_\_\_ DRYWELLS
- \_\_\_ ROCK INLET/OUTLET PROTECTION
- \_\_\_ LANDSCAPE/GROUND COVER
- \_\_\_ OTHER (DESCRIBE):

V. SITE INSPECTIONS AND MAINTENANCE

A. INSPECTION FREQUENCY

- \_\_\_ AT LEAST ONCE EVERY 7 DAYS
- EVERY 14 DAYS AND WITHIN 24 HOURS AFTER A RAINFALL EVENT OF 0.5 INCHES OR MORE.

B. INSPECTION REPORT – AN INSPECTION REPORT WILL BE COMPLETED BY THE CONTRACTOR AUTHORIZED REPRESENTATIVE FOR EACH SITE INSPECTION AND SHALL BE RETAINED FOR AT LEAST 3 YEARS FOLLOWING PERMIT EXPIRATION OR TERMINATION.

C. MAINTENANCE OF SWPPP

THE SITE SPECIFIC SWPPP MUST BE AMENDED WITHIN 15 DAYS WHEN A CHANGE IN DESIGN, CONSTRUCTION, OPERATION, OR MAINTENANCE THAT MAY HAVE A SIGNIFICANT EFFECT ON THE DISCHARGE OF POLLUTANTS TO THE WATERS OF THE UNITED STATES.

VI. CERTIFICATION OF COMPLIANCE WITH FEDERAL, STATE AND LOCAL REGULATIONS.

A. THIS EROSION AND SEDIMENT CONTROL PLAN HAS BEEN PREPARED IN ACCORDANCE WITH THE MOST CURRENT VERSION OF THE ARIZONA POLLUTANT DISCHARGE ELIMINATION SYSTEM GENERAL PERMIT FOR CONSTRUCTION ACTIVITIES, ADMINISTERED BY THE ARIZONA DEPARTMENT OF ENVIRONMENTAL QUALITY (ADEQ).

SIGNATURE: Duane H. Dana  
DATE: 09/27/2024  
NAME: DUANE DANA  
TITLE: PROJECT MANAGER

II. HYDROLOGIC INFORMATION

A. PROJECT SIZE:

LENGTH (MI.) \_\_\_\_\_  
AREA (AC.) 7.7 ACRES

B. AREA TO BE DISTURBED (AC.):

7.3 ACRES

C. RUNOFF COEFFICIENT:

EXISTING 0.45  
DEVELOPED 0.70

D. RECEIVING WATER(S):

AIRPORT WEST DETENTION BASIN

III. SOIL STABILIZATION MEASURES

A. ALL DISTURBED AREAS, WHICH WILL NOT BE PAVED, WILL BE REVEGETATED AND/OR LANDSCAPED IN ACCORDANCE WITH THE APPROVED PLANS AND SPECIFICATIONS.

IV. EROSION/SEDIMENT CONTROL MEASURES

A. TEMPORARY CONTROL MEASURES IMPLEMENTED DURING CONSTRUCTION ACTIVITIES:

- \_\_\_ SILT FENCE
- ORGANIC FILTER BARRIERS
- \_\_\_ SEDIMENT CONTROL BERMS
- \_\_\_ ROCK INLET/OUTLET PROTECTION
- \_\_\_ SAND BAG BARRIERS
- \_\_\_ SEDIMENT TRAPS
- \_\_\_ EXISTING VEGETATION
- DUST CONTROL
- STABILIZED CONSTRUCTION INGRESS/EGRESS
- STORM DRAIN INLET PROTECTION
- \_\_\_ OTHER (DESCRIBE):

PART 2 – TO BE COMPLETED BY OWNER/OPERATOR OR CONTRACTOR

I. SCHEDULE OF MAJOR ACTIVITIES

A. PROJECT SCHEDULE

START DATE: \_\_\_\_\_

END DATE: \_\_\_\_\_

B. SEQUENCE OF CONSTRUCTION ACTIVITIES

II. INVENTORY OF POLLUTANTS

A. THE FOLLOWING MATERIALS AND/OR SUBSTANCES ARE EXPECTED TO BE ONSITE DURING CONSTRUCTION

- \_\_\_ PAINTS
- \_\_\_ CONCRETE
- \_\_\_ ASPHALT
- \_\_\_ OIL
- \_\_\_ FUEL
- \_\_\_ HERBICIDES/FERTILIZERS
- \_\_\_ OTHER (LIST):

III. POLLUTION CONTROL MEASURES

A. BEST MANAGEMENT PRACTICES:

- \_\_\_ WINDEROSION/DUST CONTROL
- \_\_\_ DESIGNATED WASHOUT AREAS
- \_\_\_ SOLID WASTE MANAGEMENT
- \_\_\_ CHEMICAL/MATERIAL STORAGE AREA
- \_\_\_ STABILIZED CONSTRUCTION INGRESS/EGRESS
- \_\_\_ EQUIPMENT MAINTENANCE PROCEDURES
- \_\_\_ OTHER (DESCRIBE):

IV. SPILL PREVENTION, RESPONSE AND CLEANUP

A. SPILL PREVENTION: THE BEST MANAGEMENT PRACTICES OUTLINED UNDER POLLUTION CONTROL MEASURES ARE DESIGNED TO PREVENT AND CONTAIN SPILLS OF HAZARDOUS SUBSTANCES.

B. SPILL RESPONSE AND CLEANUP: IN THE EVENT OF ANY ACCIDENTAL SPILL OR RELEASE OF HAZARDOUS SUBSTANCES OR CHEMICALS, THE OWNER/OPERATOR SHALL REPORT SUCH DISCHARGES AS FOLLOWS:

1. IF AN IMMINENT AND SUBSTANTIAL DANGER TO PUBLIC HEALTH AND WELFARE OR TO THE ENVIRONMENT EXISTS, IMMEDIATELY CONTACT EMERGENCY ASSISTANCE (911).

IF ASSISTANCE IS NEEDED FOR THE REMOVAL OF HAZARDOUS WASTE, CONTACT THE CITY OF CHANDLER ENVIRONMENTAL MANAGEMENT OFFICE;

2. FOR DISCHARGES NOT INVOLVING AN IMMINENT AND SUBSTANTIAL DANGER TO PUBLIC HEALTH AND WELFARE OR TO THE ENVIRONMENT, CONTACT THE CITY OF CHANDLER STREETS DIVISION OF THE PUBLIC WORKS DEPARTMENT BY TELEPHONE WITHIN 24 HOURS;

3. PROVIDE A WRITTEN REPORT TO THE CITY OF CHANDLER ENVIRONMENTAL MANAGEMENT OFFICE, AND TO THE CITY OF CHANDLER STREETS DIVISION OF THE PUBLIC WORKS DEPARTMENT WITHIN 3 BUSINESS DAYS OF THE DISCHARGE. THE WRITTEN REPORT MAY BE SUBMITTED VIA MAIL, FAX, OR IN PERSON;

4. SUCH REPORTING SHALL IDENTIFY THE LOCATION, SOURCE OF DISCHARGE AND POLLUTANT(S), EXTENT OF DISCHARGE, POLLUTANT(S) DISCHARGED AND ALL MEASURES TAKEN TO MITIGATE THE DISCHARGE. ADDITIONALLY, IF THE DISCHARGE INCLUDES HAZARDOUS WASTE, THE REPORT SHALL DETAIL THE METHOD OF DISPOSAL AND DISPOSAL FACILITY RECEIVING THE WASTE MATERIAL. THE REPORT SHALL ALSO IDENTIFY ALL BEST MANAGEMENT PRACTICES IMPLEMENTED TO PREVENT SIMILAR DISCHARGES IN THE FUTURE.

V. EROSION AND SEDIMENT CONTROL PLAN PLAN CERTIFICATION

A. I CERTIFY UNDER PENALTY OF LAW, THAT THIS DOCUMENT AND ALL ATTACHMENTS WERE PREPARED UNDER MY DIRECTION OR SUPERVISION IN ACCORDANCE WITH A SYSTEM DESIGNED TO ASSURE THAT QUALIFIED PERSONNEL PROPERLY GATHERED AND EVALUATED THE INFORMATION SUBMITTED. BASED ON MY INQUIRY OF THE PERSONS WHO MANAGE THE SYSTEM OR THOSE PERSONS DIRECTLY RESPONSIBLE FOR GATHERING INFORMATION, THE INFORMATION SUBMITTED IS, TO THE BEST OF MY KNOWLEDGE AND BELIEF, TRUE, ACCURATE, AND COMPLETE. I AM AWARE THAT THERE ARE SIGNIFICANT PENALTIES FOR SUBMITTING FALSE INFORMATION, INCLUDING THE POSSIBILITY OF FINE AND IMPRISONMENT FOR KNOWING VIOLATIONS.

B. THE OWNER/OPERATOR AND/OR CONTRACTOR AS DEFINED IN THE ARIZONA POLLUTANT DISCHARGE ELIMINATION SYSTEM GENERAL PERMIT FOR CONSTRUCTION ACTIVITIES, ADMINISTERED BY THE ARIZONA DEPARTMENT OF ENVIRONMENTAL QUALITY (ADEQ), SHOULD SIGN THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP) AND MAINTAIN THE SWPP ON-SITE AT THE CONSTRUCTION SITE OR OTHER REASONABLY ACCESSIBLE LOCATION DURING NORMAL BUSINESS HOURS.

SIGNATURE: \_\_\_\_\_  
DATE: \_\_\_\_\_  
NAME: \_\_\_\_\_  
TITLE: \_\_\_\_\_

SYMBOL: FILTER SOCK / WADDLE / INLET PROTECTION (BMP-SPC-1 MOD)

SEDIMENT WATTLE STAKING DETAIL (NTS)

NEW SHOULDER BUILDUP PROTECTION SECTION (NTS)

SEDIMENT WATTLE OVERLAP (NTS)

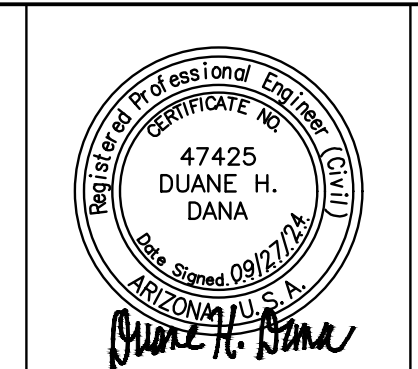
SYMBOL: STABILIZED CONSTRUCTION ENTRANCE (BMP-EC-5)

WASH RACK

WASH RACK

REV. NO.	DATE	DRWN	CHKD	REMARKS

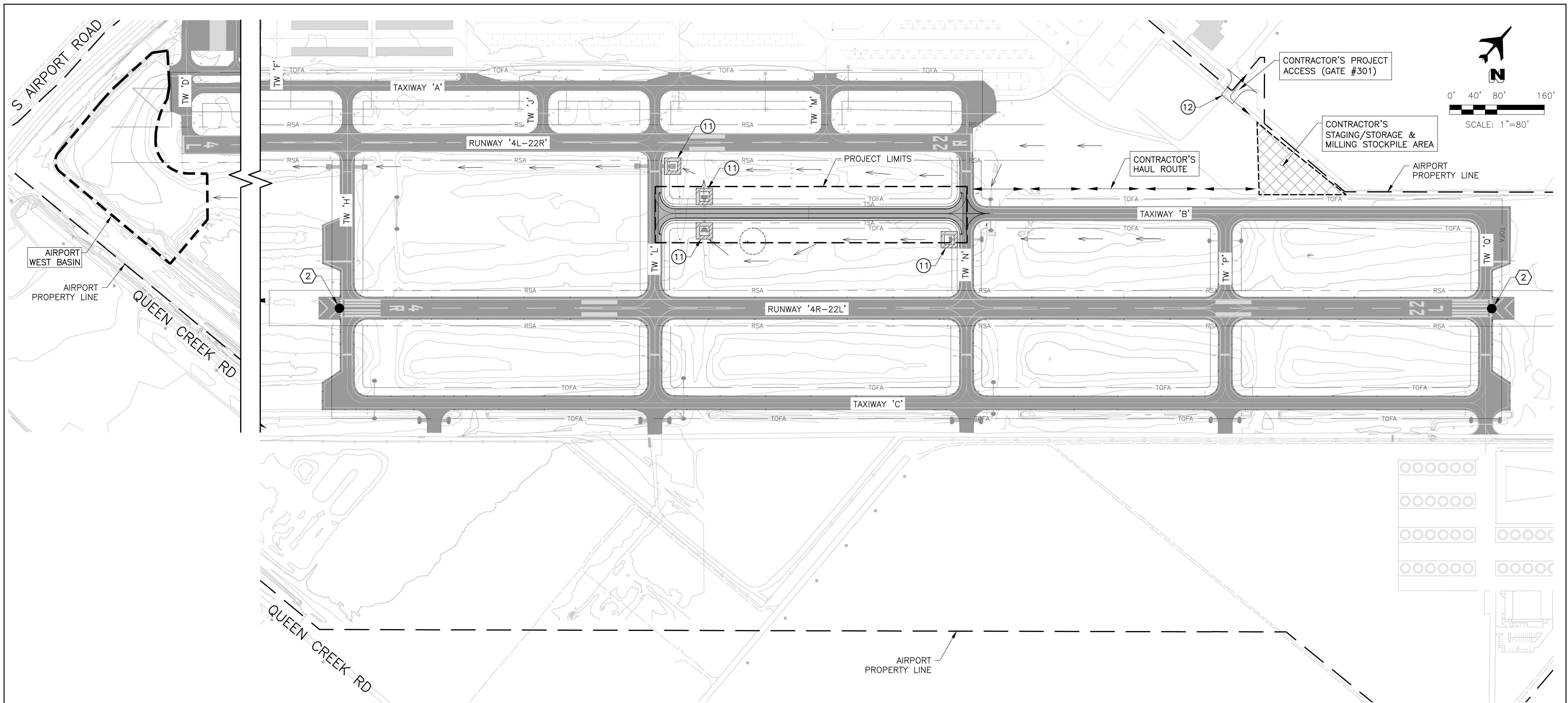
DESIGNED BY: DSO  
DRAWN BY: ERG  
SHEET CHK'D BY: MJH  
CROSS CHK'D BY: DHD  
APPROVED BY: DHD  
DATE: 09/27/24



CITY OF CHANDLER, ARIZONA  
CHANDLER MUNICIPAL AIRPORT  
TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
TAXIWAY 'L' TO TAXIWAY 'N'

EROSION & SEDIMENT CONTROL  
PLAN NOTES & DETAILS

PROJECT NO. 1021015.11  
FILE NAME: 21015\_11-SW1X.DWG  
DWG NO.  
**SW1.1**  
SHEET SW2 OF SW3



- GENERAL NOTES**
- SWPPP IS THE RESPONSIBILITY OF THE CONTRACTOR.
  - CONTRACTOR TO REVIEW TECH SPEC C-102 FOR SWPPP REQUIREMENTS.
  - ORGANIC FILTER BARRIER SHALL BE MAINTAINED UNTIL SEED HAS TAKEN ROOT AND GRASS IS ESTABLISHED.

- CONSTRUCTION NOTES**
- ⑪ FILTER SOCK/WADDLE/INLET PROTECTION (MCESD BMP-SPC-1 MOD) NPI
  - ⑫ STABILIZED CONSTRUCTION ENTRANCE (MCESD BMP-EC-5) NPI

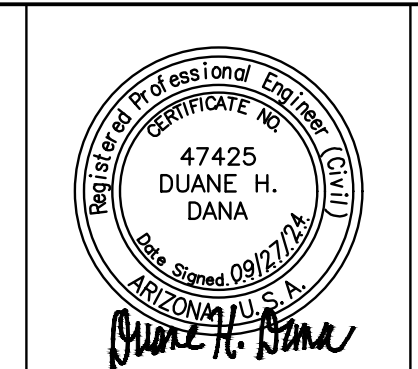
- REFERENCE NOTES**
- ② REFER TO G1.3 FOR SURVEY CONTROL PLAN

- LEGEND**
- CONTRACTOR'S STAGING/STORAGE & MILLING STOCKPILE AREA
  - FILTER SOCK/WADDLE/INLET PROTECTION (MCESD BMP-SPC-1 MOD)
  - STABILIZED CONSTRUCTION ENTRANCE (MCESD BMP-EC-5)
  - FLOW ARROW
  - CONTRACTOR'S HAUL ROUTE

J:\2024\1\021015.11\_CHD\_TW\_B\_IMPROVEMENTS\_PHASE1\CAD\21015.11-SWIX.DWG Sep. 27, 2024 11:23 AM DARIN OAKELEY

REV. NO.	DATE	DRWN	CHKD	REMARKS

DESIGNED BY: DSO  
 DRAWN BY: ERG  
 SHEET CHK'D BY: MJH  
 CROSS CHK'D BY: DHD  
 APPROVED BY: DHD  
 DATE: 09/27/24



CITY OF CHANDLER, ARIZONA  
 CHANDLER MUNICIPAL AIRPORT  
 TAXIWAY 'B' IMPROVEMENTS PHASE 1:  
 TAXIWAY 'L' TO TAXIWAY 'N'

**EROSION & SEDIMENT CONTROL PLAN**

PROJECT NO. 1021015.11  
 FILE NAME: 21015\_11-SWIX.DWG  
 DWG NO.  
**SW1.2**  
 SHEET SW3 OF SW3